



# Appendix E Transport Impact Statement





## Transport Impact Statement

LOT 564 GARVEY ROAD, DARDANUP WEST

STRUCTURE PLAN







PROJECT	Lot 564 Garvey Road, Dardanup West Structure Plan – Transport Impact Statement			
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1. INTRODUCTION

1.1 Background

This Transport Impact Statement (TIS) has been prepared by Flyt in support of the proposed Local Planning Scheme Amendment and Structure Plan for Lot 564 Garvey Road, Crooked Brook.

The site is located approximately 12.5km southeast of Bunbury and 2.2km southwest of the Dardanup townsite. The site has frontage to Garvey Road to the north, as shown in Figure 1, with the eastern boundary approximately 1km west of Boyanup-Picton Road. The subject site has a total area of 40.523 hectares.



Figure 1 Lot 564 Garvey Road site (aerial image source: MetroMap)

The subject site is bounded by rural residential development to the west and south, while the area to the east is undeveloped rural paddocks.

The Concept Plan for the subject site shows a total yield of 20 rural residential lots (see Figure 2) – with a minimum lot size of 1.008 hectares and an average lot size of 1.977 hectares. Vehicle access will be via direct frontage access to Garvey Road (for lots 1-6 and lots 9-12), as well as the existing Morellini Way to the west of the site (for lots 13 and 15), and a new subdivision road connecting Garvey Road through to Ayrshire Road (for lots 7-8, lot 14 and lots 16-20).

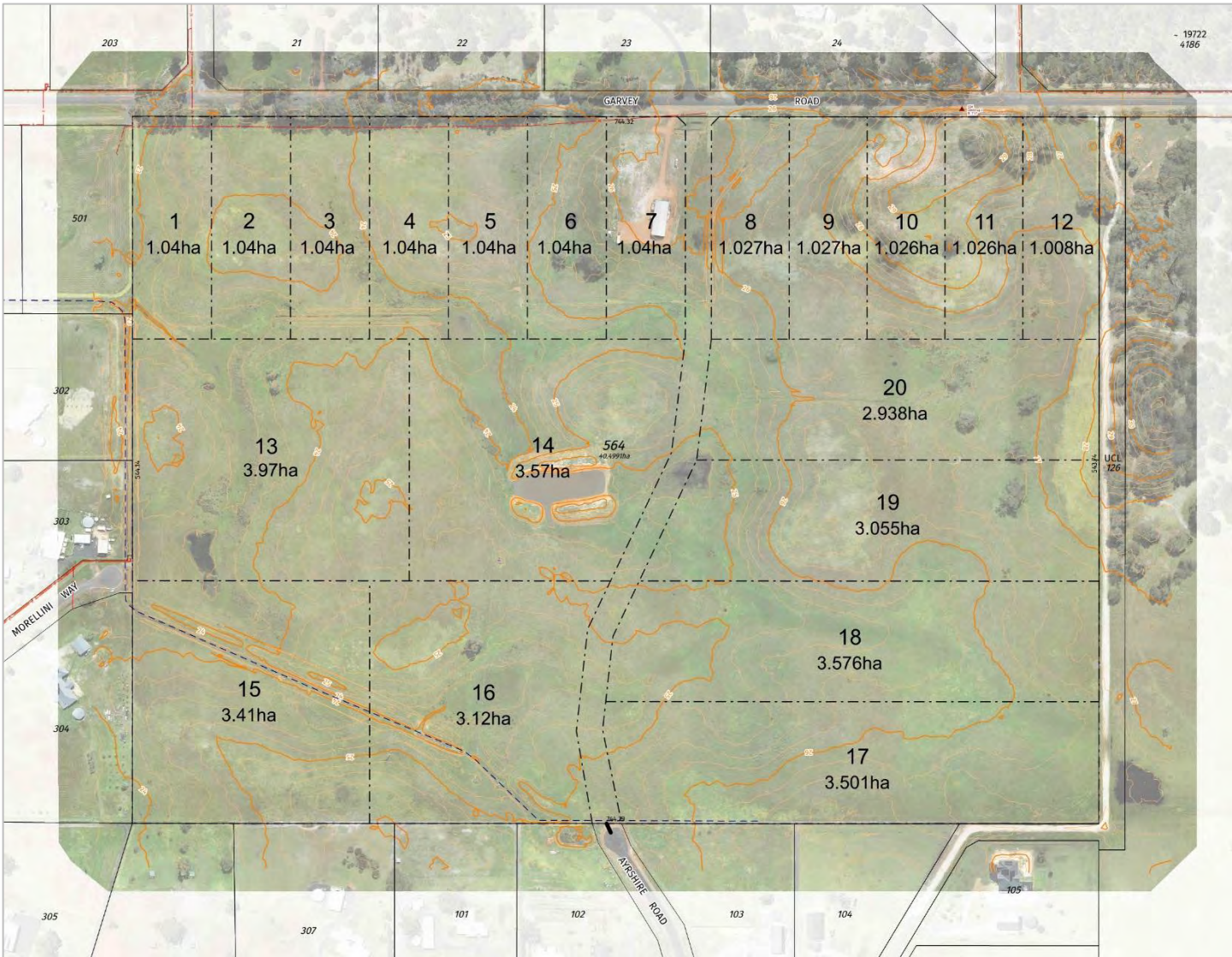


Figure 2 Lot 564 Garvey Road site Concept Plan (source: Harley Dykstra, June 2024)



1.2 Development Site Context

The subject site and all the surrounding lots are currently zoned ‘general farming’ under the existing Town Planning Scheme No.3 (TPS 3), as shown in Figure 3. The site is zoned ‘rural’ under the Shire of Dardanup’s proposed new Local Planning Scheme No. 9 (LPS 9), shown in Figure 4. Under the proposed LPS 9 the lots to the north, west and south of the subject site are zoned as ‘rural residential’.

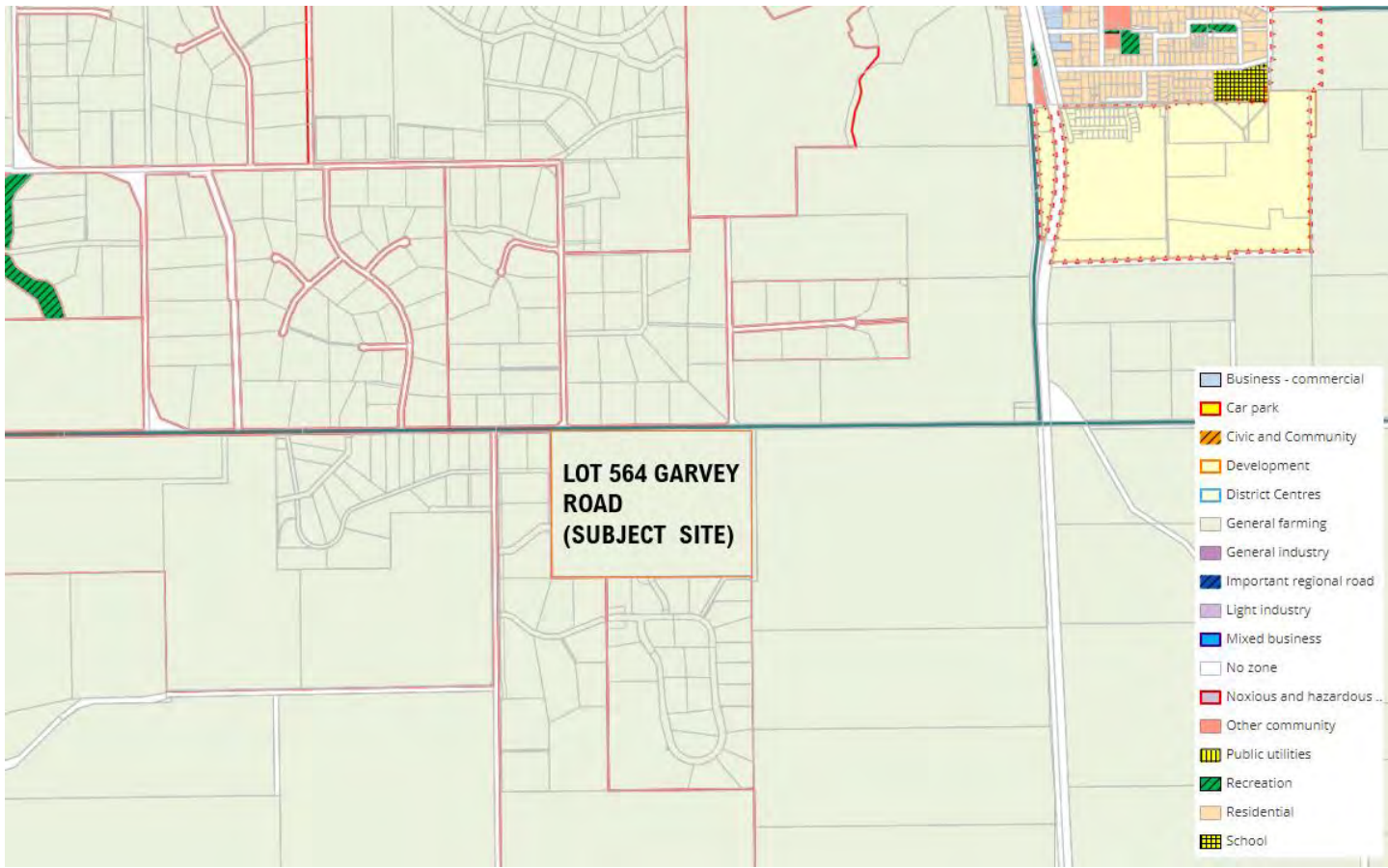


Figure 3 Lot 564 Garvey Road site land use zoning under Town Planning Scheme No. 3 (source: Shire of Dardanup)

