



APPENDICES

PART 2

ORDINARY COUNCIL MEETING

To Be Held

Wednesday, 29 July 2020
Commencing at 5.00pm

At

Shire of Dardanup
ADMINISTRATION CENTRE EATON
1 Council Drive - EATON

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~ Large Print
~ Electronic Format [disk or emailed]
Upon request.



Job Ref: 9013
5 September 2019

Chief Executive Officer
Shire of Dardanup
PO Box 7016
Eaton WA 6232

Dear Sir/Madam

Roadhouse Development Application
Lot 603 (No. 96) Martin-Pelusey Road, Picton East

Rowe Group acts on behalf Harris Road Pty Ltd (the 'Client') the registered proprietors of Lot 603 (No. 96) Martin-Pelusey Road, Picton East (the 'subject site'). We have been instructed to prepare a Development Application for a Roadhouse at the subject site.

To assist in processing this application, please find enclosed the following:

- A signed Development Application Form;
- A CD containing an electronic version of this application;
- Certificate of Title (refer Attachment 1);
- Clause 53 Bunbury Region Scheme Certificate (refer Attachment 2);
- Architectural Drawings (refer Attachment 3);
- Service Station Site (refer Attachment 4); and
- Martin-Pelusey Road Design (refer to Attachment 5).

Location and Site Detail

The subject site is located approximately 12 kilometres to the southeast of the Bunbury CBD. The site details are included within the table below.

LOT	DEPOSITED PLAN	LANDOWNER	VOLUME / FOLIO	AREA (HA)
Lot 603 Columbus Dr	246179	Harris Road Pty Ltd	2044 / 266	39.242

Refer Attachment 1 – Certificate of Title.

Level 3
369 Newcastle Street
Northbridge 6003
Western Australia

p: 08 9221 1991
f: 08 9221 1919
info@rowegroup.com.au
rowegroup.com.au



Existing Improvements

The subject site is currently used for general farming and grazing purposes. In accordance with the Department of Fire and Emergency Services ('DFES') Map of Bush Fire Prone Areas, portions of the subject site are identified as "bushfire prone". It should be noted that clearing of vegetation has recently occurred in the preparation of additional grazing and also coincides with the preparation of the proposed development.

Additionally, a bushfire management plan is currently being compiled for the entirety of the subject site as part of the proposed Structure Plan. Any interim measures to the management of bushfire setbacks to the proposed development can occur within the site, pending broader management plans of the proposed Structure Plan over the subject site and surrounding land.

Proposed Development

The proposed Roadhouse is located at the south eastern portion of the subject site and will contain a development area of approximately 8,172m². The proposed development is to contain the following:

- Retail building with a floor space of 265m²;
- Retail car canopy;
- Retail truck canopy;
- Tank farm;
- 15 car parking bays; and
- Pylon ID sign and promo sign.

Refer Attachment 3 - Architectural Drawings and Attachment 4 - Service Station Site.

Town Planning Considerations

Greater Bunbury Region Scheme

Under the provisions of the *Greater Bunbury Region Scheme*, the subject site is predominantly zoned 'Industrial Deferred', with a portion of the eastern edge reserved for Primary Regional Roads ('PRR'). The reserve varies between 98m and 82m in width and was incorporated in the GBRS to accommodate a now superseded alignment for the Bunbury Outer Ring Road ('BORR').

Shire of Dardanup Town Planning Scheme No. 3

Under the provisions of the *Shire of Dardanup Town Planning Scheme No. 3* ('TPS 3') the subject site is zoned 'General Farming' along with the above mentioned portion of land reserved for PRR associated with the former BORR alignment.



Land Use Permissibility

As the majority of the proposed development is within the 'General Farming' Zone, it is considered appropriate to be assessed under the provisions of the 'General Farming' Zone.

The proposed development is consistent with a 'Roadhouse' land use as defined in TPS 3. A 'Roadhouse' is defined under TPS 3 as follows:

Means a land and a building or buildings with a basic use as a service station and a supplementary use as a restaurant providing minor shop facilities;

Furthermore, a 'Service Station' is defined under TPS 3 as follows:

service station means premises used for –

- (a) the retail sale of petroleum products, motor vehicle accessories and goods of an incidental/convenience retail nature; and*
- (b) the carrying out of greasing, tyre repairs and minor mechanical repairs to motor vehicles, but does not include premises used for a transport depot, panel beating, spray painting, major repairs or wrecking;*

The proposed development is considered consistent with the definition of a 'Service Station' as it includes the sale of petroleum products and allows for the carrying out of greasing, tyre repairs and minor vehicle repairs within the development area (at either dedicated air and water parking or in the main parking). The proposed development is consistent with the land use definition of a 'Roadhouse' as it provides a basic use as a service station and a supplementary use as a minor restaurant.

A 'Roadhouse' use is classified as a 'D' use within the 'General Farming' Zone in accordance with the Zoning Table. Furthermore, TPS 3 states that 'Non Urban Zones shall have regard to the following:

- a) the need to protect the economic viability of the rural land use generally;*
- b) the need to preserve the rural character and a rural appearance of the area;*
- c) the need to ensure that the existing standard of roads, water and electricity supply and other services is sufficient for the additional demands that the proposed development would create; and*
- d) the need to ensure that in general all buildings are at least twenty metres from any lot boundary adjoining a street and the Council may require an additional setback in order to preserve the rural character of the area.*

The proposed development is consistent with the above mentioned objectives which is discussed further within the Design Elements section of this justification letter.



It should be noted that documents for the 'Lifting of Industrial Deferment' and a Structure Plan are being prepared for lodgement over Lots 103,110 and 603. The 'Lifting' request will also seek concurrent rezoning of the land and surrounding lots to 'General Industry' Zone under TPS3. Importantly, a 'Roadhouse' use is also classified as a 'D' use within the 'General Industry' Zone, with reduced setback requirements.

The proposed use is capable of being approved under the current zoning of the land, and importantly, does not comprise the future planning of the land or contemplated future use of the land for General Industry. In due course, the Roadhouse is fully intended to cater to the surrounding general industrial area.

Development Criteria

The following section addresses the design criteria for the 'General Farming' Zone contained within TPS 3.

Street Setbacks

In accordance with Appendix II of TPS 3, land uses within the 'General Farming' Zone require a minimum setback of 20 metres from the primary street, secondary street, rear boundary and side boundary. The proposed development is setback approximately 50 metres to the primary street being Martin-Pelusey Road. Given the size of the existing lot, the development is set back well in excess of the required distance to the rear and side boundary. The proposed development is therefore consistent with the setback requirements within the 'General Farming' Zone.

Car Parking

TPS 3 does not identify parking requirements for a 'Roadhouse' use. While the proposal is consistent with a 'Service Station' which is identified within the table below, the rate of parking provision for a service station reflects a greater retail service offering in an urban location. The proposed Roadhouse will cater to primarily surrounding industrial traffic, in particular freight traffic, with some passing light vehicle trade. Accordingly, 29 bays are proposed, comprising a mixture of service bays at the bowser, and allocated parking bays.

LAND USE	PARKING REQUIREMENT (AS PER APPENDIX IIA - CAR PARKING OF TPS3)	PROPOSED SERVICE BAYS	CAR PARKING BAYS REQUIRED	CAR PARKING BAYS PROPOSED
'Service Station' - (for comparison purposes only)	4 spaces for every service bay	14	56	15



In this instance, the reference to service station parking requirements is for comparison only. The parking proposed to be supplied on site is considered an appropriate mix of bowser bays for retail light vehicles, truck fuel sales, and general customer parking.

Development Control Policy 1.10 – Freeway Service Centres and Roadhouses, Including Signage

The Western Australian Planning Commission (WAPC) has adopted *Development Control Policy 1.10 – Freeway Service Centres and Roadhouses, Including Signage* ('DCP 1.10'). The policy applies to freeway service centres and roadhouses, including associated signage, located on state roads.

In accordance with DCP 1.10 a Roadhouse is defined as the following:

Roadhouse means premises that has direct access to a State road other than a freeway and which provides the services or facilities provided by a freeway service centre and may provide any of the following additional facilities or services-

- (a) a full range of automotive repair services;*
- (b) wrecking, panel beating and spray painting services;*
- (c) transport depot facilities;*
- (d) short-term accommodation for guests (including motel and caravan sites, generally in remote locations);*
- (e) facilities for being a muster point in response to accidents, natural disasters and other emergencies.*

As the proposed development is located along a proposed PRR (being Martin-Pelusey Road). Martin-Pelusey Road will remain a significant north-south connection link for movement between Southern Western Highway and the proposed BORR. The proposed Roadhouse will service this key route, and additionally it is located between the Picton Industrial Park and the Waterloo Industrial District Structure Plan area and will provide a service to these industrial areas through truck and car fuelling areas, and retail building.

The proposed development is considered generally consistent with the design, signage and siting measures, which is discussed in greater detail in the Design Elements section of this justification letter.

Refer to Attachment 5 – Martin-Pelusey Road Design.

Design Elements

The following design elements discussed below can be referred to within Attachment 3 – Architectural Drawings.

Building Height and Scale

The proposed development incorporates the following building heights:



- Retail building (5.15 metres);
- Retail car canopy (5.5 metres); and
- Retail truck canopy (6.5 metres).

The proposed development incorporates building heights which are consistent with that of a two (2) storey building. The fuel tank farm, serving both light vehicle and truck canopy areas is centrally located.

Building Design and Materials

The proposed retail building incorporates building design features and materials which reflect the nature of the use. The proposal has been design to be visible to passing traffic while being safely site side on to Martin-Pelusey Road.

A significant portion of the retail building façade contains glazing, allowing for passive surveillance into the car parking area, retail vehicle canopy and primary road. Blade awnings with a black and white pattern are also located along the façade of the retail building, providing shelter for the seating area, further softening the scale of the proposed building.

Access and Egress

The proposed development contains a separate left in, heavy vehicle ingress crossing (14.245 metres wide) which 'loops' into the retail truck canopy and towards the heavy vehicle egress crossing (22.375 metres wide). Swept paths have been provided which demonstrate RAV 4 and RAV 7 movements within the proposed development.

The proposed development also contains a full movement crossing ('light vehicle crossing' - 8 metres wide) which allows vehicles to access the retail car canopy and car parking bays.

The entrance to the proposed retail building is clearly identified at the front of the store and immediately adjoining car parking bays. Pedestrian access is also consistent with Australian Standard AS1428.1 (Disabled and Limited Mobility).

Signage

The proposed development includes a 2.3 metre promotion sign which is located within northern portion of the development. This sign contains company promotions relating to the retail building.

The proposed development also includes a 12 metre pylon identification sign with fuel price lines within the north eastern portion of the development. This sign is a necessary form of signage for all Roadhouse developments as it provides customers with fuel pricing and locational details. A sign of this nature is appropriate at the subject site and should be supported.



Lighting

Sufficient lighting is provided within the surrounding car parking area and retail building. Furthermore, adequate lighting is proposed under the retail truck canopy and retail car canopy.

Stormwater and Drainage

All stormwater runoff from the truck and light vehicle canopy areas will be captured in the purceptor located within the designated landscaping area. The purceptor will filter all fuel elements from stormwater for safe disposal with treated clean water able to be discharged into soak wells and /or landscaping. Roof runoff from fuel canopy areas or the retail building will be discharged to soak wells and landscaped drainage areas.

Landscaping

The proposed development contains landscaping to the front and sides of the development. Further landscaping is to be provided within the centre of the site providing separation from vehicles using the retail truck canopy or the retail car canopy.

The following planting list will be used for the proposed landscaping area.

- › Trees
 - Agonis flexuosa
- › Shrubs
 - Melaleuca huegelii
 - Hakea oleifolia
 - Acacia cyclops
 - Acacia stenoptera
 - Hovea trisperma
- › Strap Like
 - Anigozanthos manglesii

The above-mentioned planting list is considered to represent the local landscape setting, while also providing adequate sightlines for pedestrians and vehicles.