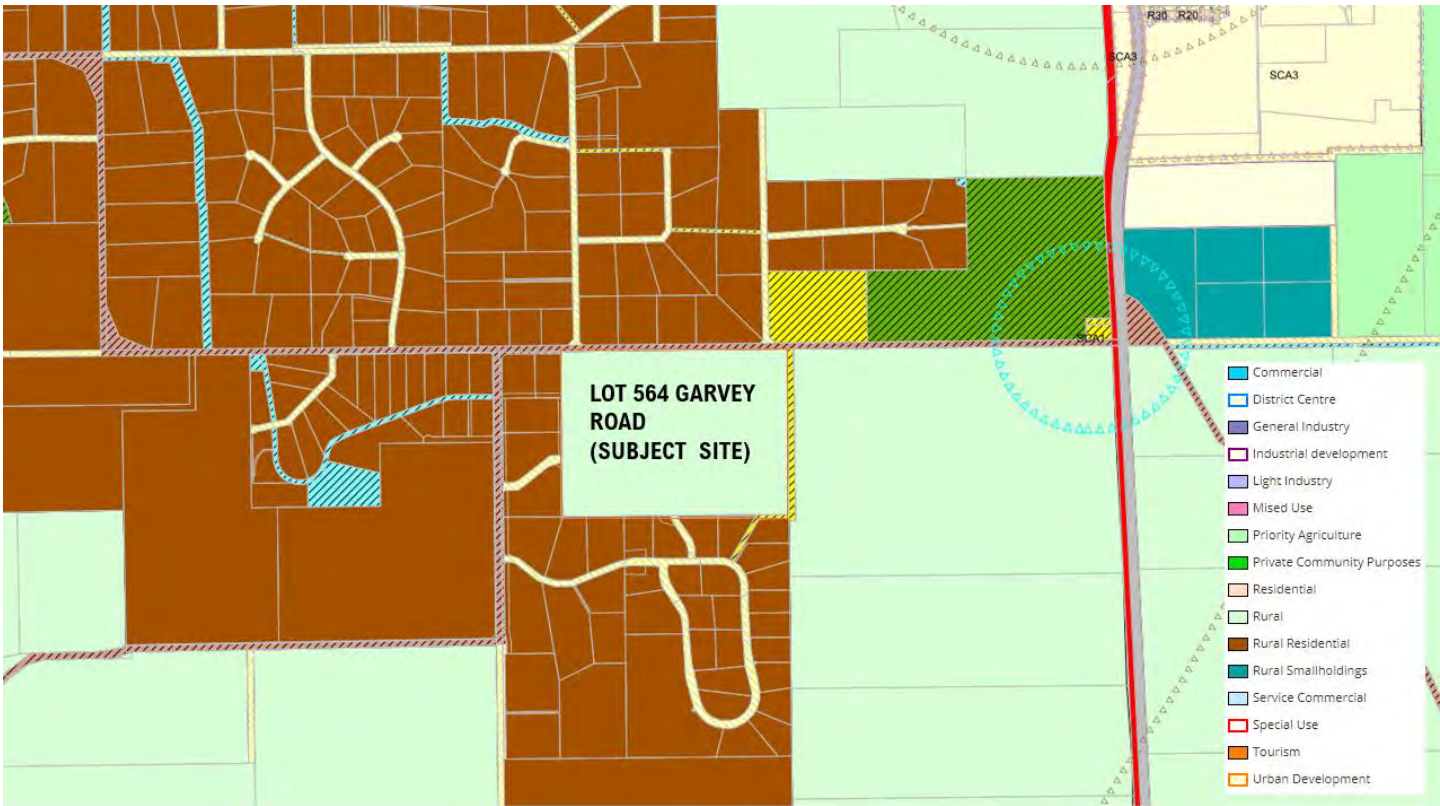


Figure 4 Lot 564 Garvey Road site land use zoning under proposed Local Planning Scheme No. 9 (source: Shire of Dardanup)

1.3 Scope of Work

The scope of the movement network analysis for the concept planning process is based on the assessment provisions of a Structure Plan level assessment set out in Volume 2 of the WA Planning Commission’s (WAPC) Transport Impact Assessment Guidelines (2016).

This TIA report provides a high-level impact assessment based on the concept plan potential land uses. It is expected that the information contained within this report would form the basis of more detailed assessments in the future at Structure Plan refinement and/or Development Application (DA) stage for discrete stages of development across the subject site.



## 2. CONCEPT PLAN PROPOSALS

### 2.1 Site Location

The site is located approximately 12.5km southeast of Bunbury and 2.2km southwest of the Dardanup townsite, as shown in Figure 5.

The site is largely vacant open pastureland with a single shed and a farm dam. Vehicular access to the shed is via Garvey Road. The site is located immediately to the south of Garvey Road (with the eastern boundary approximately 1km west of Boyanup-Picton Road), with rural residential properties to the west and south and undeveloped rural paddocks to the east. The Dardanup Cemetery is located 170m to the east of the site's eastern boundary, on the northern side of Garvey Road.



Figure 5 Lot 564 Garvey Road site (aerial image source: MetroMap)

### 2.2 Potential Land Uses

The proposed Concept Plan for Lot 564 Garvey Road includes the following land uses:

- 20 rural residential lots ranging from 1.008 ha to 3.970 ha, with an average lot size of 1.977 ha.

Based on a total site area of 40.523 ha, the maximum potential rural residential yield is 40 lots.

### 2.3 Access Arrangements

The proposed Concept Plan comprises three vehicle accesses:

- Garvey Road – lots fronting Garvey Road (proposed lots 1-6 and lots 9-12 with an approximate size of 1 hectare) will have direct access to Garvey Road. The length of Garvey Road frontage for each lot is approximately 60m.
- Morellini Way – lots on the western side of the concept plan area (proposed lots 13 and 15 with an approximate size of 3.4 ha to 3.9 ha) will be accessed from the existing Morellini Way cul-de-sac head.
- New Subdivision Road – a new subdivision road connecting Garvey Road through to Ayrshire Road will provide access to lots 7 and 8 (approximately 1 ha) adjacent to Garvey Road, and lots 14, 16-20 (approximately 3 ha to 3.5 ha) in the centre of the site.
  - The new subdivision road will have a 25m road reserve width.
  - The new subdivision road will be aligned to support a posted speed limit of 70km/h.
  - The new subdivision road would form a minor approach road with Garvey Road retaining priority.

The proposed concept plan layout and access arrangements are shown in Figure 6.



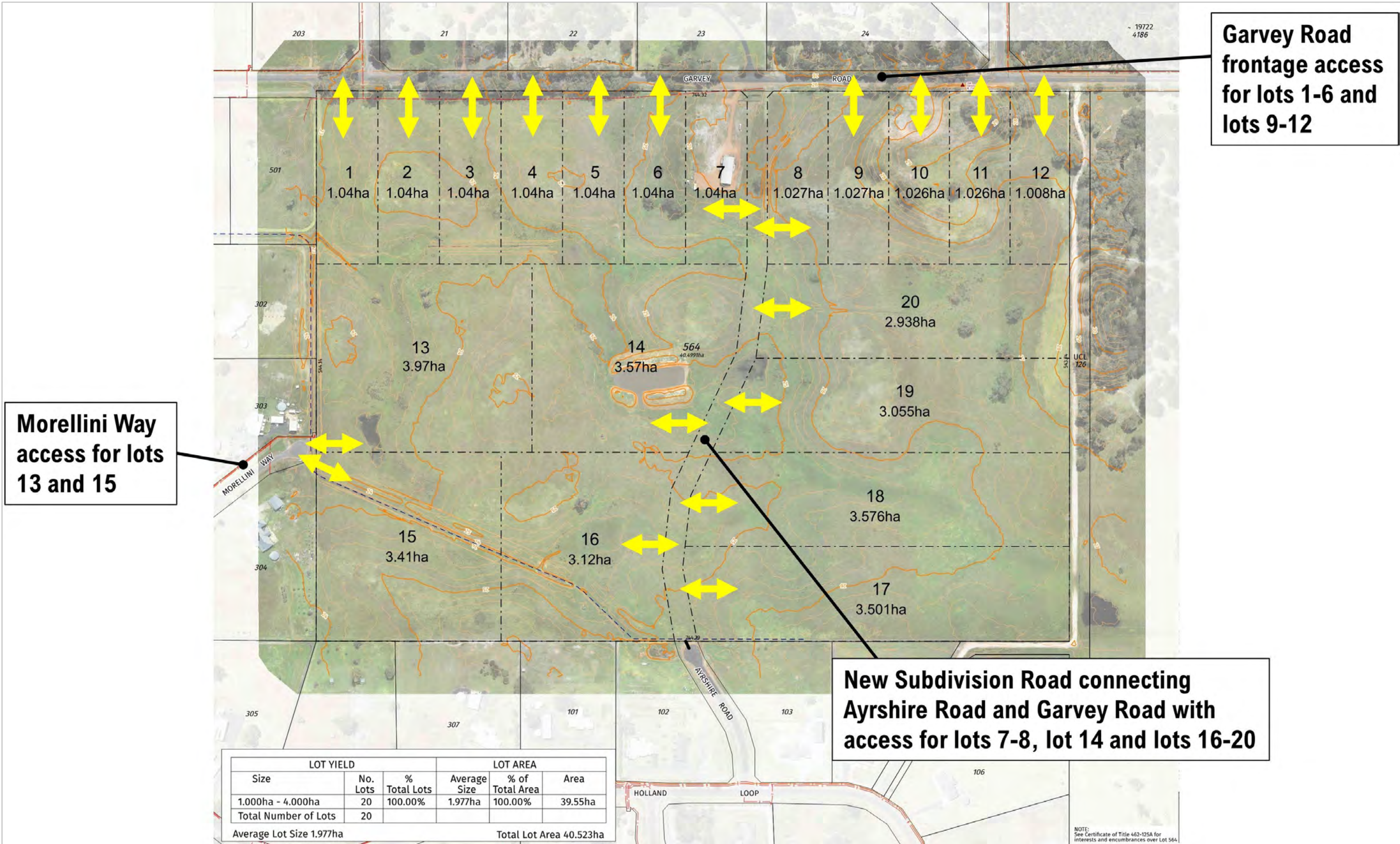


Figure 6 Proposed concept layout plan and access arrangements for the Lot 564 Garvey Road site (source: Harley Dykstra, June 2024)



### 3. EXISTING SITUATION

#### 3.1 Regional Context

The site is located approximately 12.5km southeast of Bunbury and 2.2km southwest of the Dardanup townsite, as shown in Figure 5.

Access to the development site is reliant upon private vehicle access via Garvey Road. State roads such as Boyanup-Picton Road, South Western Highway and the Bunbury Outer Ring Road can be accessed directly via Garvey Road or via other local roads such as Dardanup Road West and Moore Road.

There are no existing footpaths or cycle paths connecting to the development site therefore it is assumed all access to and from the site will be via private vehicle.

#### 3.2 Local Context

The subject site and all the surrounding lots are currently zoned 'general farming' under the existing TPS 3. The site is zoned 'rural' under the Shire of Dardanup's proposed new LPS 9, while the lots to the north, west and south of the subject site are zoned as 'rural residential'. The land to the east of the site, between the site and Boyanup-Picton Road, has also been proposed as 'rural' under LPS 9. The proposed land use zonings under TPS 9 are shown in Figure 7.

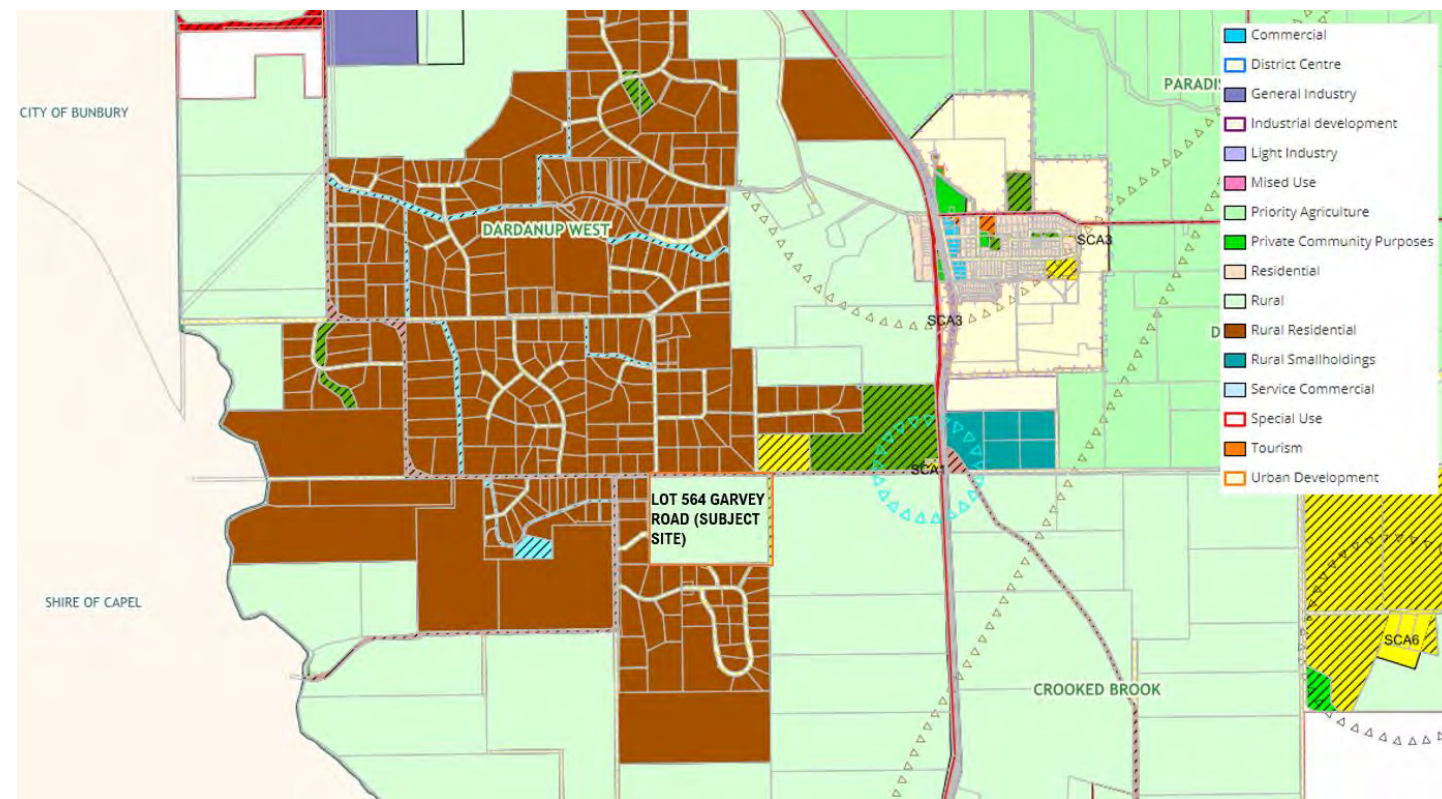


Figure 7 Lot 564 Garvey Road site and surrounding area land use zoning under proposed Local Planning Scheme No. 9 (source: Shire of Dardanup)

The townsite of Dardanup is a 2.4km driving distance from the northeast boundary of the site. The townsite offers amenities such as two primary schools, library, service station, post office, bakery, and general store.

A wider variety of amenities are available in Bunbury which is a 16km driving distance from the northwest of the site, via Dardanup Road West, South Western Highway, Robertson Drive and Picton Road.

#### 3.3 Pedestrian Connections

There is currently no formal pedestrian network connecting the subject site to the surrounding areas of rural residential development, or any of the local land uses.

The Walk Score walkability assessment tool considers the site to be "car-dependent" where almost all errands require a car, with a walk score of 0 out of 100. The 10-minute and 30-minute walkable catchments from the subject site are shown in Figure 8 and Figure 9.

All pedestrian movements within the catchments would be via unsealed gravel shoulders alongside local roads, which is not suitable for all users.

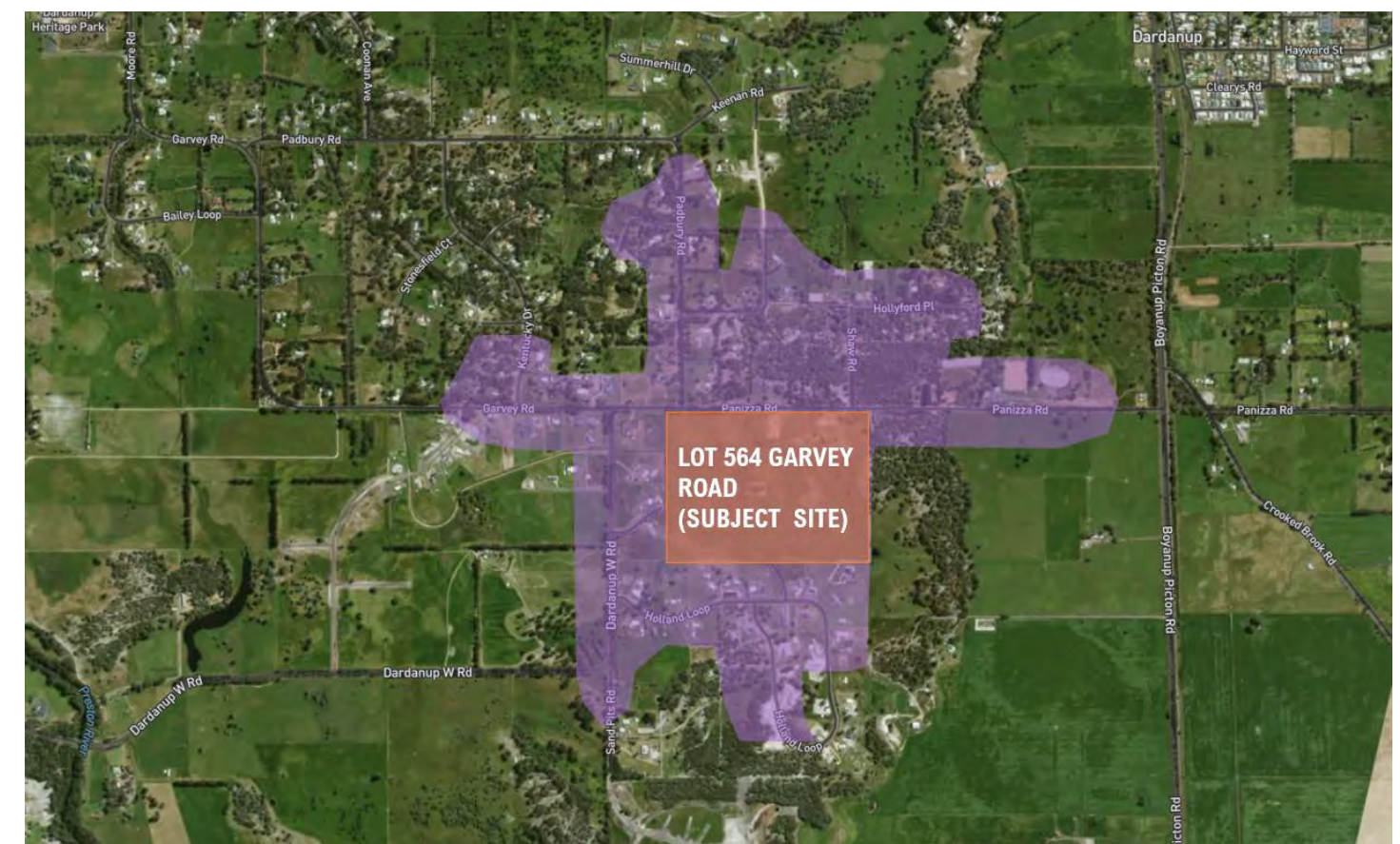


Figure 8 10-minute walking catchment from the Lot 564 Garvey Road site (source: Planwisely)



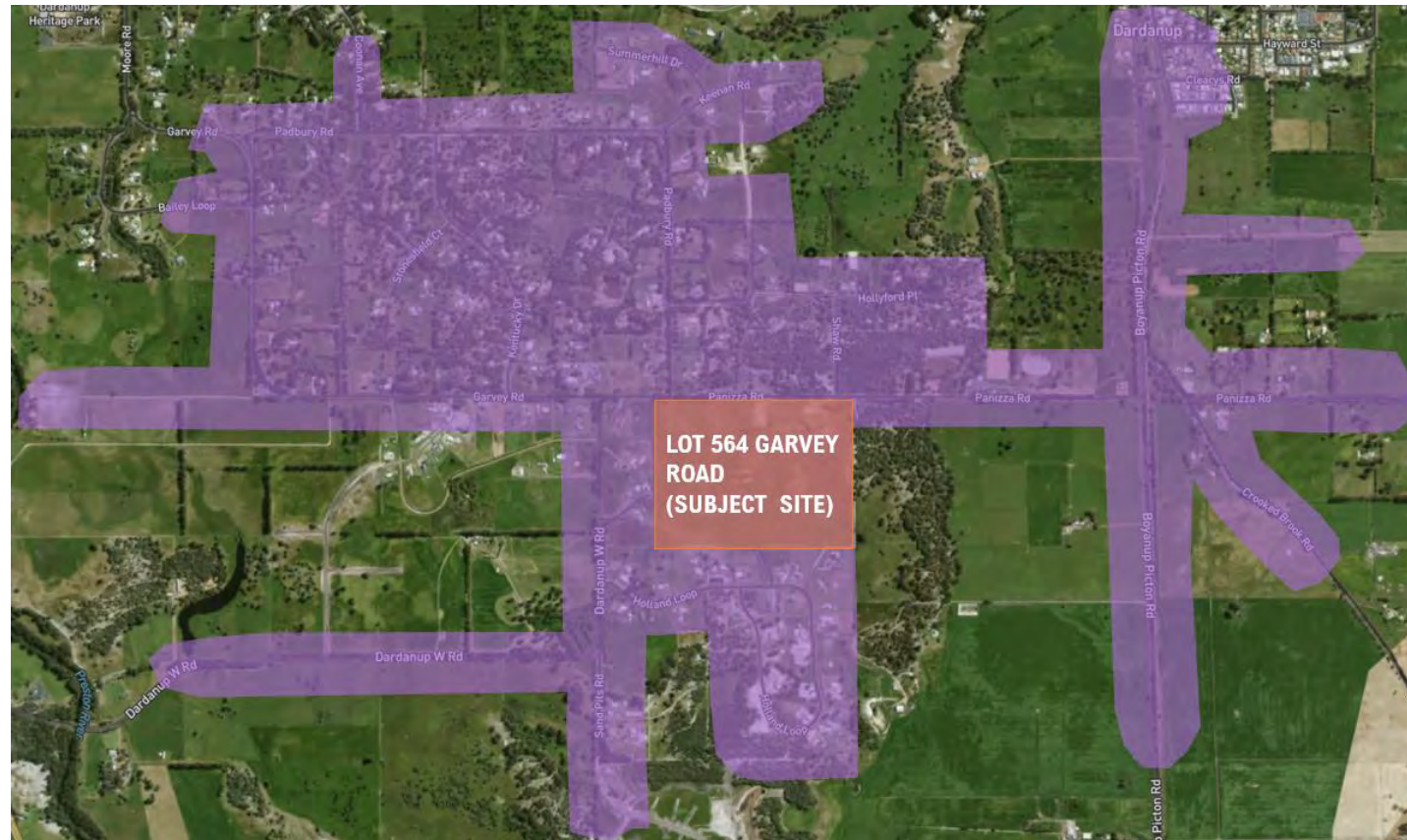


Figure 9 30-minute walking catchment from the Lot 564 Garvey Road site (source: Planwisely)

### 3.4 Bicycle Routes

There are currently no bicycle routes connecting the subject site to the surrounding areas of rural residential development, or any of the local land uses.