Summary

This letter has been prepared in support of the development of a Roadhouse at the subject site, being portion of Lot 603 (No. 96) Martin-Pelusey Road, Picton East.

The proposed Roadhouse is considered and appropriate and justified at the subject site for the following reasons outlined herein. The chosen location will cater to existing traffic on Martin-Pelusey Road and ultimately serve an important function centrally within the wider Picton / Waterloo Industrial Areas within the new Bunbury Outer Ring Road.



Should you require any further information or clarification in relation to this matter, please contact either Blair Stroud on 9221 1991 or the undersigned.

Yours faithfully,

A handwritten signature in black ink, appearing to read 'Rod Dixon'.

Rod Dixon

Rowe Group



Attachment One

Certificate of Title



Attachment Two

Clause 53 Bunbury Region Scheme Certificate



Enquiries: Trevor Servaas (08) 6551 9110
Our Ref: 53 / 59683025
Your Ref: 9013

GREG ROWE PTY LTD
LEVEL 3, 369 NEWCASTLE STREET
NORTHBRIDGE
6007 WA

Dear Sir/Madam

**CERTIFICATE UNDER CLAUSE 53 OF THE GREATER BUNBURY REGION SCHEME
ISSUED BY THE WESTERN AUSTRALIAN PLANNING COMMISSION**

In reply to your request, please find enclosed
Certificate Number: 59683025

It is advised that the enclosed Certificate has been prepared to conform with the current Statutory requirements (as at the date of signature) of the Greater Bunbury Region Scheme

Yours faithfully,

A handwritten signature in black ink, appearing to read "S Fagan".

Ms Sam Fagan
Secretary
Western Australian Planning Commission

5 August 2019



wa.gov.au

Postal address: Locked Bag 2506 Perth WA Street address: 140 William Street Perth WA 6000
Tel: (08) 6551 8002 Fax: (08) 6551 9001 TTY: 6551 9007 Infoline: 1800 626 477
info@dph.wa.gov.au www.dph.wa.gov.au
ABN 35 482 341 493



Scheme Certificate

In accordance with clause 53 of the

Greater Bunbury Region Scheme the following information relates to:

Certificate:
59683025

Receipt:
None

Date:
6/08/2019

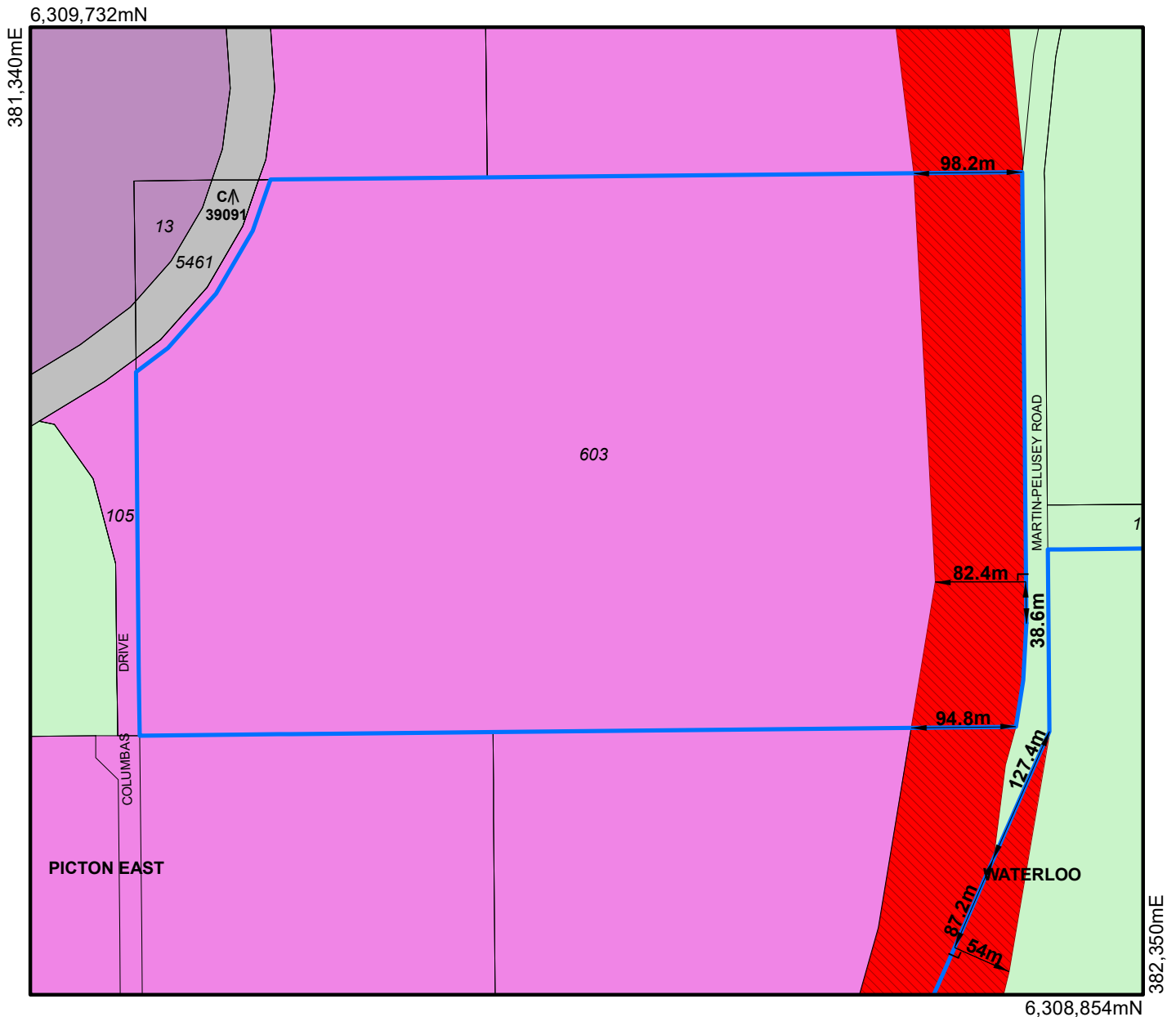
Location: Martin-Pelusey Rd, Picton East

Certificate of Title: Vol: 2044 Folio:266

Deposited Plan: 246179

Legend for reserved land and zones

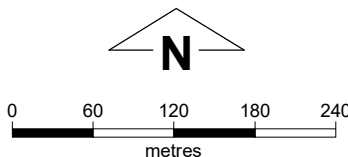
- Environmental conditions
- Industrial deferred
- Railways
- Industrial
- Primary regional roads
- Rural



This certificate relates only to the provisions of the:
Greater Bunbury Region Scheme
and does not claim to indicate the land use allocation under any local government provision.

Produced by Data Analytics,
Department of Planning, Lands and Heritage, Perth WA.

Base information supplied by:
Western Australian Land Information Authority SLIP 1096-2018-1



Coordinates based on MGA Zone 50 (GDA 94)
All dimensions are in metres
Subject to survey

Ms Sam Fagan
Secretary
Western Australian Planning Commission



MINISTER FOR THE ENVIRONMENT; SCIENCE

STATEMENT THAT A SCHEME MAY BE IMPLEMENTED
(PURSUANT TO THE PROVISIONS OF DIVISION 3 OF PART IV OF THE
ENVIRONMENTAL PROTECTION ACT 1986)

GREATER BUNBURY REGION SCHEME

Scheme Purpose: To guide and regulate the use and development of land and to make provisions for regional infrastructure and regional open space within the Greater Bunbury Region.

Responsible Authority: Western Australian Planning Commission

Responsible Authority Address: 469 Wellington Street, PERTH WA 6000

Assessment Number: 1048

Report of the Environmental Protection Authority: Bulletin 1108

Subject to the following conditions, there is no known environmental reason why the Greater Bunbury Region Scheme to which the above report of the Environmental Protection Authority relates should not be implemented:

1 Additional Land to be Reserved

1-1 All or portions of the following sites shall be reserved for conservation, in accordance with the requirements set out in Attachment 1 of the Minister for the Environment's "Statement that a Scheme may be Implemented" No. 000697 published on (date):

- 1) Reserve 35061 Paris Road, Australind - Decommissioned Wastewater Treatment Plant
- 2) Reserve 31012 Harewoods Road, Dalyellup
- 3) Foreshore adjoining Port Installations Reserve
- 4) Twin Rivers - Pt Lot 211 Barnes Avenue, Australind
- 5) College Grove - Lot 1000 Bussell Highway
- 6) Pt Loc 632 Parade Road, Glen Padden
- 7) Reserve 670 North Boyanup Road, Davenport
- 8) Picton Waters - Environmental Protection Policy Lake on portion of Lots 40-44 Jeffery Road, Picton
- 9) Pt Lot 1 North Boyanup Road, Davenport
- 10) Lot 317 Harewoods Road, Dalyellup
- 11) Lot 1, Dalyellup Boulevard, Dalyellup

Published on

31 OCT 2005

2 Realignment of Primary Regional Roads Reserves

- 2-1 Portions of the following Primary Regional Roads Reserves shall be realigned, in accordance with the requirements set out in Attachment 1 of the Minister for the Environment's "Statement that a Scheme may be Implemented" No. 000697 published on (date):
- 1) Port Access Road
 - 2) Bunbury Outer Ring Road

CONDITIONS TO BE INCORPORATED INTO THE SCHEME BY INSERTION OF PROVISIONS IN SCHEME TEXT

3 Management Plans

- 3-1 The following Environmental Management Plans may be required in accordance with the specifications set out in Attachment 1 in the Minister for the Environment's "Statement that a Scheme may be Implemented" No. 000697 published on (insert date), and shall be subsequently implemented in accordance with the provisions of the Management Plans, to the satisfaction of the Western Australian Planning Commission:
- 1) Environmental Management Plans for schemes, subdivisions and developments which impact on Regional Open Space in the scheme, Crown conservation or nature reserves, a National Park or bushland, waterways, wetlands or other land that may be part of an ecological linkage;
 - 2) Environmental Management Plans for industrial development within the Kemerton Industrial Area and Special Control Area No. 2;
 - 3) Drainage, Nutrient and Water Management Plans in areas where the Average Maximum Groundwater Level is less than 1.2 metres below the natural ground surface or where any proposed off-site drainage could lead to degradation of wetlands or waterways; and
 - 4) Acid Sulfate Soil Management Plans where the presence of acid sulphate soils is confirmed or there is likely to be a significant risk of disturbing acid sulphate soils.

4 Biological Survey

- 4-1 As part of a scheme amendment or application to subdivide or develop land which has the potential to impact on regionally significant native remnant vegetation or native fauna, the Western Australian Planning Commission or local government, as the case requires, may require a biological survey, including a search for Declared Rare Flora and Fauna, Priority Flora, Threatened Flora Communities and Threatened Fauna, to be undertaken.

(Appendix ORD: 12.4A)

The biological survey shall be undertaken to the satisfaction of the Western Australian Planning Commission or local government, as the case requires, having due regard for advice from relevant government agencies, and shall be taken into account when considering the rezoning and subsequent subdivision and development applications.

5 Provision of Environmental Offsets

- 5-1 Prior to construction of the Port Access Road and Bunbury Outer Ring Road, an environmental offset strategy shall be prepared to mitigate unavoidable impacts on wetlands and native vegetation associated with the Port Access Road and Bunbury Outer Ring Road to the satisfaction of the Western Australian Planning Commission on advice of the Environmental Protection Authority. With respect to the Port Access Road, the strategy shall include a foreshore management plan for the Ferguson River in the vicinity of the road. With respect to the Bunbury Outer Ring Road, the strategy shall include rehabilitation of the strip of land adjacent to the road in the vicinity of Lot 15 North Boyanup Road (South Western Highway) and design of the intersection with the Australind Bypass to minimize impacts on environmental values of the area.

Dr Judy Edwards MLA
MINISTER FOR THE ENVIRONMENT; SCIENCE

31 OCT 2005

STATEMENT THAT A SCHEME MAY BE IMPLEMENTED – GREATER BUNBURY REGION SCHEME

SPECIFICATIONS FOR ENVIRONMENTAL MANAGEMENT PLANS, ADDITIONAL LAND TO BE RESERVED AND REALIGNMENT OF PRIMARY REGIONAL ROAD RESERVES

1 Environmental Management Plans for Schemes, Subdivisions and Developments

1-1 Prior to amending local town planning schemes, or finally approving subdivisions or developments (whichever is sooner), the Western Australian Planning Commission or local government, as the case requires, may require an Environmental Management Plan to be prepared and implemented to achieve the objective of managing the potential impacts of the proposed subdivision or development on the following:

- 1) land which is reserved as Regional Open Space in the Scheme;
- 2) a Crown conservation or nature reserve;
- 3) a National Park; or
- 4) bushland, waterway, wetland or land that may be part of an ecological linkage.

The Environmental Management Plan shall include:

- i) a description of existing environmental values, and the identification of the environmental outcome to be achieved through the implementation of the Plan;
- ii) clear delineation of boundaries of significant areas to be protected;
- iii) fire management;
- iv) drainage and nutrient management;
- v) management of access and rehabilitation;
- vi) vegetation and/or wetland mitigation strategies;
- vii) a program for implementation;
- viii) allocation of responsibilities and identification of timing and duration of implementation;
- ix) provision for routine monitoring of environmental values; and
- x) provision of details of contingency plans in the event that the monitoring surveys indicate that the development is having or has had an adverse impact upon environmental values.

(Appendix ORD: 12.4A)

- 1-2 Environmental Management Plans required by condition 1-1 shall be prepared to the satisfaction of the Western Australian Planning Commission or local government, as the case requires, having due regard for advice from relevant government agencies and shall be implemented in accordance with a program defined in the Environmental Management Plan.

2 Environmental Management Plans for Industrial Development within the Kemerton Industrial Area and Special Control Area No. 2

- 2-1 Prior to approving subdivision or development within the Kemerton Industrial Area and the Special Control Area No. 2, the Western Australian Planning Commission or local government, as the case requires, may require an Environmental Management Plan to be prepared and implemented to achieve the objective of managing the potential impacts of the proposed development.

The Environmental Management Plan shall include:

- 1) a description of existing environmental values (including vegetation, fauna and wetlands) and the identification of the environmental outcome to be achieved through the implementation of this Plan;
 - 2) management of potential impacts on visual amenity;
 - 3) clear delineation of significant areas to be protected;
 - 4) a program for implementation;
 - 5) allocation of responsibilities and identification of timing and duration of implementation;
 - 6) provision for routine monitoring of environmental values; and
 - 7) provision of details of contingency plans in the event that the monitoring surveys indicate that the subdivision or development is having or has had an adverse impact upon environmental values.
- 2-2 An Environmental Management Plan required by condition 2-1 shall be prepared to the satisfaction of the Western Australian Planning Commission or local government, as the case requires, having due regard for advice from relevant government agencies and shall be implemented in accordance with a program defined in the Environmental Management Plan.

3 Drainage, Nutrient and Water Management Plans

- 3-1 Prior to amending local town planning schemes, or finally approving subdivision or developments (including those for intensive horticulture), whichever is sooner, in areas where the Average Maximum Groundwater Level is less than 1.2 metres below the natural ground surface, or where any proposed off-site drainage could lead to degradation of wetlands or waterways, the Western Australian Planning Commission or local government, as the case requires, may require a Drainage, Nutrient and Water Management Plan to be prepared and implemented.

(Appendix ORD: 12.4A)

- 3-2 A Drainage, Nutrient and Water Management Plan, if required, shall ensure that there is no net increase in nitrogen export to the Leschenault Estuary as a result of development within the Greater Bunbury Region.
- 3-3 The Drainage, Nutrient and Water Management Plan shall be prepared to the satisfaction of the Western Australian Planning Commission or local government, as the case requires, having due regard for advice from relevant government agencies, and shall be implemented in accordance with a program defined in the Drainage, Nutrient and Water Management Plan.

4 Acid Sulfate Soil Management Plans

- 4-1 Prior to amending local town planning schemes, or finally approving subdivisions or development, the Western Australian Planning Commission or local government, as the case requires, may require a Preliminary Acid Sulfate Soils Assessment to be prepared where there is likely to be a significant risk of disturbing acid sulfate soils.
- 4-2 Where the presence of acid sulfate soils is confirmed, an Acid Sulphate Soil Management Plan shall be prepared to the satisfaction of the Western Australian Planning Commission or local government, as the case requires, having due regard for advice from relevant government agencies and implemented in accordance with a program defined in the Acid Sulphate Soil Management Plan.

5 Additional Land to be Reserved

5-1 Reserve 35061 Paris Road, Australind - Decommissioned Wastewater Treatment Plant

Land shall be reserved for conservation purposes to protect the integrity, function and environmental value of the bushland on the northern and western portions of Reserve 35061 Paris Road, Australind, to the requirements of the Western Australian Planning Commission on advice of the Environmental Protection Authority, and shall only be used for conservation and complementary purposes.

5-2 Reserve 31012 Harewoods Road, Dalyellup

Land shall be reserved for conservation purposes to protect the integrity, function and environmental value of the bushland on the northern portion of Reserve 31012 Harewoods Road, Dalyellup to the requirements of the Western Australian Planning Commission on advice of the Environmental Protection Authority and shall only be used for conservation, landscape and recreational purposes.

5-3 Foreshore adjoining Port Installations Reserve

Land shall be reserved for conservation purposes to protect the integrity, function and environmental value of the foreshore adjacent to the Port Installations Reserve to the requirements of the Western Australian Planning Commission on advice of the

(Appendix ORD: 12.4A)

Environmental Protection Authority and shall only be used for conservation and complementary purposes.

5-4 **Twin Rivers - Pt Lot 211 Barnes Avenue, Australind**

A portion of Pt Lot 211 Barnes Avenue as detailed in the Minister for the Environment's determination of appeals relating to the Environmental Protection Authority's report and recommendations on the Scheme, dated 30 November 2004, shall be reserved for conservation purposes to protect the integrity, function and environmental values of the land to the requirements of the Western Australian Planning Commission on advice of the Environmental Protection Authority, and shall only be used for conservation and complementary purposes.

5-5 **Picton Waters – Environmental Protection Policy Lake on Portion of Lots 40-44 Jeffery Road, Picton**

Land shall be reserved for conservation purposes to protect the integrity, function and environmental value of the wetland on Lots 40-44 Jeffery Road, Picton, to the requirements of the Western Australian Planning Commission on advice of the Environmental Protection Authority, and shall only be used for conservation and complementary purposes. The land requirements shall include an appropriate buffer to protect the core wetland.

5-6 **College Grove – Lot 1000 Bussell Highway**

A portion of Lot 1000, Bussell Highway, as detailed in the Minister for the Environment's determination of appeals relating to the Environmental Protection Authority's report and recommendations on the Scheme, dated 30 November 2004, shall be reserved for conservation purposes to protect the integrity, function and environmental values of the land to the requirements of the Western Australian Planning Commission on advice of the Environmental Protection Authority, and shall only be used for conservation and complementary purposes.

5-7 **Pt Loc 632 Parade Road, Glen Padden**

Land shall be reserved for conservation purposes to protect the integrity, function and environmental value of the bushland on Pt Loc 632 Parade Road, Glen Padden, to the requirements of the Western Australian Planning Commission on advice of the Environmental Protection Authority, and shall only be used for conservation and complementary purposes.

5-8 **Reserve 670 North Boyanup Road, Davenport**

Land shall be reserved for conservation purposes to protect the integrity, function and environmental value of the bushland on Reserve 670 North Boyanup Road, Davenport to the requirements of the Western Australian Planning Commission on advice of the Environmental Protection Authority, and shall only be used for conservation and complementary purposes.

5-9 **Pt Lot 1 North Boyanup Road, Davenport**

Land shall be reserved for conservation purposes to provide an appropriate buffer to the Preston River on Pt Lot 1, North Boyanup Road, Davenport to the requirements of the Western Australian Planning Commission on advice of the Environmental Protection Authority, and shall only be used for conservation and complementary purposes.

5-10 **Lot 317 Harewoods Road, Dalyellup**

Lot 317 Harewoods Road, Dalyellup shall be reserved for conservation purposes to protect the integrity, function and environmental value of the bushland on the land to the requirements of the Western Australian Planning Commission on advice of the Environmental Protection Authority, and shall only be used for conservation and complementary purposes.

5-11 **Lot 1 Dalyellup Boulevard, Dalyellup**

Lot 1 Dalyellup Boulevard, Dalyellup shall be reserved for conservation purposes to protect the integrity, function and environmental values of the land to the requirements of the Western Australian Planning Commission on advice of the Environmental Protection Authority and shall only be used for conservation and complementary purposes.

6 Realignment of Primary Regional Roads Reserves

6-1 Port Access Road

The Port Access Road Primary Regional Roads Reserve shall be realigned in accordance with the Minister for the Environment's determination of appeals relating to the Environmental Protection Authority's report and recommendations on the Scheme, dated 30 November 2004.

6-2 Bunbury Outer Ring Road

The Bunbury Outer Ring Road Primary Regional Roads Reserve shall be realigned in accordance with the Minister for the Environment's determination of appeals relating to the Environmental Protection Authority's report and recommendations on the Scheme, dated 30 November 2004.



Attachment Three

Architectural Drawings

GENERAL

- G001 DRAWING LIST
- G002 SCHEDULE FINISHES

ARCHITECTURAL

- A101 PLAN SITE EXISTING
- A102 PLAN SITE PROPOSED
- A103 PLAN BUILDING
- A104 PLAN CANOPY CAR
- A105 PLAN CANOPY TRUCK
- A201 ELEVATION BUILDING SHEET 1
- A202 ELEVATION BUILDING SHEET 2
- A203 ELEVATION CANOPY CAR SHEET 1
- A204 ELEVATION CANOPY CAR SHEET 2
- A205 ELEVATION CANOPY TRUCK SHEET 1
- A206 ELEVATION CANOPY TRUCK SHEET 2

TRAFFIC

- T101 PLAN SITE SWEEP PATHS

LANDSCAPING

- L101 PLAN SITE LANDSCAPING

SIGNAGE

- S101 PLAN SITE SIGNAGE
- S201 ELEVATION SIGNAGE

DEVELOPMENT
NOT FOR CONSTRUCTION



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IN WHOLE OR PART WITHOUT WRITTEN
CONSENT FROM PROJECT CONSULTANTS WA PTY LTD

PROJECT

EASTCOURT PROPERTY GROUP
PIGTON FUEL STATION
LOT 603 MARTIN PELUSEY ROAD
PIGTON EAST
1920-014

SHEET

DRAWING LIST

REV. DESCRIPTION

A Development Application

26.08.19

G001

NTS

A

EXTERIOR FINISHES SCHEDULE						
NO	DESCRIPTION	LOCATION	MANUFACTURER	CODE	COLOUR	NOTES
TIMBER LOOK CLADDING						
DW1	Decowood Aluminium Column	Front of Building	Decorative Imaging	Decowood Aluminium	Natural American Oak	150x150mm Aluminium extrusion with base bracket and colour matching end caps
DW2	Decowood Aluminium Cladding	Awning Fascia	Decorative Imaging	Decowood Aluminium	Natural American Oak	Cladding to fascia
DW3	Decowood Aluminium Battens	Fascia	Decorative Imaging	Decobatten	Natural American Oak	50x50mm with matching end caps
ALUMINIUM COMPOSITE CLADDING						
AB1	<i>Superseded</i>					
AB2	ACM	Signage / Fascia	Signage Contractor	326 Black	Black	
AB3	<i>Superseded</i>					
POWDERCOATING						
PC1	Black Powdercoat	Window Framing	Dulux	Duralloy - 19268	Black Satin	
TILE						
BR1	External Brick Facing Tile	Wall	Robertson's Building Products		Rustic Red & Rustic Apricot	50/50 Blend
T10	Mosaic Tiles	Wall	Royal Tiles	Matt Mosaic	White	45 x 75mm. Laid horizontally. Grout - Latcrete #88 Silver Shadow
MISCELLANEOUS						
AW1	Drop Awning	Between Front of Building Columns	Luxaflex	Alpha Straight Drop Awning	Black & White Stripes	Dickson C022 Black/White
PA1	<i>Superseded by PT8</i>					
PA2	Steel Sheet Cladding	Soffit Lining	Bluescope Lysaght	Easyclad 2PF 300	Powdercoat Paperbark Satin	
PT1	Sealed Off Shutter Concrete Wall	Front Concrete Wall - Below DW3 Fascia	Aftek	Fairing Coat Mortar	Natural	
PT2	Paint	Rear Wall & Concrete Fascia Behind DW3	Dulux		Black Acrylic	Exterior grade - Low Sheen, Low VOC
PT8	Paint	Front & Return Corner Concrete Walls	Dulux		Black	Low Sheen Weathershield with Anti-graffiti clear
PT9	Paint	Side Concrete Wall	Dulux	SN4G3	Endless Dusk	Low Sheen Weathershield. If panels require painting
PT10	Wall Cladding	Retrofit Building - Front & Return Corner	James Hardie	Scyon Axom 400mm - 404419	PT8 Top coat paint	Pre-primed exterior cladding. Elevation not to exceed 3m height. Sheet Size 3000x1200mm