The City of Bunbury and the Shires of Dardanup, Capel, Collie, Donnybrook-Balingup, and Harvey, in collaboration with the Department of Transport and the South West Development Commission, developed the Bunbury Wellington 2050 Cycling Strategy.

The strategy aims to identify central themes and opportunities for enabling bike riding across the region and produce strategic and operational cycle network plans for identified regional centres and surrounding areas.

The 2050 cycling network proposed across the Bunbury Wellington area is shown in Figure 10 and includes:

- Primary, secondary, and local routes centered around Bunbury and bounded by the Bunbury Outer Ring Road
- Beyond the Bunbury Outer Ring Road and in the vicinity of Dardanup, tourist trails are proposed along the Preston River, Gavin's Gully and along the freight rail line. All these potential routes are within an easy cycling distance of 1.0-2.5km from the subject site.
- On-road routes to the east of Dardanup through the Ferguson Valley.

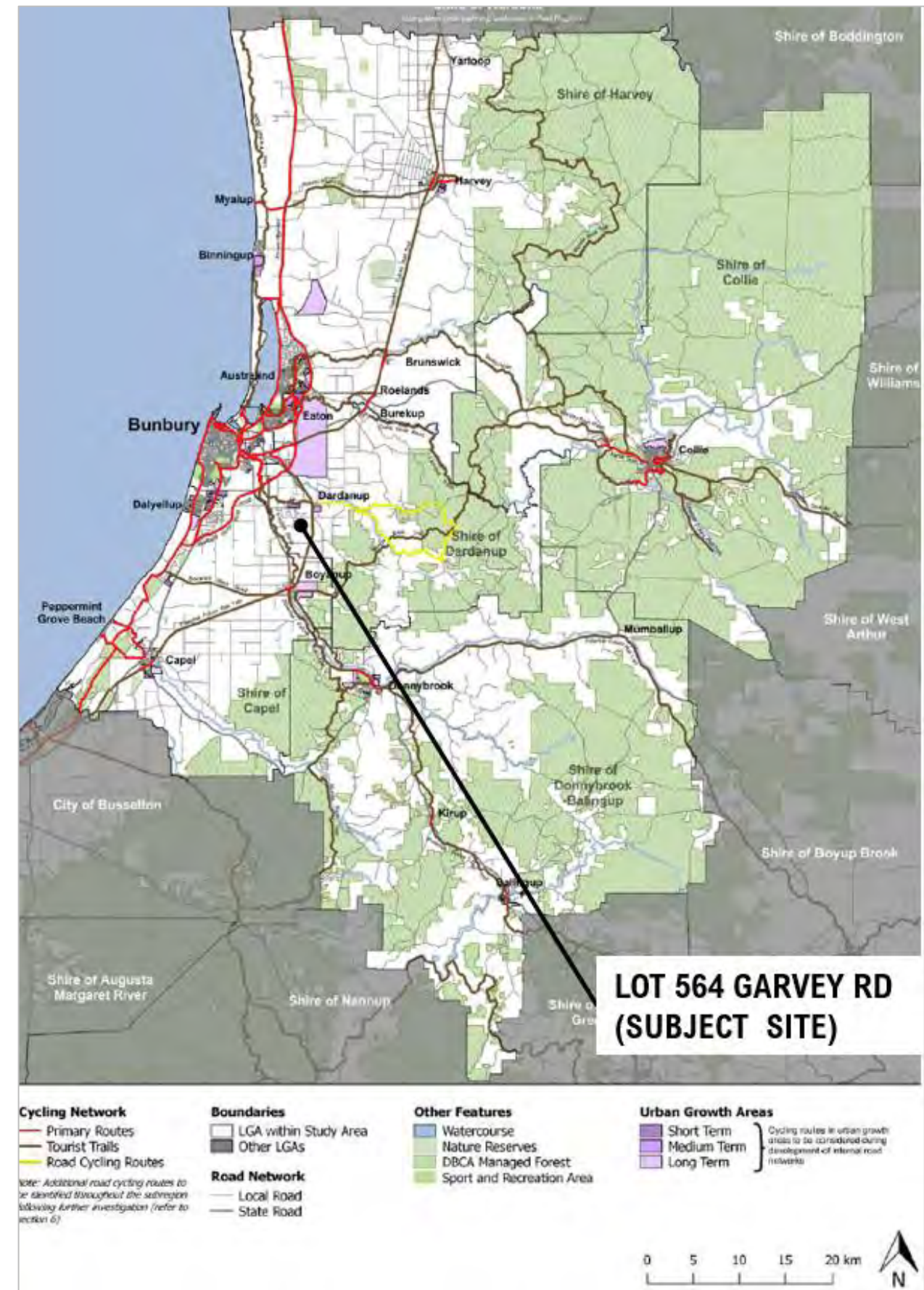


Figure 10 Proposed 2050 cycling network (source: Department of Transport – Bunbury-Wellington 2050 Cycling Strategy)



The Shire of Dardanup's Local Bike Plan was last updated in March 2023, however no routes were proposed in the vicinity of the subject site. All cycling movements to and from the site would be on the existing local road network, which is not suitable for all cyclists.

### 3.5 Public Transport Access

There are no public transport services that provide access to the subject site. The nearest public transport services are the twelve Bunbury bus routes operated by TransBunbury between Bunbury and Dalyellup, the Bumbury Health Campus, Australind, and Eaton.

### 3.6 Local Road Network

All roads in Western Australia are designed a road hierarchy category by Main Roads WA – the category assigned to a road indicates the function of the road and indicates which roads form part of higher order corridors – a description of the road hierarchy categories is provided below:

- **Primary Distributors:** Provide for major regional and inter-regional traffic movement and carry large volumes of generally fast moving traffic. Some are strategic freight routes, and all are State Roads. They are managed by Main Roads WA.
- **District Distributor A:** Urban area roads (not applicable to the Lot 564 Garvey Road site).
- **District Distributor B:** Urban area roads (not applicable to the Lot 564 Garvey Road site).
- **Regional Distributor:** Rural – Roads that are not Primary Distributors, but which link significant destinations and are designed for efficient movement of people and goods within and beyond regional areas. They typically have traffic volumes above 100 vehicles per day. They are managed by Local Government.
- **Local Distributor:** Rural – Connect to other Rural Distributors and to Rural Access Roads. Not Regional Distributors, but which are designed for efficient movement of people and goods within regional areas. They typically have traffic volumes up to 6,000 vehicles per day in built up areas and only up to 100 vehicles per day in non-built up areas. They are managed by Local Government.
- **Access Roads:** Provide access to abutting properties with amenity, safety and aesthetic aspects having priority over the vehicle movement function. These roads are bicycle and pedestrian friendly. They typically have traffic volumes up to 3,000 vehicles per day in built up areas and only up to 75 vehicles per day in non-built up areas. They are managed by Local Government.

The local road hierarchy within proximity of the Lot 564 Garvey Road site is shown in Figure 11. This can be summarised as:

- South Western Highway and Boyanup-Picton Road are both classified as **Primary Distributors** under the control of Main Roads WA.
- Garvey Road and Dardanup Road West are classified as a **Local Distributors** under the control of the Shire of Dardanup.
- All the other roads in the immediate vicinity of the site, including Morellini Way and Ayrshire Road, are classified as **Access Roads** under the control of the Shire of Dardanup.