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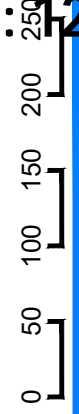
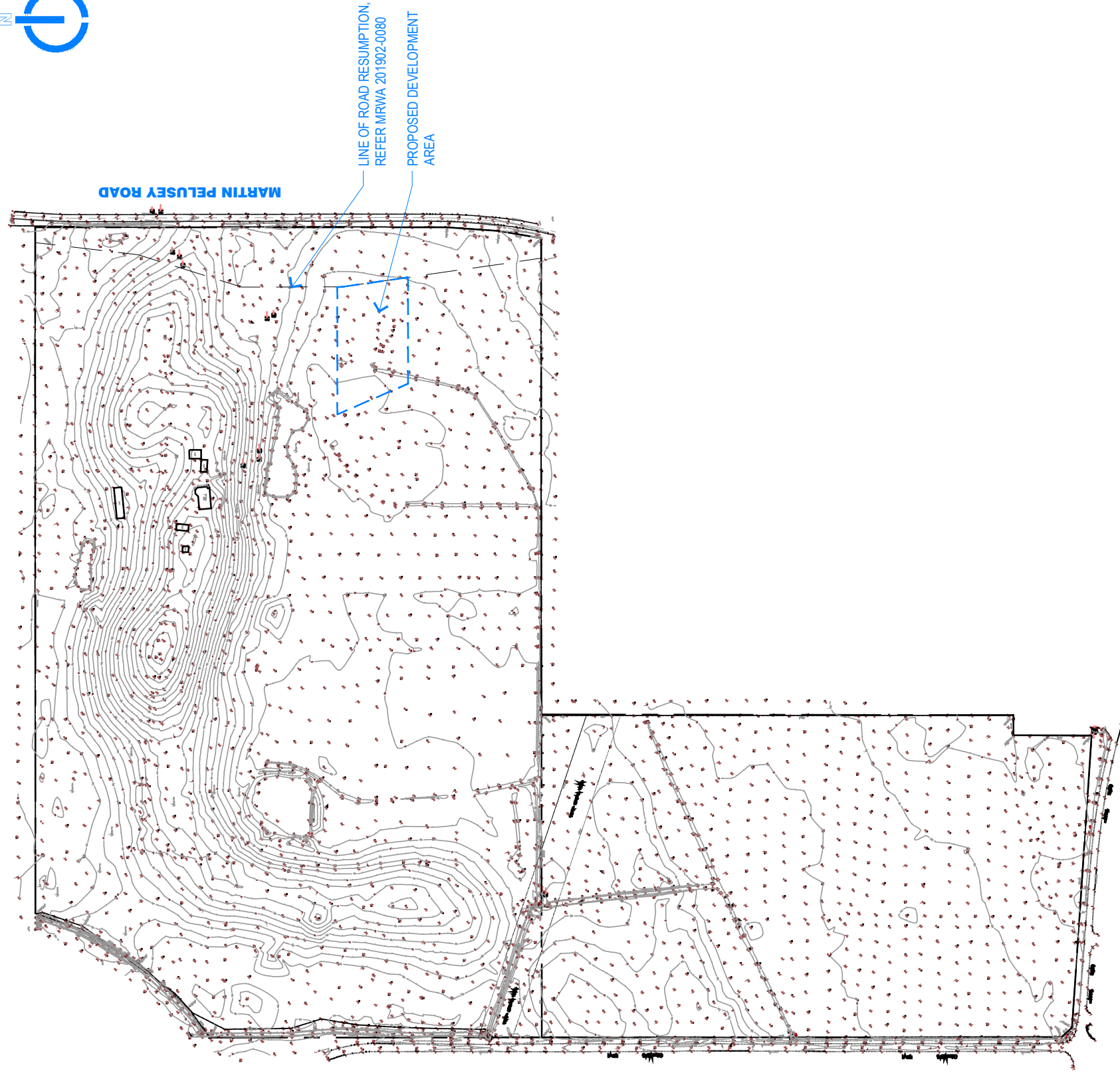
PROJECT
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PIGTON FUEL STATION
LOT 603 MARTIN PELUSEY ROAD
PIGTON EAST
1920-014

SHEET
SCHEDULE FINISHES

REV. DESCRIPTION
A Development Application

26.08.19

GOO02
NTS | A



27.08.19

A101

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REV.	DESCRIPTION
A	Development Application

SHEET

PLAN
SITE
EXISTING (SURVEY)

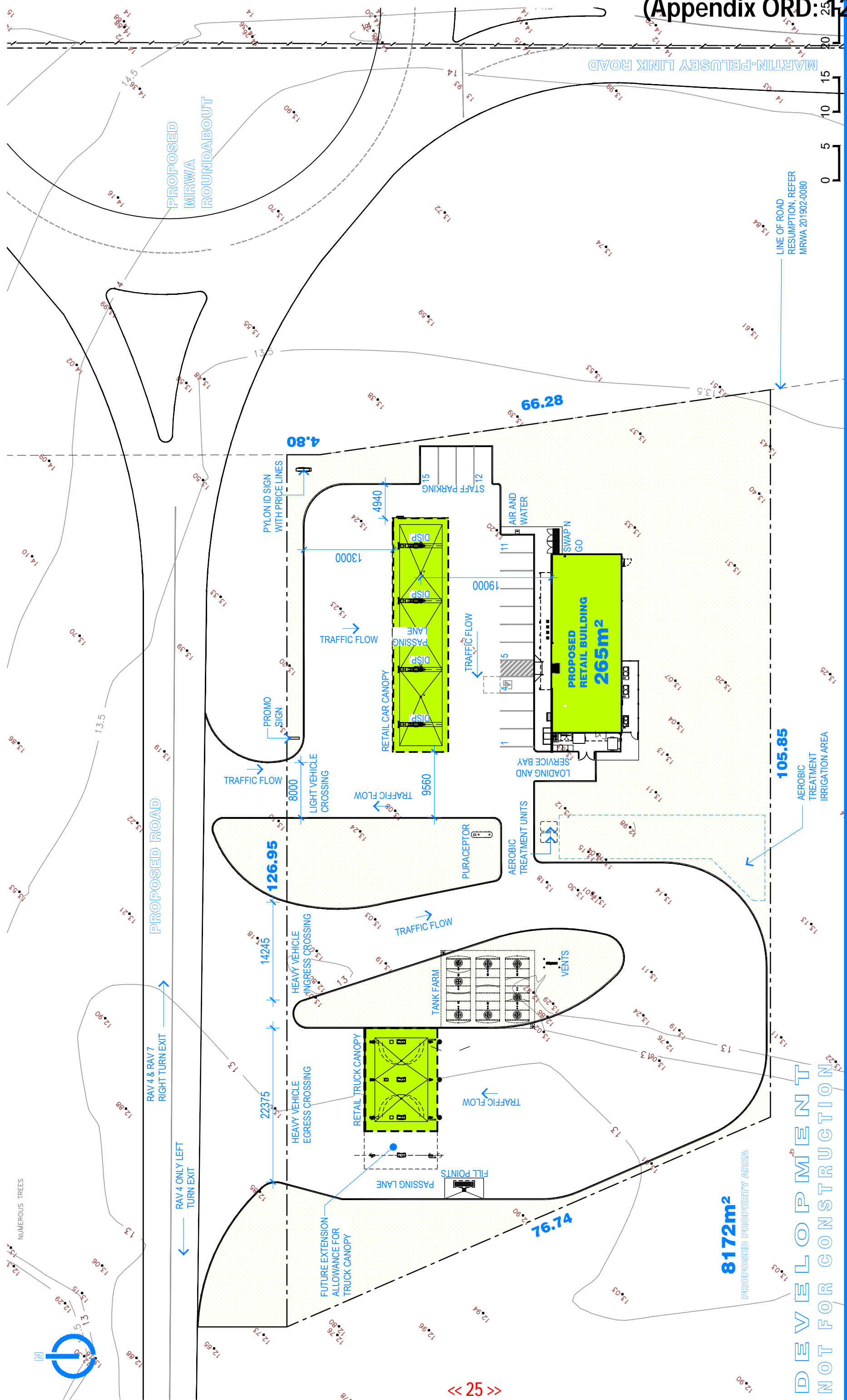
PROJECT

EASTCOURT PROPERTY GROUP
PIGTON FUEL STATION
LOT 603 MARTIN PELUSEY ROAD
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A102
1:500

28.08.19

REV.	DESCRIPTION
A	Development Application

SHEET
PLAN
SITE
PROPOSED

PROJECT
EASTCOURT PROPERTY GROUP
PIGTON FUEL STATION
LOT 603 MARTIN PELUSEY ROAD
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REV. DESCRIPTION
A Development Application

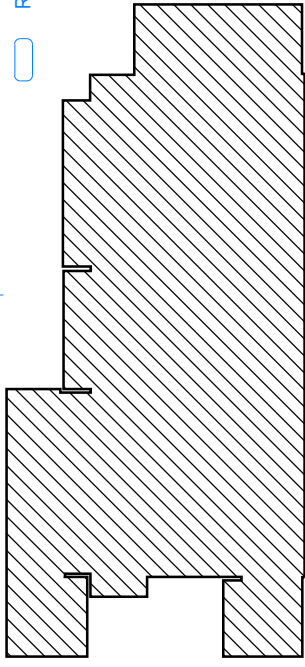
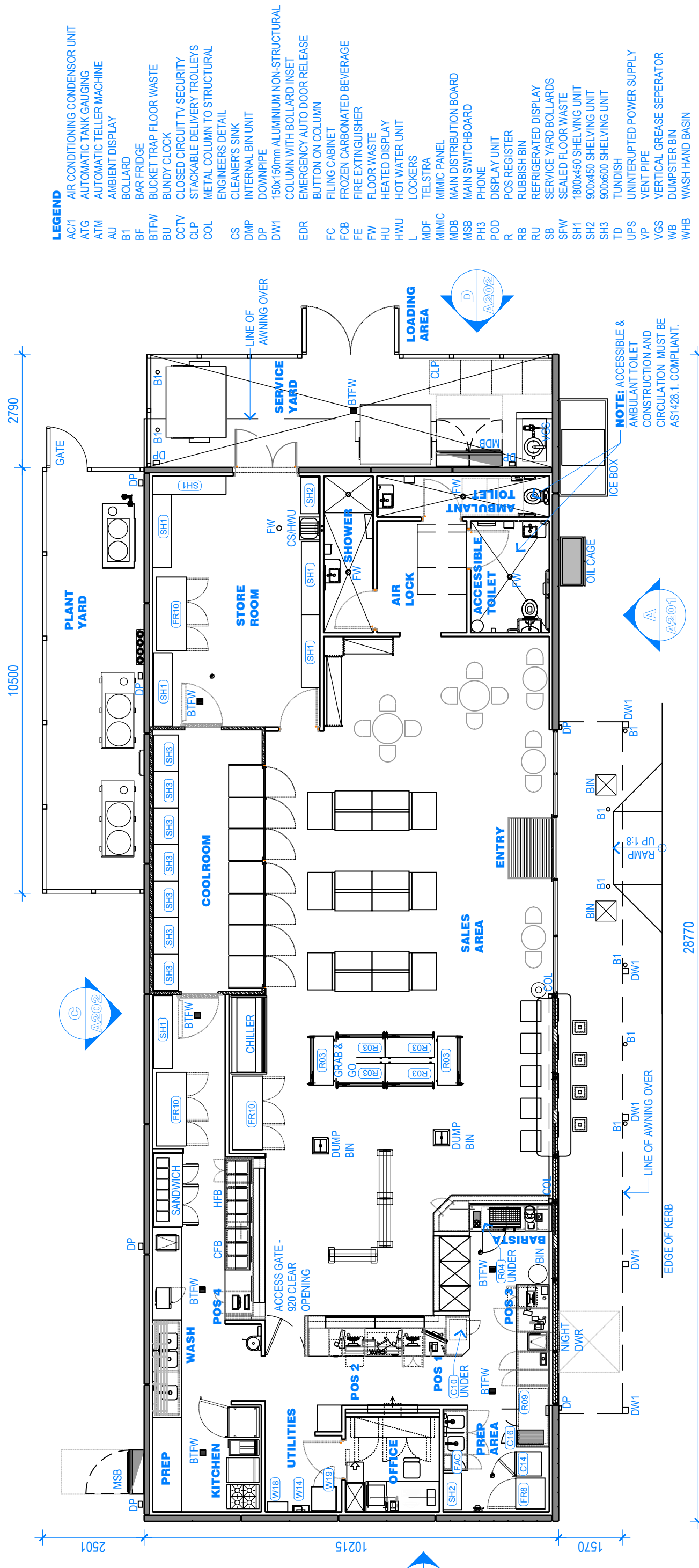
SHEET
PLAN
BUILDING

PROJECT
EASTCOURT PROPERTY GROUP
PIGTON FUEL STATION
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TOTAL SALES ROOM AREA : 171m²
BUILDING G.F.A. : 265m²
(EXCLUDES SERVICE & PLANT YARDS)

LEGEND - EQUIPMENT

- (C14) MIVE OVEN
- (C16) MERRYCHEF
- (C17) DISHWASHER
- (FAC) FIRST AID CABINET
- (FR8) UPRIGHT FREEZER
- (R09) UNDERCOUNTER FRIDGE
- (SH1) SHELF
- (SH2) SHELF
- (W14) DOMS
- (W18) ATG
- (W19) DATA RACK
- (W20) LOCKER
- (R03) SCOPE GRAB AND GO UNITS
- (FR2) UPRIGHT FREEZER
- (R02) WALL CHILLER
- (R04) BARISTA MILK FRIDGE
- (C10) MICROWAVE

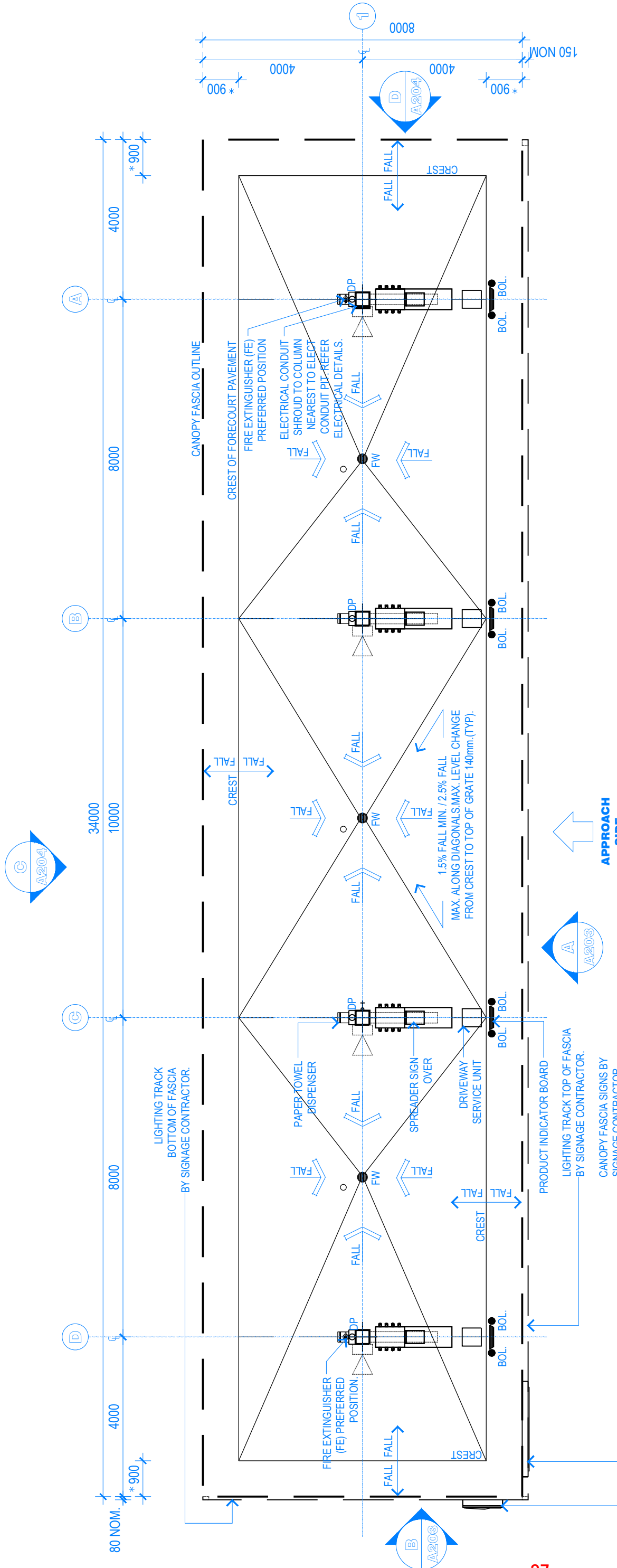
WALL LEGEND

- CONCRETE PANEL WALLS PLUS 76mm METAL STUD FRAME AND 10mm GYROCK LINING.
- INTERNAL METAL STUD FRAMED WALL WITH 10mm GYROCK LINING ON ALL EXPOSED SURFACES
- COOLROOM & FREEZER PANELLING, REFER TO REFRIGERATION DRAWINGS.
- GLAZED SHOPFRONT

NOTES : CONSTRUCTION

- C.01 PROVIDE ADDITIONAL BLOCKING / FRAMING WITHIN WALLS AS NECESSARY TO SUPPORT FIXTURES.
- C.02 REFER TO DRAWING A202 FOR FITTINGS, FIXTURE AND EQUIPMENT SCHEDULES.

REFER TO EQUIPMENT SCHEDULE



- KEY**
- DP DOWNPIPE.
 - FW FLOOR WASTE
 - BOL METAL BOLLARD 1650 x 1200mm HIGH (TYPICAL)
 - △ SPEAKER
 - ↘ FALL
 - ↘ FALL IN FORECOURT PAVING
 - ☐ DISPENSER WITH SPREADER SIGN OVER
 - +o WATER POINT
- NOTE : CONSTRUCTION**
- C.01 2 FIRE EXTINGUISHERS REQUIRED BY AS1940-PLACE ON COLUMNS EACH SIDE OF CANOPY FACING SALESROOM.
 - C.02 ADDITIONAL FIRE EXTINGUISHERS MAY BE REQUIRED TO SUIT SITE SPECIFIC CONDITIONS.
 - C.03 PUMP POSITION AND SPREADER SIZE TO BE CONFIRMED ON SITE TO SUIT CANOPY COLUMN SIZE.
 - C.04 FOR FLOOR WASTE / FORECOURT DRAINAGE, DOWNPIPE AND CANOPY BOX GUTTER DETAILS, REFER TO THE HYDRAULIC ENGINEERS DOCUMENTATION.

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SHEET
PLAN
CANOPY
CAR

REV. DESCRIPTION
A Development Application

27.08.19

A104

1:100

A



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KEY

DP	DOWNPIPE.
FW	FLOOR WASTE
BOL	CONCRETE BOLLARD 600Ø x 1400mm HIGH (TYPICAL)
COL	COLUMN
	SPEAKER
	FALL IN FORECOURT PAVING

	3 HOSE INTEGRATED DIESEL & AdBlue DISPENSER (GALLAGHER)
	6 HOSE INTEGRATED DIESEL & AdBlue DISPENSER (GALLAGHER)
	ISLAND CARD READER
	WATER POINT

PUMP TYPE

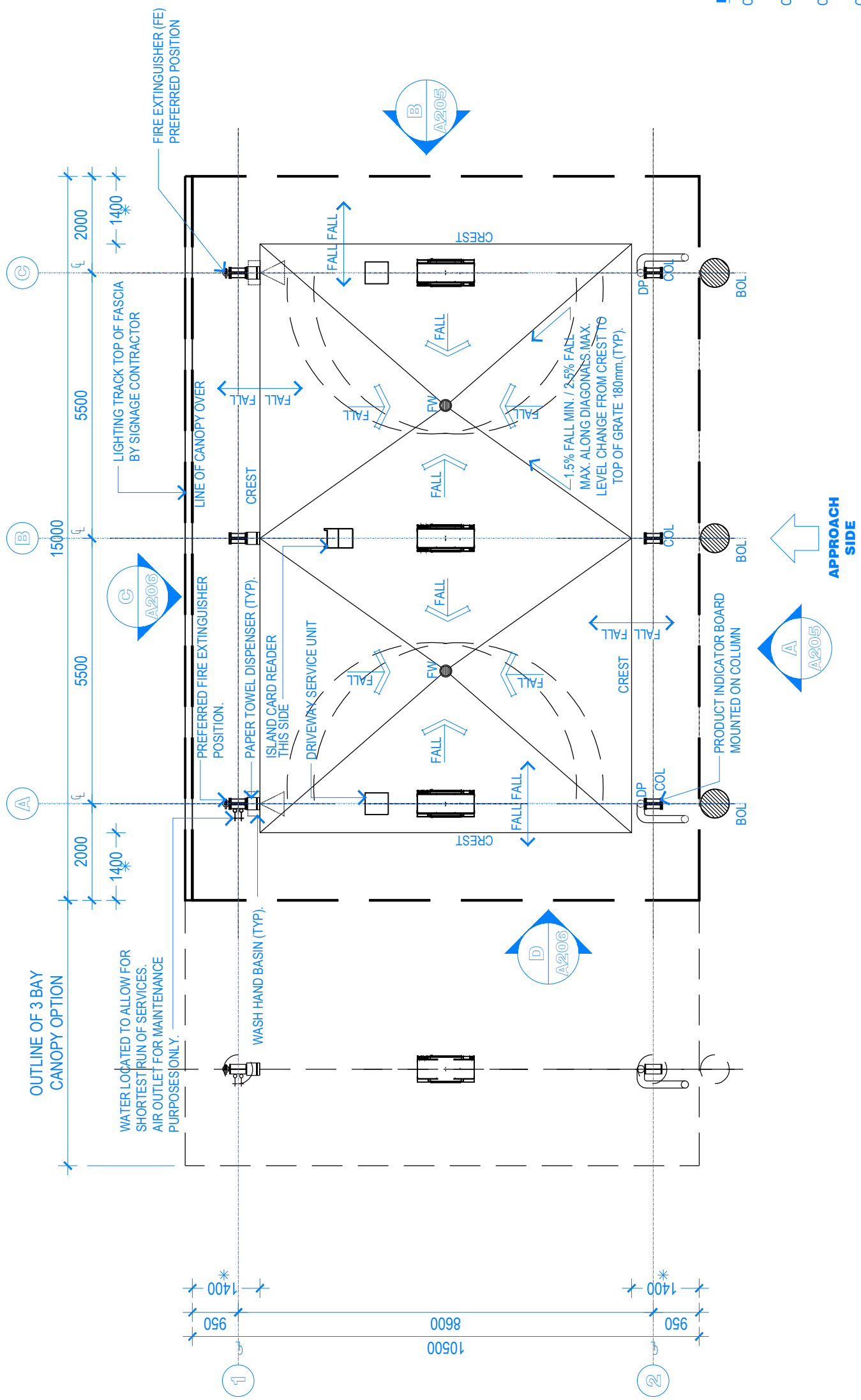
HF - HIGH FLOW	Flow Rate Nom. 80 l/m
UHF - ULTRA HIGH FLOW	Nom. 120 l/m
DEF - DIESEL EXHAUST FLUID	Nom. 30 l/m