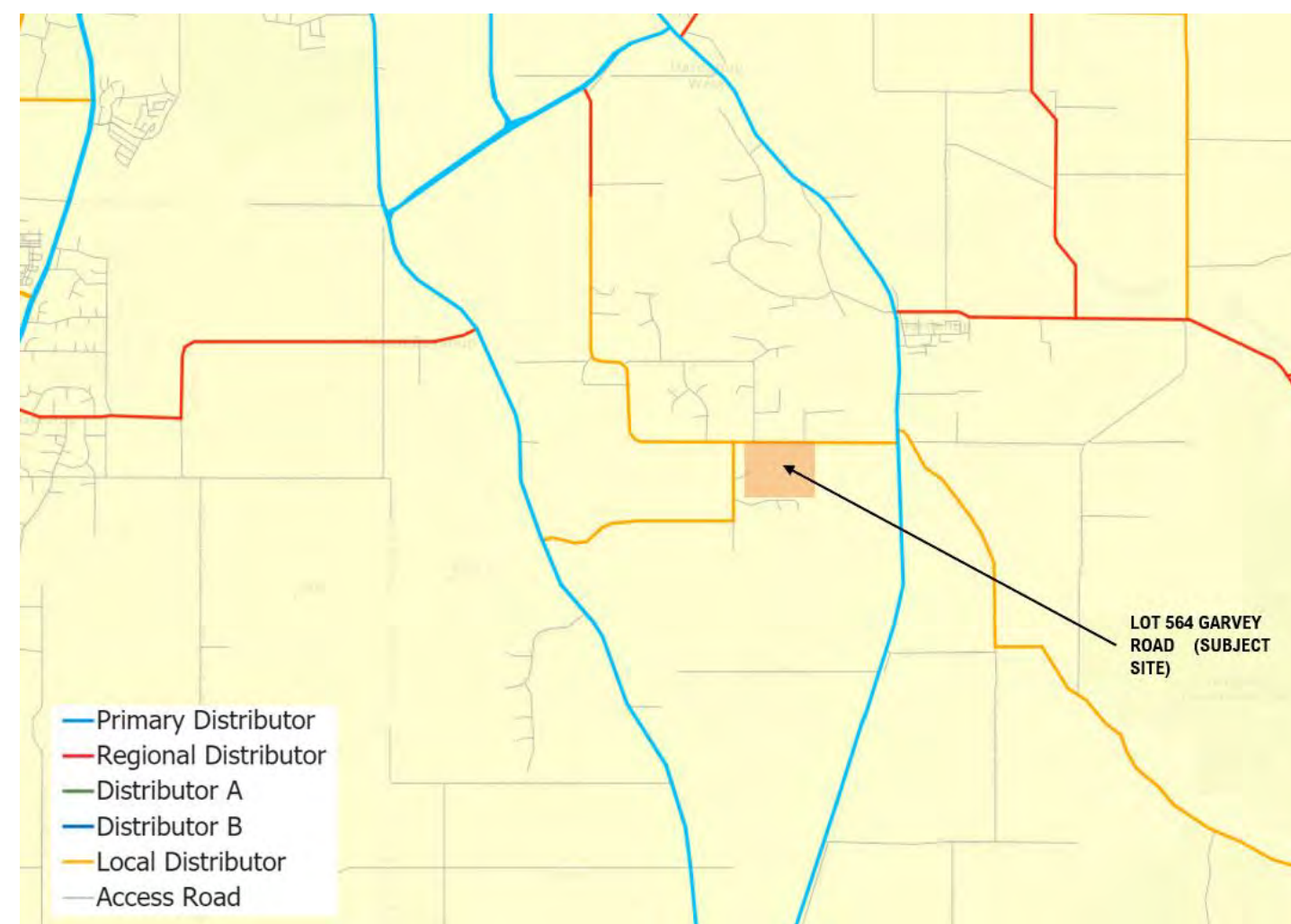


Figure 11 Local road hierarchy in vicinity of the Lot 564 Garvey Road site (source: Main Roads WA)



Figure 12 shows the posted speed limits on the roads surrounding the Lot 564 Garvey Road site. Garvey Road and Dardanup Road West currently have an 80km/h posted speed limit, while South Western Highway and Boyanup-Picton Road have an 110km/h speed limit. The local access roads servicing the rural residential land uses typically have a speed limit of 70km/h.

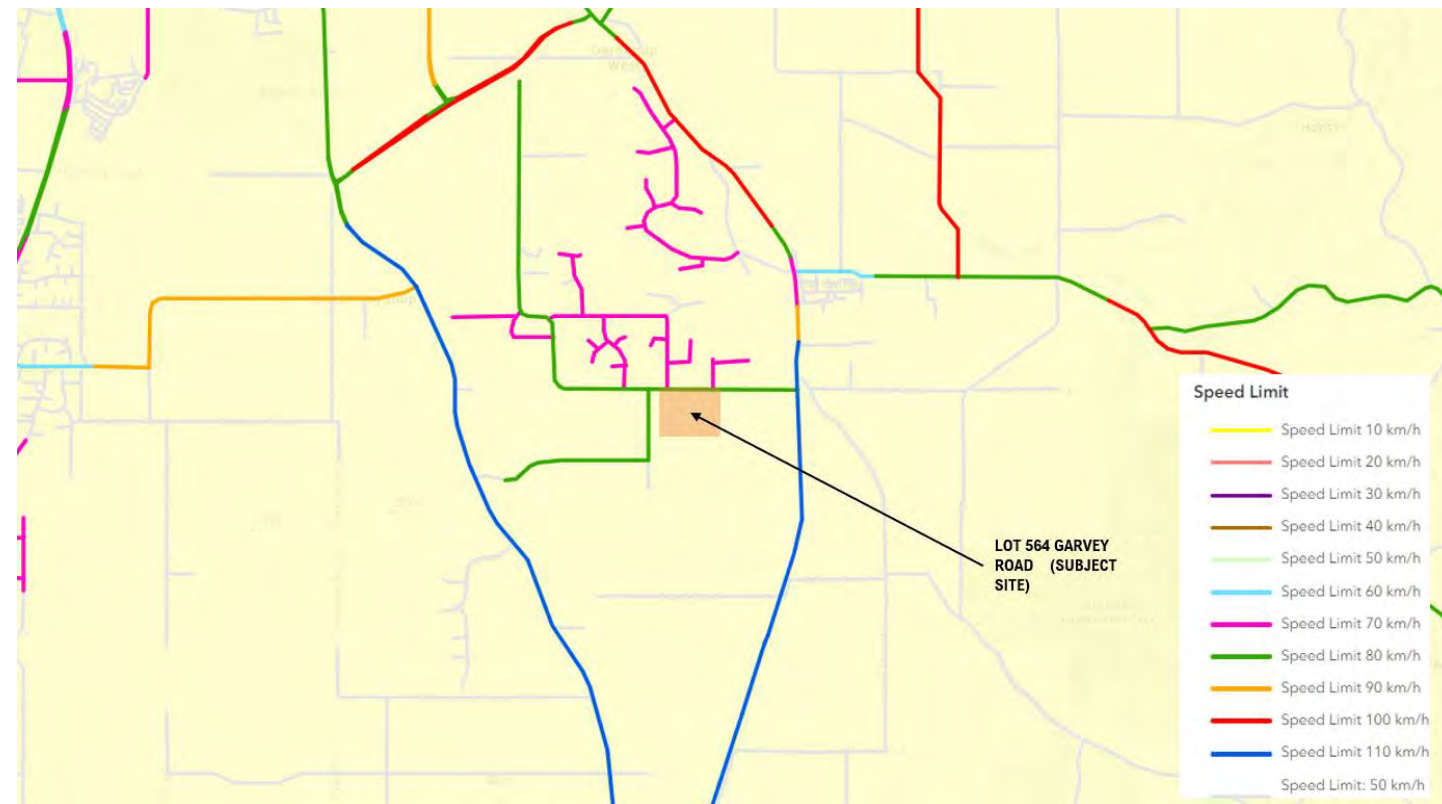


Figure 12 Posted speed limits in vicinity of the Lot 564 Garvey Road site (source: Main Roads WA)

### 3.7 Frontage Roads

The Lot 564 Garvey Road site has frontage access to Garvey Road, while the lots on the western side of the concept plan area will be accessed via Morellini Way (to the west of the site), and the lots in the centre of the concept plan area will be accessed via a new subdivision road connecting Garvey Road through to Ayrshire Road in a north-south alignment across the site.

#### Garvey Road

Garvey Road runs east-west for 3.3km west of Boyanup-Picton Road, before making a right angled turn and running north-south before terminating at the roundabout controlled intersection with Moore Road and Bailey Loop. A cross section of Garvey Road along the development site frontage is shown in Figure 13.

Garvey Road has the following features:

- Sealed to a width of 6m with unsealed shoulders within a 20m road reserve.
- Central separation line marking.
- 80 km/h speed limit.



Figure 13 Garvey Road along development frontage – looking west (source: Google Streetview)



### Morellini Way

Morellini Way is an existing cul-de-sac to the west of the development site, approximately 220m long, which connects to Dardanup Road West. A cross section of Morellini Way to the west of the cul-de-sac head is shown in Figure 14.

Morellini Way has the following features:

- Sealed to a width of 6m with unsealed shoulders within a 25m road reserve.
- No central line marking.



Figure 14 Morellini Way approaching the cul-de-sac – looking east (source: Google Streetview)

### Ayrshire Road

Ayrshire Road is an existing cul-de-sac to the south of the development site, approximately 150m long, which connects to Holland Loop (which itself connects to Dardanup Road West). Ayrshire Road was constructed within the last 3 years. An aerial view of the cross section of Ayrshire Road to the south of the development site is shown in Figure 15.

Ayrshire Road has the following features:

- Sealed to a width of 6m with unsealed shoulders within a 22m road reserve.
- No central line marking.



Figure 15 Ayrshire Road aerial view (source: Shire of Dardanup Intramaps)



### New Subdivision Road

A new subdivision road is proposed connecting Garvey Road through to Ayrshire Road in a north-south alignment across the site. The alignment of the new subdivision road is shown in Figure 16.

The new subdivision road would have the following features:

- A 25m road reserve width to accommodate sealed pavement, shoulders, 1:6 batters and 1.0m wide level surface between the property boundary and drainage to accommodate services.
- The road aligned would support a posted speed limit of 70km/h.
- The new subdivision road would form a minor approach road with Garvey Road retaining priority.

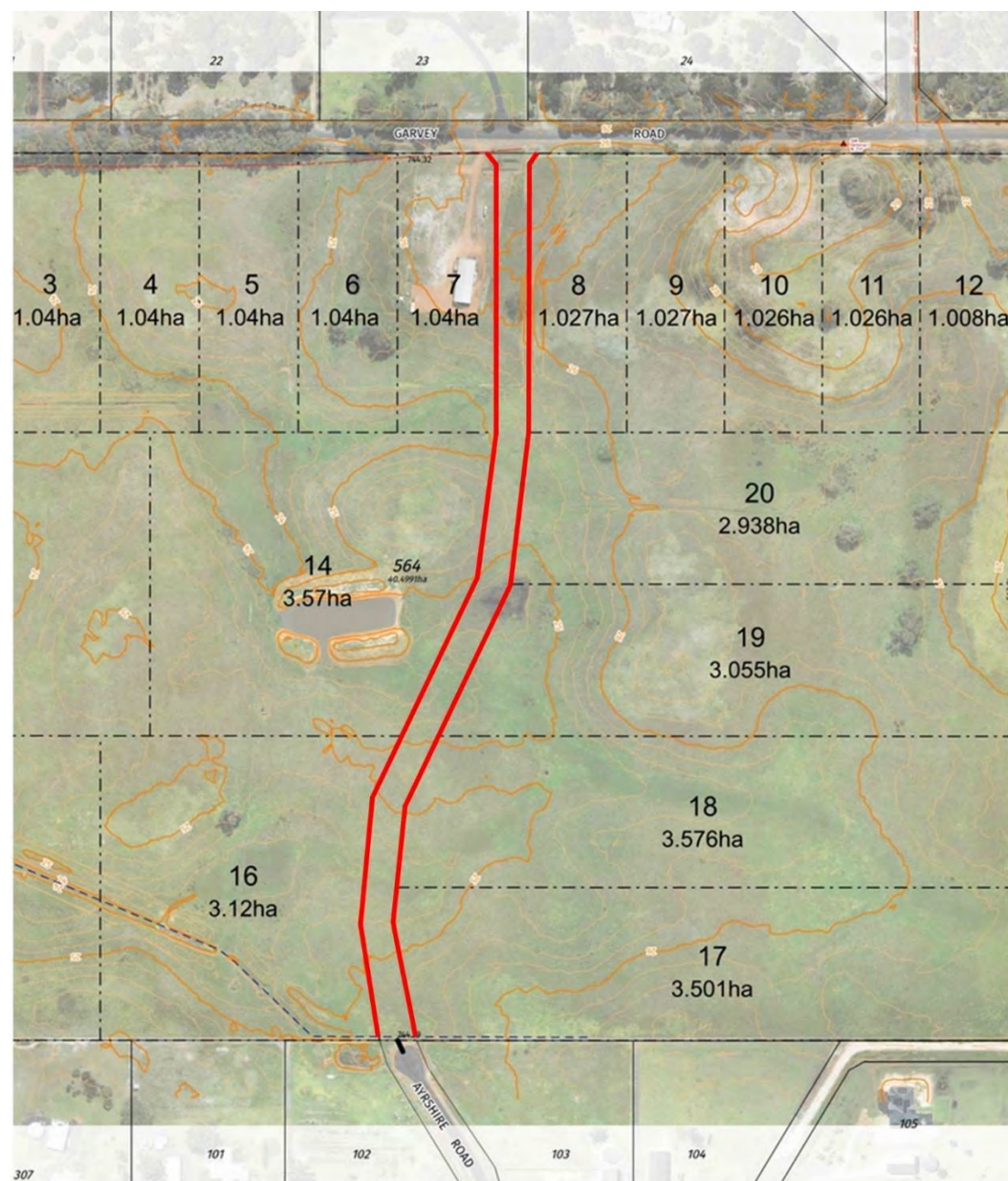


Figure 16 New subdivision road connecting Garvey Road through to Ayrshire Road (source: Harley Dykstra, June 2024)