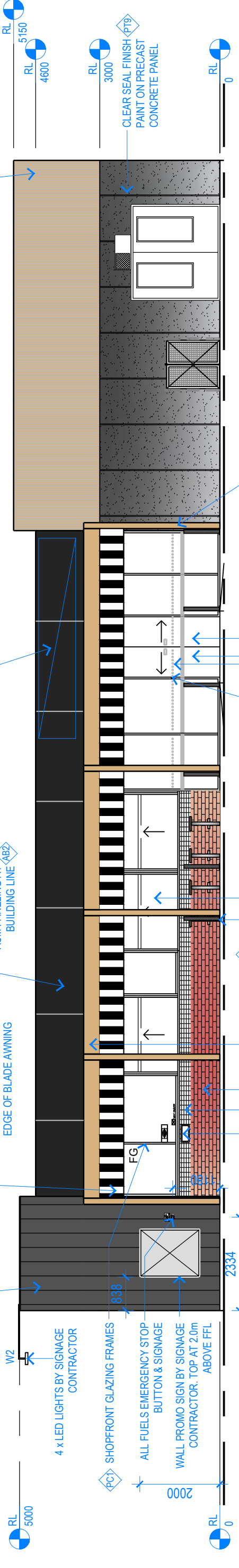
NOTE : CONSTRUCTION

- C.01 2 FIRE EXTINGUISHERS REQUIRED BY AS1940-PLACE ON COLUMNS EACH SIDE OF CANOPY FACING SALESROOM.
- C.02 ADDITIONAL FIRE EXTINGUISHERS MAY BE REQUIRED TO SUIT SITE SPECIFIC CONDITIONS.
- C.03 PUMP POSITION AND SPREADER SIZE TO BE CONFIRMED ON SITE TO SUIT CANOPY COLUMN SIZE.
- C.04 FOR FLOOR WASTE / FORECOURT DRAINAGE, DOWNPIPE AND CANOPY BOX GUTTER DETAILS, REFER TO THE HYDRAULIC ENGINEERS DOCUMENTATION.

NOTE

* DIMENSION= 25% OF VERTICAL HEIGHT FROM TOP OF CONTAINMENT CREST TO DRIP LINE OF ROOF/VERTICAL FASCIA FACE (DO NOT INCLUDE ANGLED FASCIA) OR AS REQUIRED BY OTHER LOCAL WATER AUTHORITIES.





FRONT ELEVATION
Scale: 1:100

RIGHT ELEVATION
Scale: 1:100

NOTES: CONSTRUCTION
C.01 PROVIDE POWER & SUPPORT FRAMING BEHIND ALL ILLUMINATED SIGNS AS NECESSARY.
C.02 PROVIDE INDIVIDUAL POWER SUPPLY POINTS BEHIND 'FOODARY' LETTERS.

KEY: GENERAL

- RL 5000 - HEIGHT IN MILLIMETERS IN RELATION TO DATUM LEVEL 0.00
- RL 3000 - SLAB LEVEL OF SALES AREA
- FINISHES REFERENCE SYMBOL
- AVF ANTI-VANDAL FILM
- B1 BOLLARD
- DP DOWN PIPE
- FG FIXED GLASS
- SG SLIDING GLASS

NOTES:
REFER TO DRAWING G002 FOR EXTERIOR FINISHES SCHEDULES.

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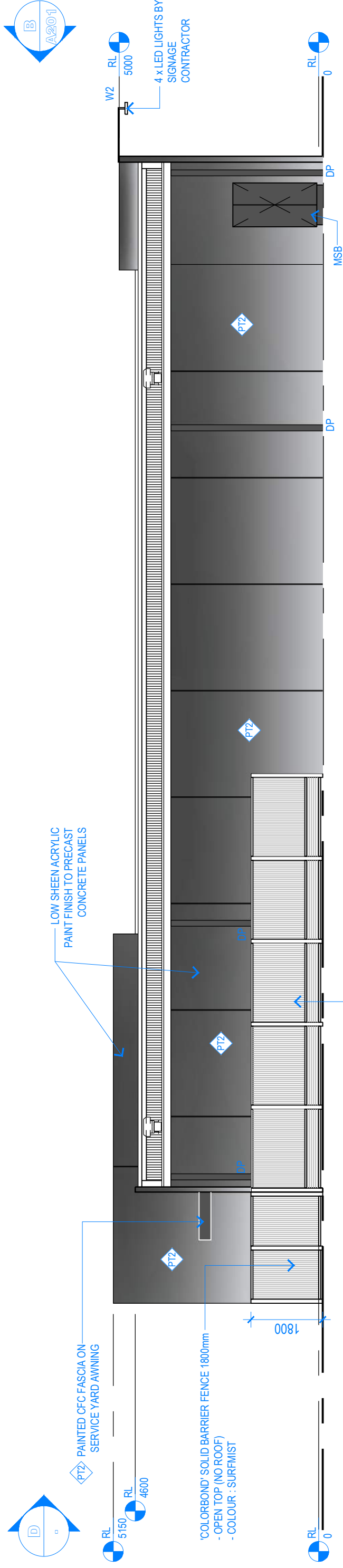
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SHEET
ELEVATION
BUILDING
SHEET 1

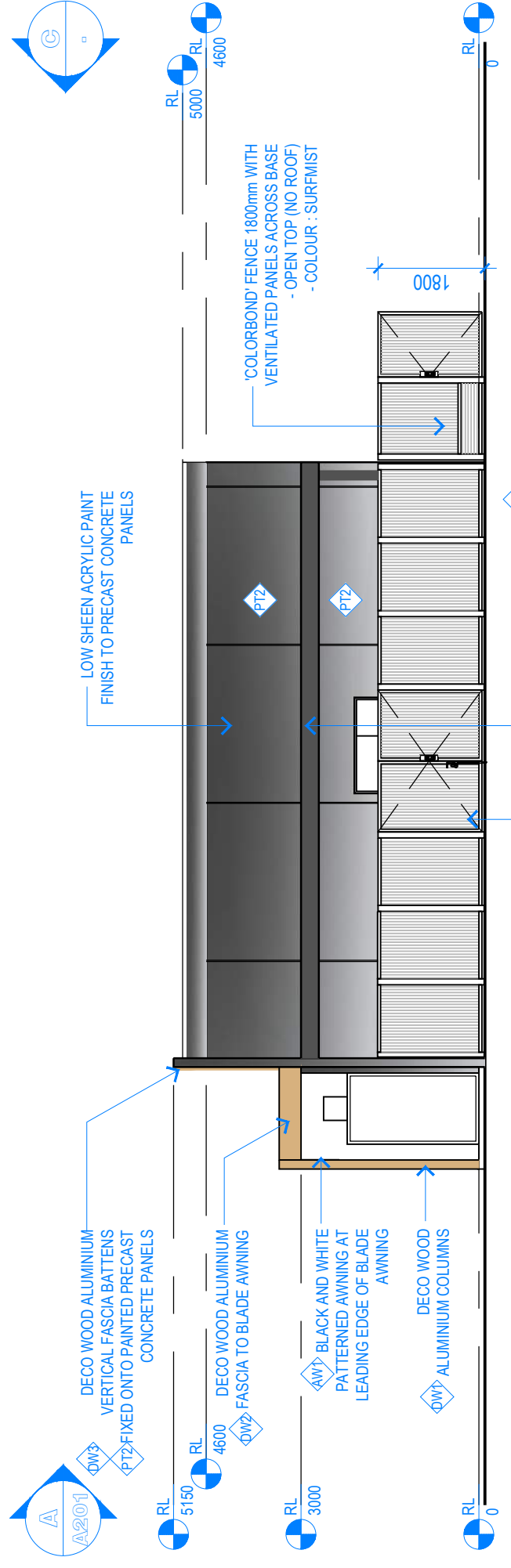
REV. DESCRIPTION
A Development Application

0 1 2 3 4 5

27.08.19
A201
1:100
A



REAR ELEVATION
Scale: 1:100



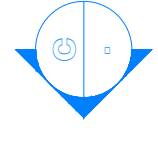
LEFT ELEVATION
Scale: 1:100

NOTES: CONSTRUCTION
C.01 PROVIDE POWER & SUPPORT FRAMING BEHIND ALL ILLUMINATED SIGNS AS NECESSARY.
C.02 PROVIDE INDIVIDUAL POWER SUPPLY POINTS BEHIND 'FOODARY' LETTERS.

KEY: GENERAL

- RL 5000 - HEIGHT IN MILLIMETERS IN RELATION TO DATUM LEVEL 0.00 - SLAB LEVEL OF SALES AREA
- PT9 - FINISHES REFERENCE SYMBOL
- AVF - ANTI-VANDAL FILM
- B1 - BOLLARD
- DP - DOWN PIPE
- FG - FIXED GLASS
- SG - SLIDING GLASS

NOTES:
REFER TO DRAWING G002 FOR EXTERIOR FINISHES SCHEDULES.



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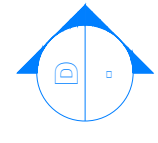
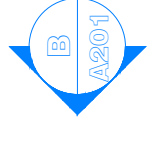
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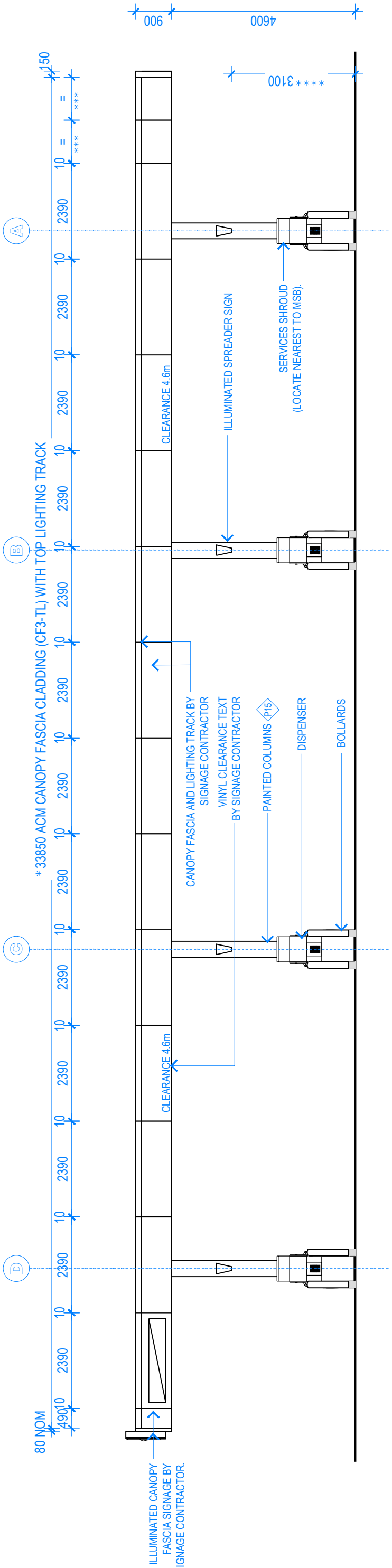
SHEET
ELEVATION
BUILDING
SHEET 2