### 3.8 Existing Vehicle Access

Lot 564 Garvey Road is largely vacant open pastureland with a single shed and a farm dam. The location of the existing vehicular access to the property, via Garvey Road, is shown in Figure 17.

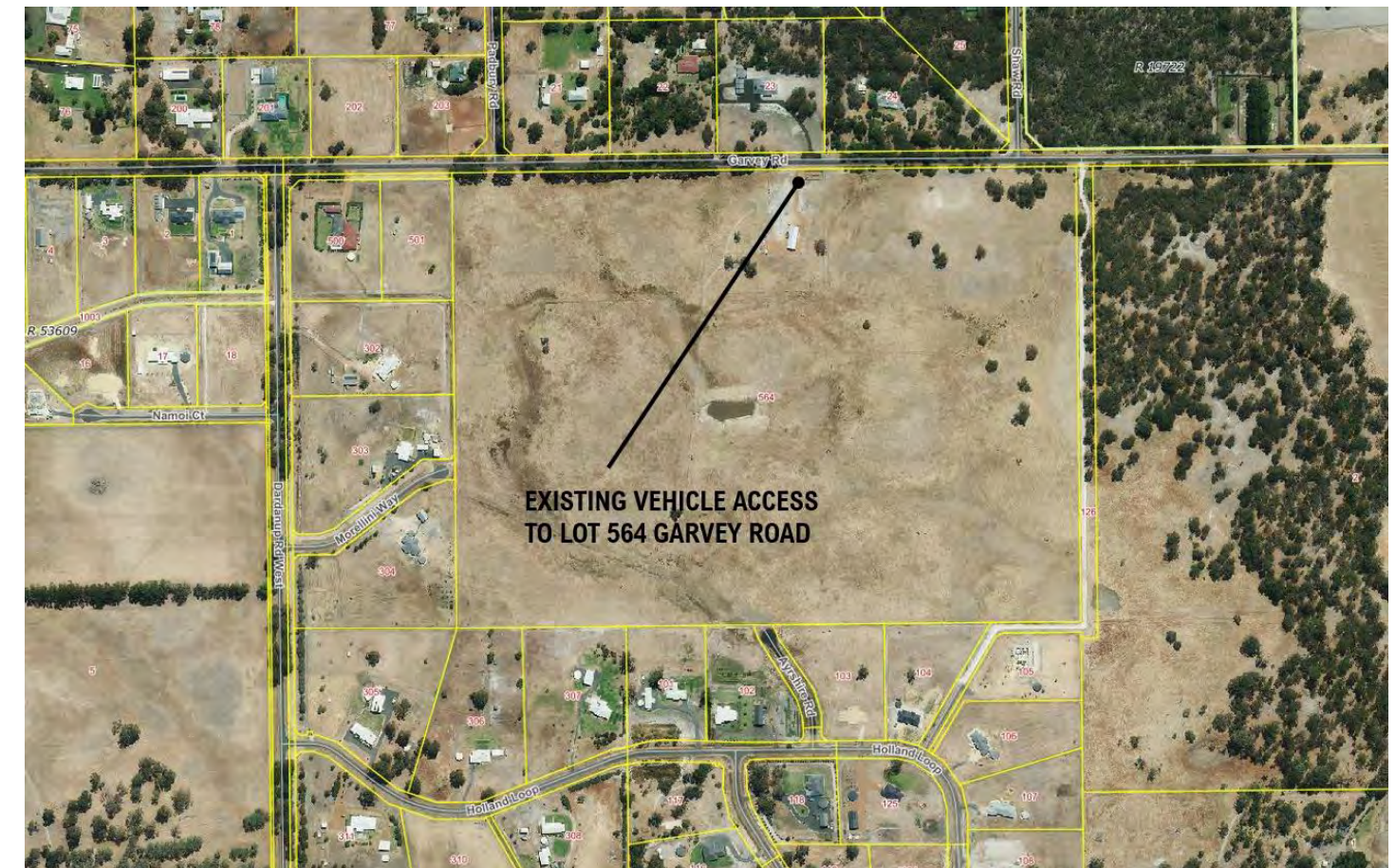


Figure 17 Access to existing building on site via Garvey Road (source: Shire of Dardanup Intramaps)



## 4. INTERNAL TRANSPORT NETWORKS

### 4.1 Road Network

The proposed Concept Plan includes an internal access road providing direct access to 8 lots (lots 7-8, lot 14 and lots 16-20).

It is proposed that 10 lots would be directly accessed off the existing Garvey Road corridor (lots 1-6 and lots 9-12), with the remaining 2 lots being accessed off the existing Morellini Way cul-de-sac head (lots 13 and 15).

### 4.2 Path Network

No path network is proposed within the Lot 564 Garvey Road site. The proposed development has a maximum potential rural residential yield of 40 lots, with the Concept Plan proposing only 20 lots.

The development site is not considered a through route for local walking or cycling trips – as such no path network is proposed within the development site.

### 4.3 Vehicular Access

The proposed Concept Plan comprises three vehicle accesses which are shown in Figure 18 and discussed below:

#### Garvey Road

- Lots fronting Garvey Road (proposed lots 1-6 and lots 9-12 with an approximate size of 1 hectare) will have direct access to Garvey Road. The length of Garvey Road frontage for each lot is approximately 60m.
- The 10 rural lots fronting Garvey Road would account for approximately 50% of all daily traffic movements generated by the site.
- The existing rural residential land uses to the west of the proposed development site includes direct frontage access to Garvey Road.

#### Morellini Way

- Lots on the western side of the concept plan area (proposed lots 13 and 15 with an approximate size of 3.4 ha to 3.9 ha) will be accessed from the existing Morellini Way cul-de-sac head.
- These 2 rural lots would account for 10% of all daily traffic movements generated by the site.

#### New Subdivision Road

- A new subdivision road connecting Garvey Road through to Ayrshire Road will provide access to lots 7 and 8 (approximately 1 ha) adjacent to Garvey Road, and lots 14, 16-20 (approximately 3 ha to 3.5 ha) in the centre of the site.

- These 8 rural lots would account for 40% of all daily traffic movements generated by the site.
- Of this 40% of all daily traffic generated by the site it is expected that 30% would travel via the Garvey Road corridor and 10% by the Ayrshire Road corridor.

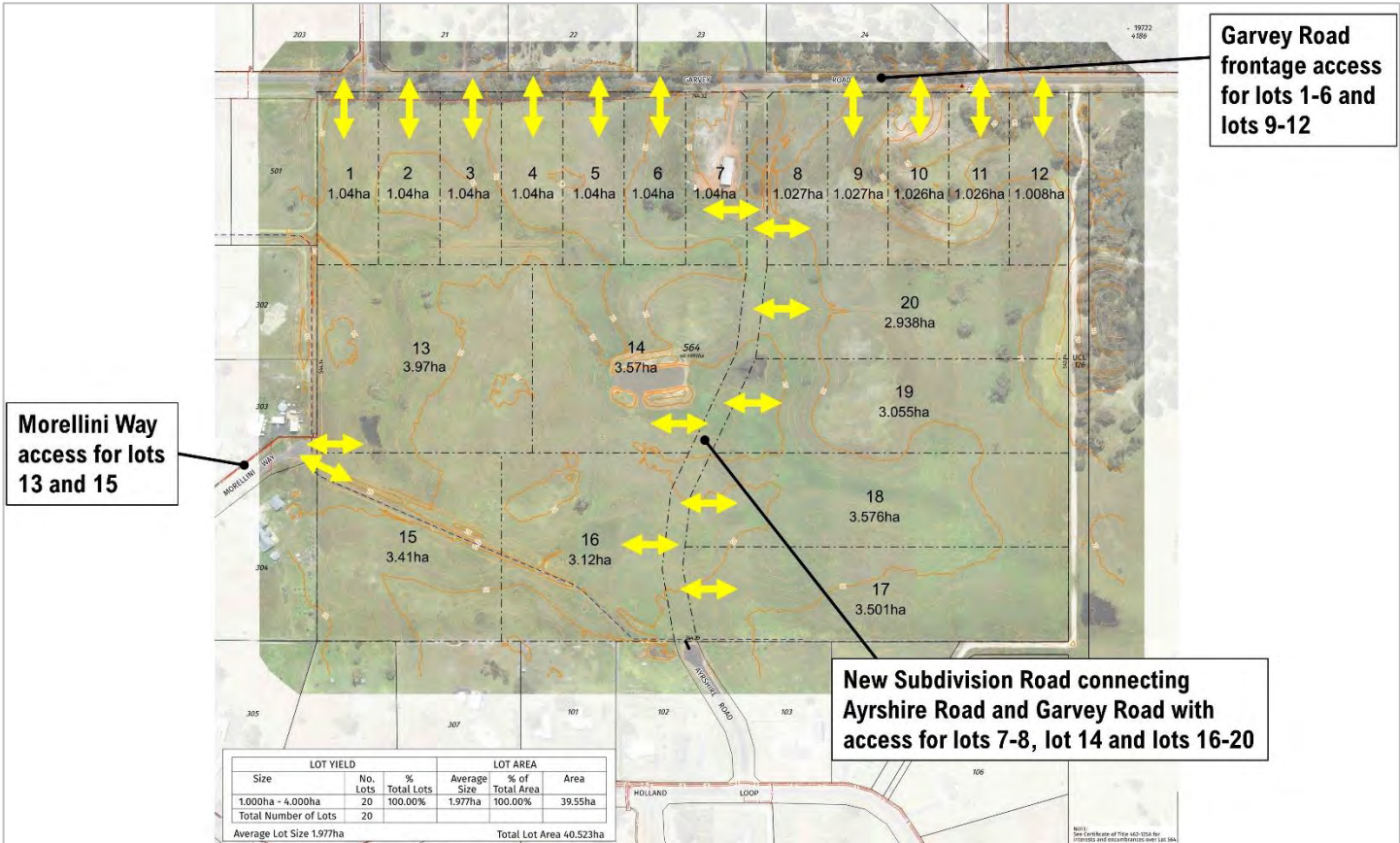


Figure 18 Lot 564 Garvey Road proposed concept plan access arrangements (source: Harley Dykstra, June 2024)



## 5. EXTERNAL TRANSPORT NETWORKS

### 5.1 Traffic Volumes

The most recent traffic count for Garvey Road at count site 53861 (approximately 2.1km to the west/north of the development site) was undertaken by Main Roads WA in 2023.

Garvey Road was found to carry 260 vehicles per day (vpd) on an average weekday, with 20.6% heavy vehicles.

The variation of traffic volumes over a typical 24 hours and the observed 85th percentile vehicle travel speeds are shown in Figure 19.

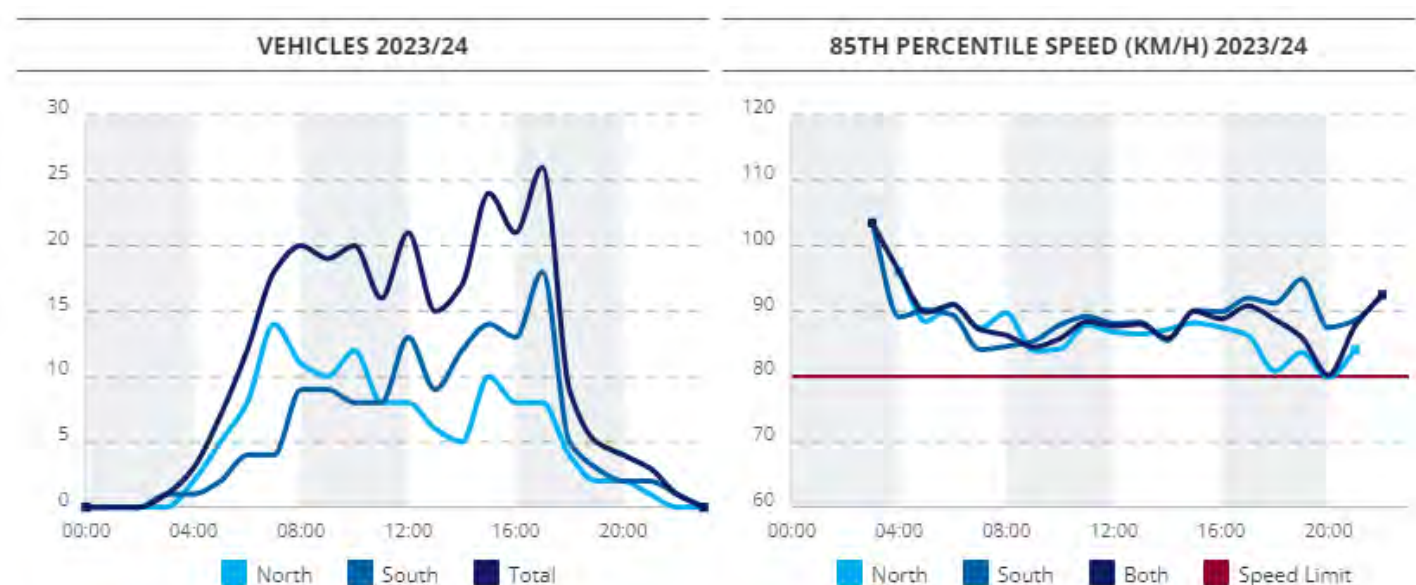


Figure 19 Garvey Road weekday traffic count and travel speeds (source: Main Roads WA)

The 85th percentile vehicle speed is best described as the speed at or below which 85 percent of all vehicles are observed to travel past a monitored point – this is considered to be the speed adopted by reasonable people, according to the road environment. Where the 85th percentile vehicle speed is greater than the road's posted speed limit, it suggests that the road environment does not fit with the posted speed limit. This is the case at Garvey Road count site 53861, where the observed 85th percentile speeds are greater than the posted speed limit.

The most recent traffic count for Dardanup Road West at count site 53865 (approximately 450m to the south of the intersection with Holland Loop) was undertaken by Main Roads in 2023. Dardanup Road West was found to carry 435 vpd on an average weekday, with 24.8% heavy vehicles. The variation of traffic volumes over a typical 24 hours and the observed 85th percentile vehicle travel speeds are shown in Figure 20.

This indicates there is an imbalance in the speeds travelled by each direction of traffic, with the observed 85th percentile speeds of eastbound traffic exceeding the speed limit by between 15 and 25 km/h.

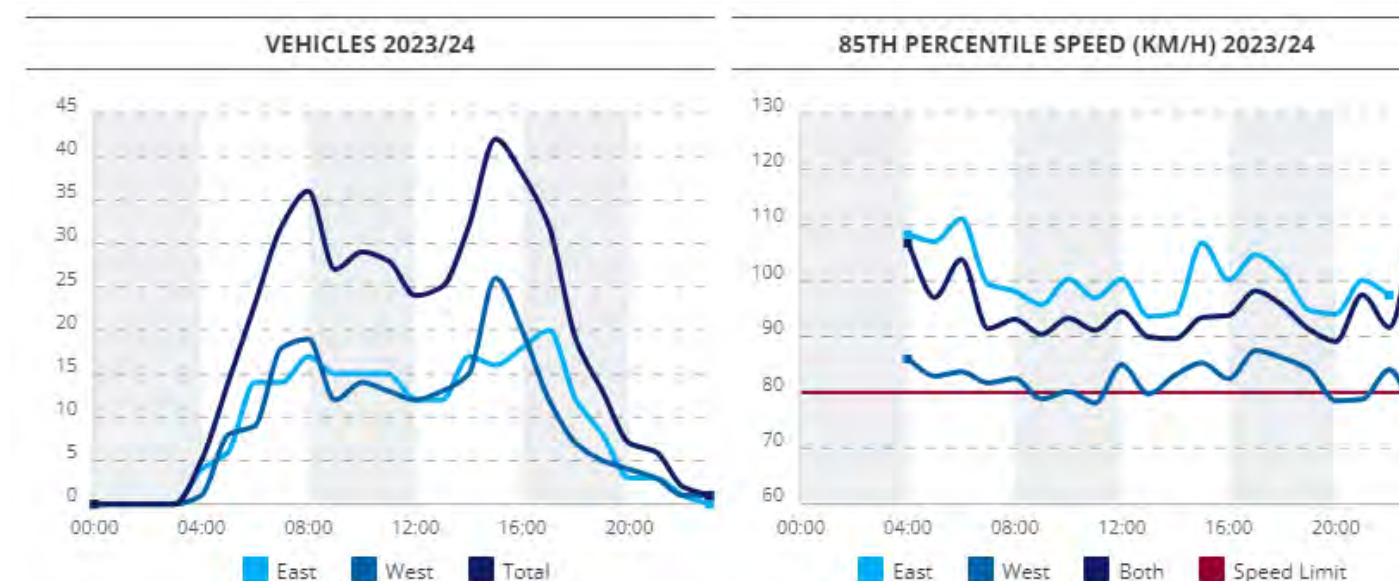


Figure 20 Dardanup Road West weekday traffic count and travel speeds (source: Main Roads WA)

### 5.2 Intersection Sight Distance

The roads immediately surrounding the development site have a posted speed limit of 80 km/h. For this speed, Main Roads WA require a minimum Safe Intersection Sight Distance (SISD) of 192m (with a 2.5 second reaction time) and an absolute minimum of 181m (with a 2.0 second reaction time).

The flat terrain and horizontal alignment of Garvey Road and Dardanup Road West ensure adequate sight distance is available at the existing intersections (of Dardanup Road West with Morellini Way and Holland Loop) and along the Garvey Road frontage.

### 5.3 Road Crashes

In the five year period ending December 31 2023, there were no reported crashes along the Garvey Road frontage or along Dardanup Road West, Morellini Way, Holland Loop or Ayrshire Road in the vicinity of the development site.

The location of reported crashes which did occur in the five year period are shown in Figure 21.