REV. DESCRIPTION
A Development Application

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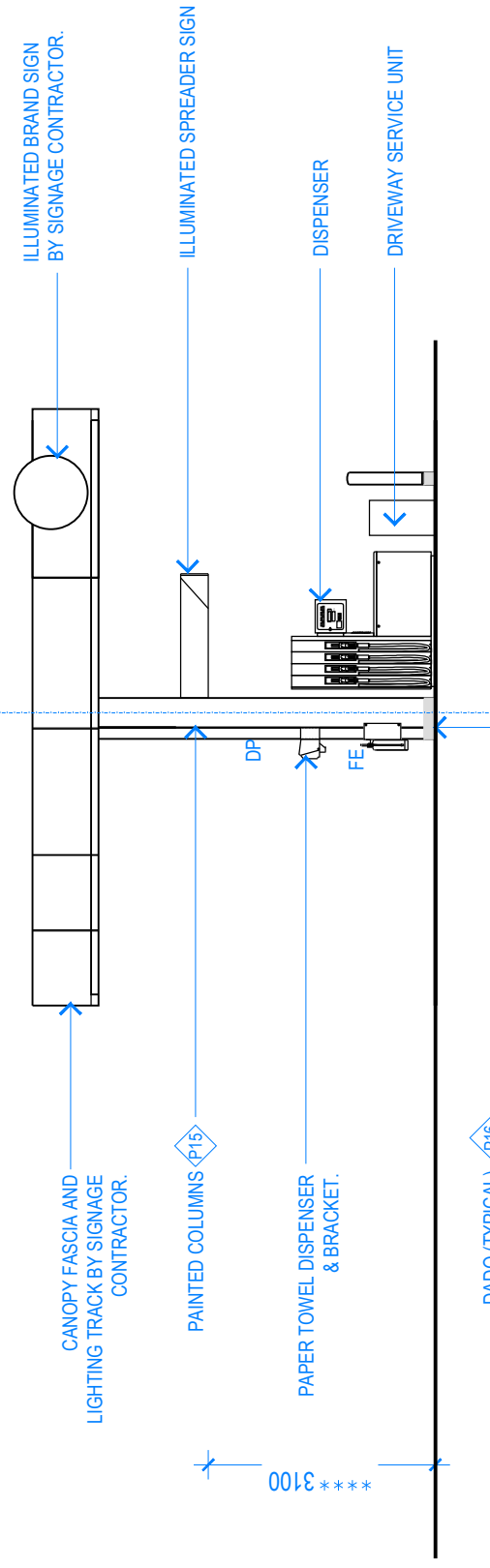
A202
1:100
A





A APPROACH SIDE ELEVATION
Scale: 1:100

* 8150 ILLUMINATED A.C.M. CANOPY FASCIA CLADDING (CF3-BL)
= 0 1720 10 2050 10 2300



B SIDE ELEVATION
Scale: 1:100

GUIDANCE NOTES

- * THE EAVES OVERHANG DIMENSIONS SHOWN ARE INDICATIVE ONLY AND ARE BASED ON AN APPROXIMATE 10° LINE OFF VERTICAL FROM CANOPY EDGE TO CREST OF FORECOURT. SITE SPECIFIC REQUIREMENTS FOR DRAINAGE/OVERHANGS SHALL BE DETERMINED BY THE CIVIL ENGINEER AND DIMENSIONS ADJUSTED ACCORDINGLY
- ** MAKE UP PANEL
- *** IF END PANEL IS LESS THAN HALF OF 2390mm THEN DIVIDE EQUALLY THE TWO END PANELS.
- **** CRITICAL OH&S DIMENSION

NOTE : CONSTRUCTION

C.01 PRODUCT INDICATOR BOARDS NOT SHOWN FOR DRAWING CLARITY.

NOTE : FINISHES

CANOPY FINISHES WILL BE DEPENDENT ON COMPANY BRANDING



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 PIGTON EAST
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SHEET
 ELEVATION
 CANOPY
 CAR
 SHEET 1

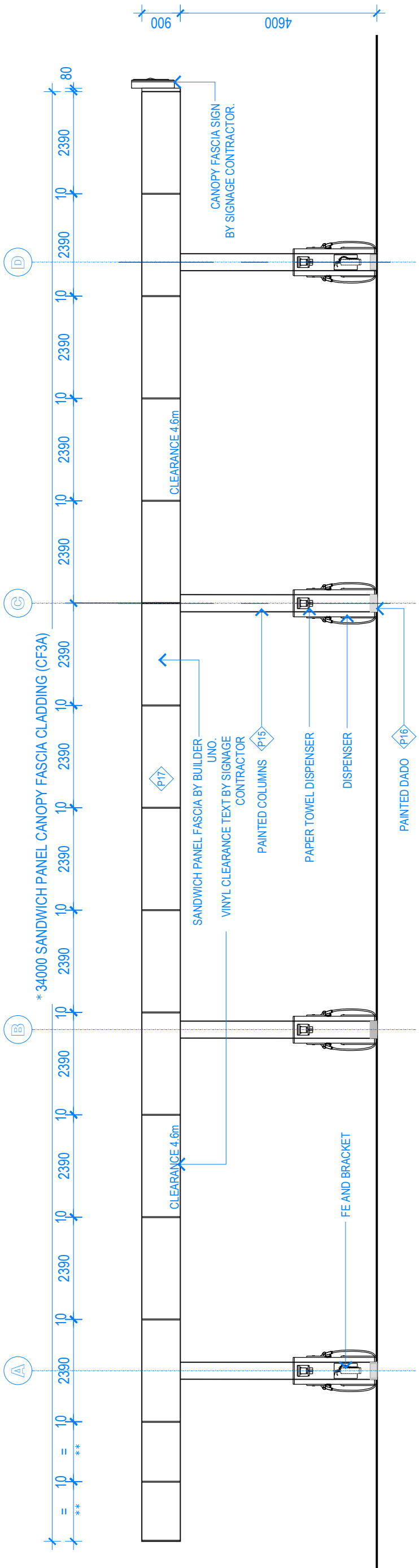
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A203

1:100

A

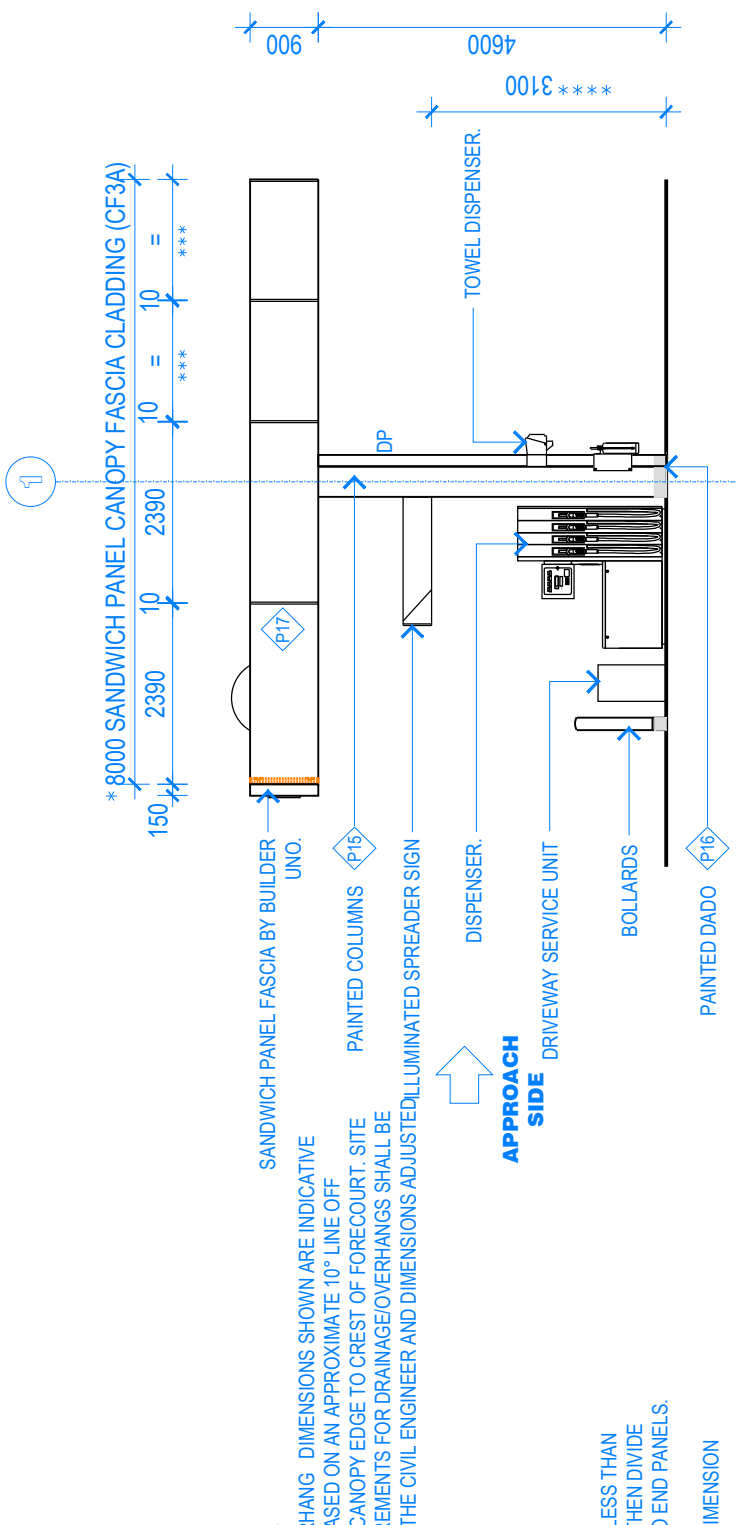


SHOP SIDE ELEVATION
Scale: 1:100

NOTE : FINISHES
CANOPY FINISHES WILL BE DEPENDENT ON COMPANY BRANDING

CANOPY - FINISHES SCHEDULE

KEY	MATERIAL	MANUFACTURER	REF. NO.	FINISH	COLOUR
ACM FASCIA	ALUMINIUM COMPOSITE SHEET	SIGNAGE SUPPLIER	CF3 - TL	SATIN	RED
SANDWICH PANEL TOP FLASHING	STEEL	BLUESCOPE LYSAGHT	-	DULUX PAINT FINISH - SATIN	DULUX CANOPY RED
ROOF	STEEL	BLUESCOPE LYSAGHT	KLIPOK 700 HI-STRENGTH 0.48 BMT	'COLORBOND' SATIN	SURFMIST
SOFFIT LINING AND TRIM	STEEL	BLUESCOPE LYSAGHT	EASYCLAD 2PF 300	'COLORBOND' SATIN	SURFMIST
BOLLARDS	METAL	LEDA	HIG150RCAL + FULL STOP RING	VARIOUS	VARIOUS
KEY	ITEM/SUBGRADE	COLOUR/CODE	SPECIFICATION		
P15	CANOPY - METAL COLUMNS - METAL BOLLARDS	SPIDERS WEB DULUX 84645 (DELTA LIGHT GREY)	AU_SD08562	DULUX SUPER ENAMEL HIGH GLOSS ON NEW ZINC COATED STEEL / GALVANISED STEEL (COMMERCIAL) [EXTERIOR / COASTAL]	
			1ST COAT	AUDD0156 DULUX PROFESSIONAL GALVANISED IRON PRIMER	
			2ND COAT	AUDD0026 SUPER ENAMEL HIGH GLOSS	
			3RD COAT	AUDD0026 SUPER ENAMEL HIGH GLOSS	
P16	CANOPY - METAL COLUMNS (DADO)	DULUX84599 MEDIUM GREY	AU_SD08562	DULUX SUPER ENAMEL HIGH GLOSS ON NEW ZINC COATED STEEL / GALVANISED STEEL (COMMERCIAL) [EXTERIOR / COASTAL]	
			1ST COAT	AUDD0156 DULUX PROFESSIONAL GALVANISED IRON PRIMER	
			2ND COAT	AUDD0026 SUPER ENAMEL HIGH GLOSS	
			3RD COAT	AUDD0026 SUPER ENAMEL HIGH GLOSS	
P17	CANOPY - INSULATED ALUMINIUM SANDWICH PANEL	DULUX CANOPY RED - A4010	AU13768	DULUX WEATHERSHIELD GLOSS ON PREVIOUS. POWDER COATED ALUMINIUM (EXTERIOR)	
			1ST COAT	AUD10540 DULUX LUXEPOXY 4 WHITE PRIMER TWO PACK PRIMER	
			2ND COAT	AUDD0054 DULUX WEATHERSHIELD GLOSS	
			3RD COAT	AUDD0054 DULUX WEATHERSHIELD GLOSS	