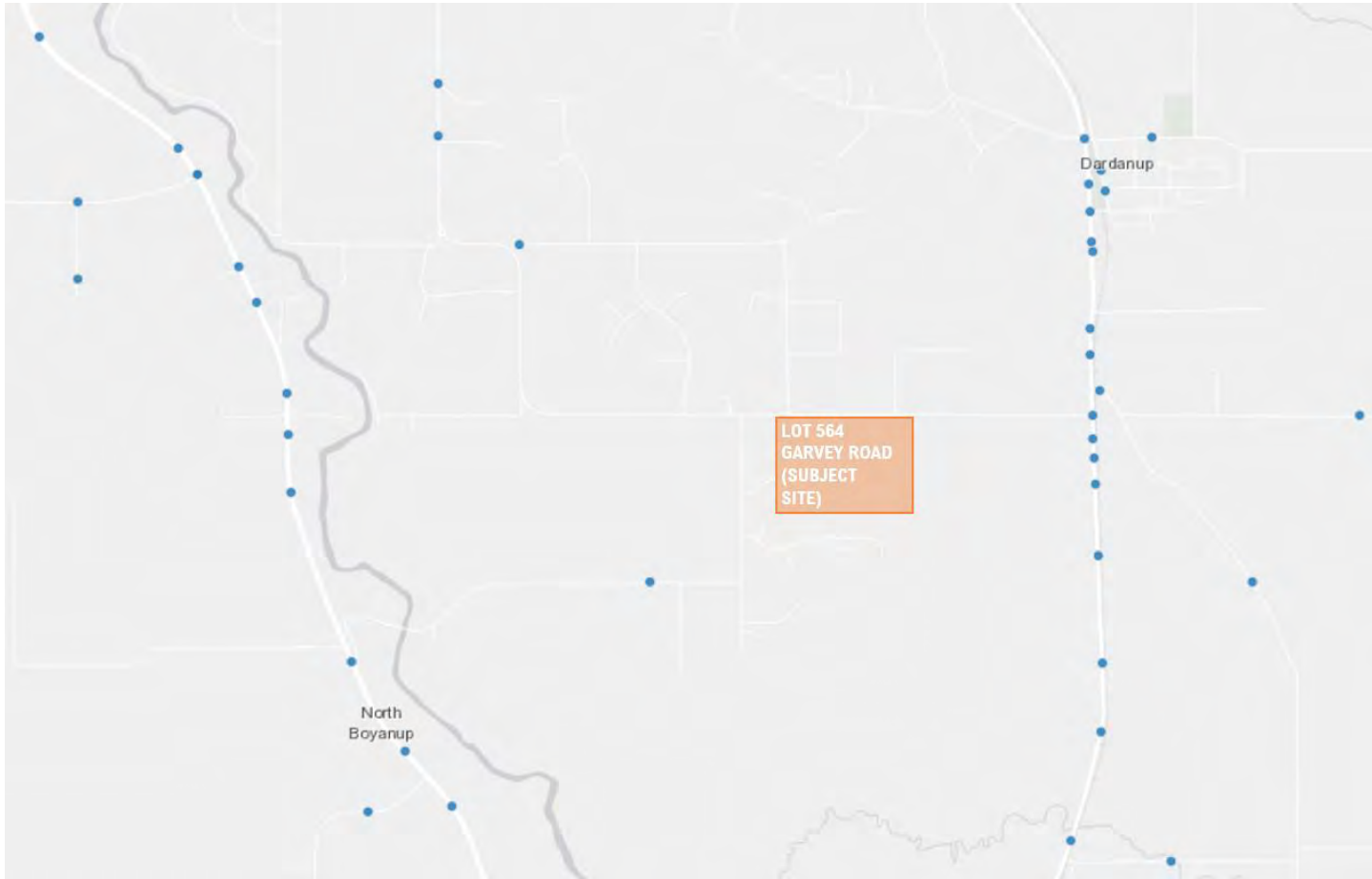


Figure 21 Location of five year reported crashes in vicinity of Lot 564 Garvey Road (source: Main Roads WA)



6. ANALYSIS OF TRANSPORT NETWORKS

6.1 Concept Plan Potential Land Uses

The proposed Concept Plan for Lot 564 Garvey Road includes 20 rural residential lots ranging from 1.008 ha to 3.970 ha, with an average lot size of 1.977 ha.

Based on a total site area of 40.523 ha, the maximum potential rural residential yield is 40 lots.

6.2 Trip Generation

The WAPC’s Transport Impact Assessment Guidelines – Volume 5 Technical Guidance (2016) provides peak hour trip rates as shown in Table 1. The trip rates are based on the Perth and Regions Travel Surveys (PARTS) data averaged over the range of dwelling types/commercial land uses across the survey area.

Table 1 – Typical vehicle trip rates (source: WAPC Transport Impact Assessment Guidelines – Volume 5 Technical Guidance, 2016)

Land Use	Unit	AM Peak Hour Trip Rate			PM Peak Hour Trip Rate			Effective Daily Rate
		IN	OUT	TOTAL	IN	OUT	TOTAL	TOTAL
Residential	Dwellings	0.2	0.6	0.8	0.5	0.3	0.8	8.0

The WAPC trip rates represent an average of the entire Perth and Peel Region survey area. These trip rates reflect sites with high levels of walk trips, cycling trips and public transport trips – as well as sites with high levels of private vehicle trips and limited access to public transport services or destinations within a walking/cycling catchment.

To determine if the WAPC residential trip rates are appropriate to be applied to a rural residential setting, Flyt have reviewed traffic volumes available on the Main Roads WA Traffic Maps website for locations across the South West. The townsite of Gracetown has approximately 150 properties and is accessed via a single road connection – Cowaramup Bay Road from Caves Road.

The Traffic Maps website includes two traffic count sites along the access road to the Gracetown townsite, with traffic data available from 2017-18 – which is pre-COVID-19 pandemic impacts on travel movements and travel behaviour. The annualised traffic data for the two count locations indicates that the properties in the Gracetown townsite have a daily vehicle trip rate of between 7.3-8.0 vehicle trips per dwelling. This vehicle trip rate is consistent with the WAPC daily vehicle trip rate of 8.0 vehicle trips per dwelling.

The proposed development has a maximum potential rural residential yield of 40 lots, with the Concept Plan proposing only 20 lots. Whilst the 20 rural residential lots that could be provided across the subject site are of a range of sizes – it is assumed for the purposes of this assessment that properties developed across the site would each generate a similar number of daily vehicle trips and therefore a uniform trip rate has been applied. For the purposes of this assessment a

daily vehicle trip rate of 8 trips per lot has been assumed for the site – this is in-line with the 8 vehicle trips per dwelling assumed in the WAPC Guidance and reflective of the Gracetown townsite’s vehicle trip rate of between 7-8 vehicle trips per dwelling.

Table 2 shows the estimated total traffic generated by the proposed rural residential land use across the subject site.

Table 2 – Estimated total traffic generated by the proposed rural residential land use

Land Use	Potential Development	AM Peak Hour Traffic			PM Peak Hour Traffic			Effective Daily Traffic
		IN	OUT	TOTAL	IN	OUT	TOTAL	TOTAL
Residential	20 dwellings	4	12	16	10	6	16	160

As outlined in Section 4.3, lots fronting Garvey Road are estimated to account for 50% of all daily traffic movements generated by the site. This amounts to 8 peak hour trips or approximately 80 vpd.

Lots accessed from the existing Morellini Way are estimated to account for 10% of all daily traffic movements generated by the site. This amounts to 2 peak hour trips or approximately 16 vpd.

Lots access via the proposed new subdivision road are estimated to account for 40% for all daily traffic movements generated by the site. This amounts to 6 peak hour trips or approximately 64 vpd. Of these trips the following distribution is expected:

- To/from Garvey Road – 4 peak hour trips or approximately 48 vpd
- To/from Ayrshire Road – 2 peak hour trips or approximately 16 vpd

6.3 Trip Distribution

The distribution of trips to and from the development site has been estimated based on likely trip origins/ destination (for example Bunbury and its surrounding suburb and the Dardanup townsite), as follows:

- Dardanup Road west (for trips to/from South Western Highway) 10% (16 vpd)
- Garvey Road west (for trips to/from Bunbury Outer Ring Road) 45% (72 vpd)
- Garvey Road east (for trips to/from Boyanup-Picton Road) 45% (72 vpd)

The forecast additional daily traffic resulting from the Lot 564 Garvey Road Concept Plan is outlined in the turning movement diagram shown in Figure 22.



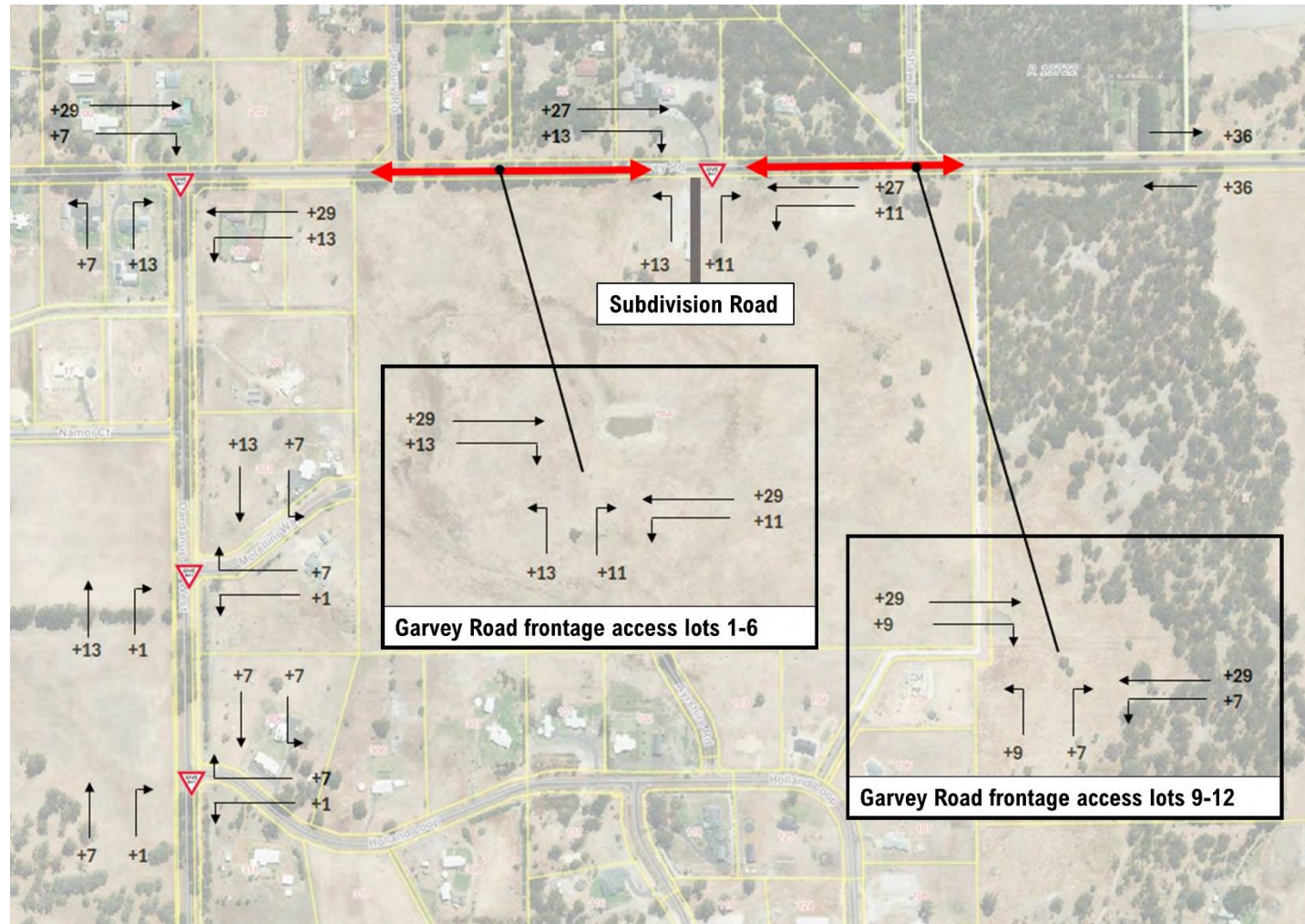


Figure 22 Lot 564 Garvey Road forecast additional daily traffic volumes

## 6.4 Summary

The forecast traffic volumes resulting from the proposed rezoning and Concept Plan development of Lot 564 Garvey Road are very low, and no road or intersection upgrades are required.

However, with the proposed intensification of frontage access along Garvey Road, and Main Roads traffic count information indicating a potential issue with vehicles exceeding the existing 80 km/h speed limit, the Shire of Dardanup should consider measures to modify the road environment to encourage adherence to the speed limit.