SIDE ELEVATION
Scale: 1:100

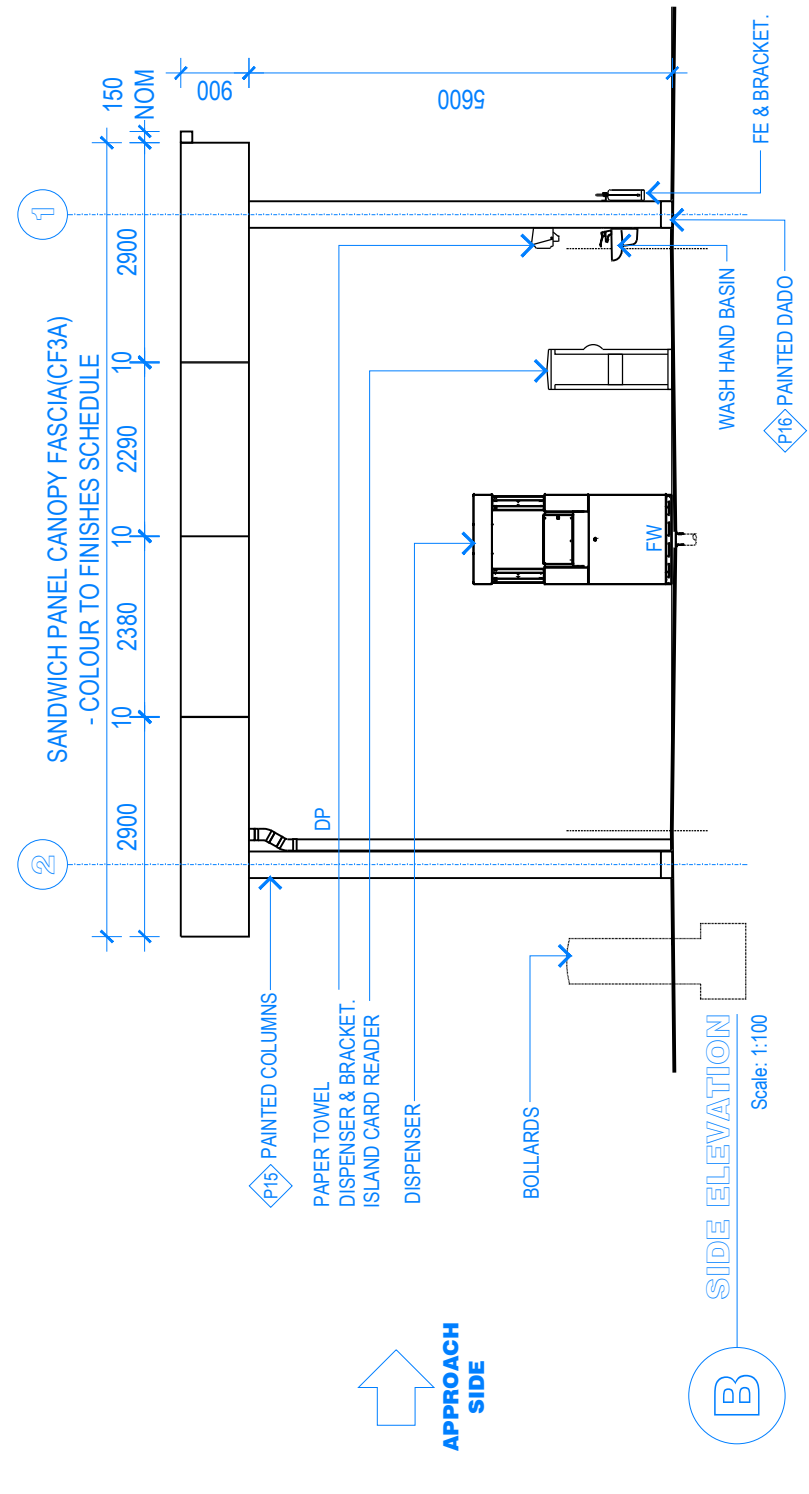
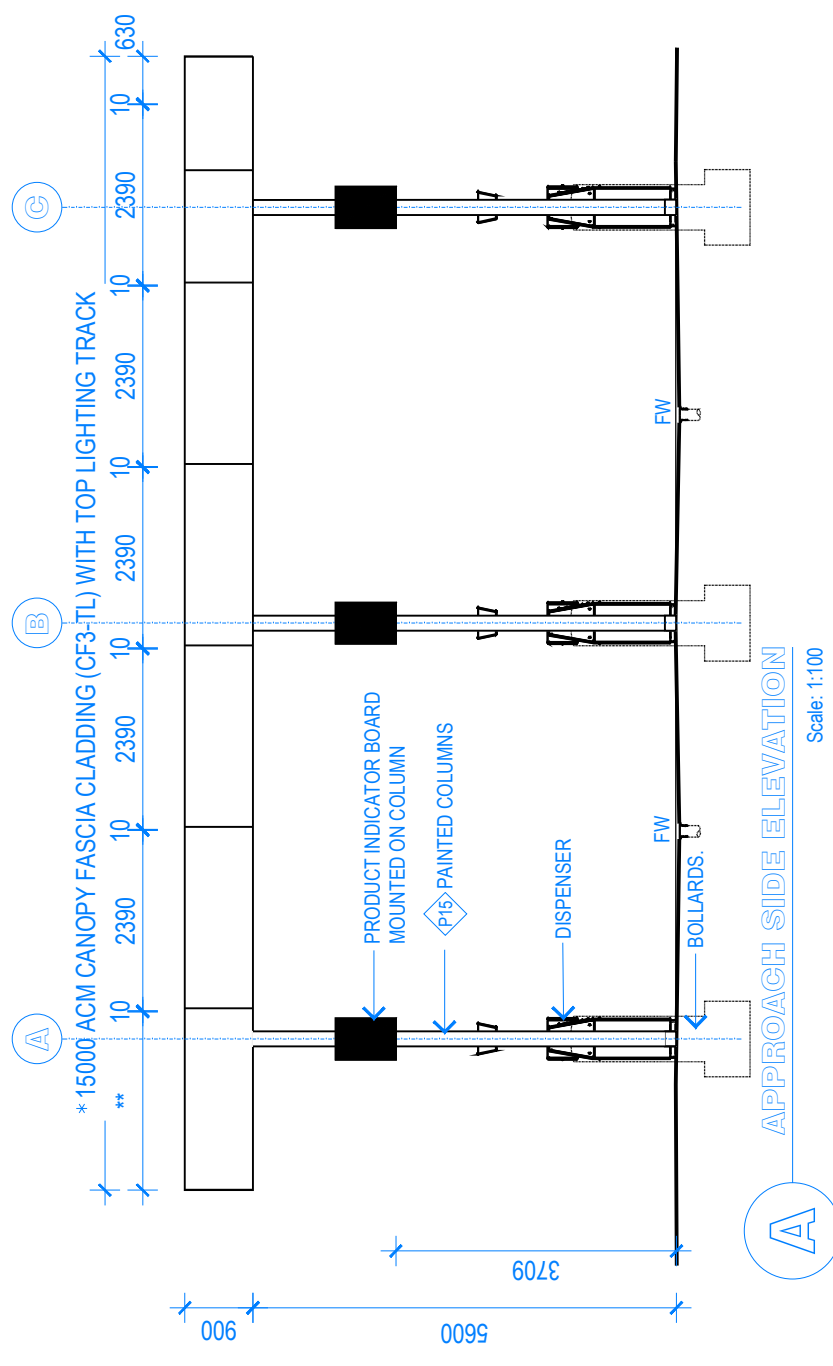
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- ** MAKE UP PANEL
- *** IF END PANEL IS LESS THAN HALF OF 2390mm THEN DIVIDE EQUALLY THE TWO END PANELS.
- **** CRITICAL OH&S DIMENSION

NOTE : CONSTRUCTION

C01 PRODUCT INDICATOR BOARDS NOT SHOWN FOR DRAWING CLARITY.

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CANOPY - FINISHES SCHEDULE

KEY	MATERIAL	MANUFACTURER	REF. NO.	FINISH	COLOUR
ACM FASCIA	ALUMINIUM COMPOSITE SHEET	SIGNAGE SUPPLIER	CF3 - TL	SATIN	RED
SANDWICH PANEL TOP FLASHING	STEEL	BLUESCOPE LYSAGHT	-	DULUX PAINT FINISH - SATIN	DULUX CANOPY RED
ROOF	STEEL	BLUESCOPE LYSAGHT	KLIPOK 700 HI-STRENGTH 0.48 BMT	'COLORBOND' SATIN	SURFMIST
SOFFIT LINING AND TRIM	STEEL	BLUESCOPE LYSAGHT	EASYCLAD 2PF 300	'COLORBOND' SATIN	SURFMIST
BOLLARDS	METAL	LEDA	HIG150RCAL + FULL STOP RING	VARIOUS	VARIOUS
KEY	ITEM/SUBGRADE	COLOUR/CODE	SPECIFICATION		
P15	CANOPY - METAL COLUMNS - METAL BOLLARDS	SPIDERS WEB DULUX 84645 (DELTA LIGHT GREY)	AU_SD08562 DULUX SUPER ENAMEL HIGH GLOSS ON NEW ZINC COATED STEEL / GALVANISED STEEL (COMMERCIAL) [EXTERIOR / COASTAL] 1ST COAT AUDD00156 DULUX PROFESSIONAL GALVANISED IRON PRIMER 2ND COAT AUDD00226 SUPER ENAMEL HIGH GLOSS 3RD COAT AUDD00226 SUPER ENAMEL HIGH GLOSS		
P16	CANOPY - METAL COLUMNS (DADO)	DULUX84599 MEDIUM GREY	AU_SD08562 DULUX SUPER ENAMEL HIGH GLOSS ON NEW ZINC COATED STEEL / GALVANISED STEEL (COMMERCIAL) [EXTERIOR / COASTAL] 1ST COAT AUDD00156 DULUX PROFESSIONAL GALVANISED IRON PRIMER 2ND COAT AUDD00226 SUPER ENAMEL HIGH GLOSS 3RD COAT AUDD00226 SUPER ENAMEL HIGH GLOSS		
P17	CANOPY - INSULATED ALUMINIUM SANDWICH PANEL	DULUX CANOPY RED - A4010	AUS13768 DULUX WEATHERSHIELD GLOSS ON PREVIOUSLY POWDER COATED ALUMINIUM (EXTERIOR) AUD10540 DULUX LUXEPOXY 4 WHITE PRIMER TWO PACK PRIMER AUDD0054 DULUX WEATHERSHIELD GLOSS AUDD0054 DULUX WEATHERSHIELD GLOSS		

NOTE : CONSTRUCTION

C.01 PRODUCT INDICATOR BOARDS NOT SHOWN FOR DRAWING CLARITY.

NOTE : FINISHES

CANOPY FINISHES WILL BE DEPENDENT ON COMPANY BRANDING

GUIDANCE NOTES

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** MAKE UP PANEL

*** IF END PANEL IS LESS THAN HALF OF 2390mm THEN DIVIDE EQUALLY THE TWO END PANELS.

**** CRITICAL OH&S DIMENSION

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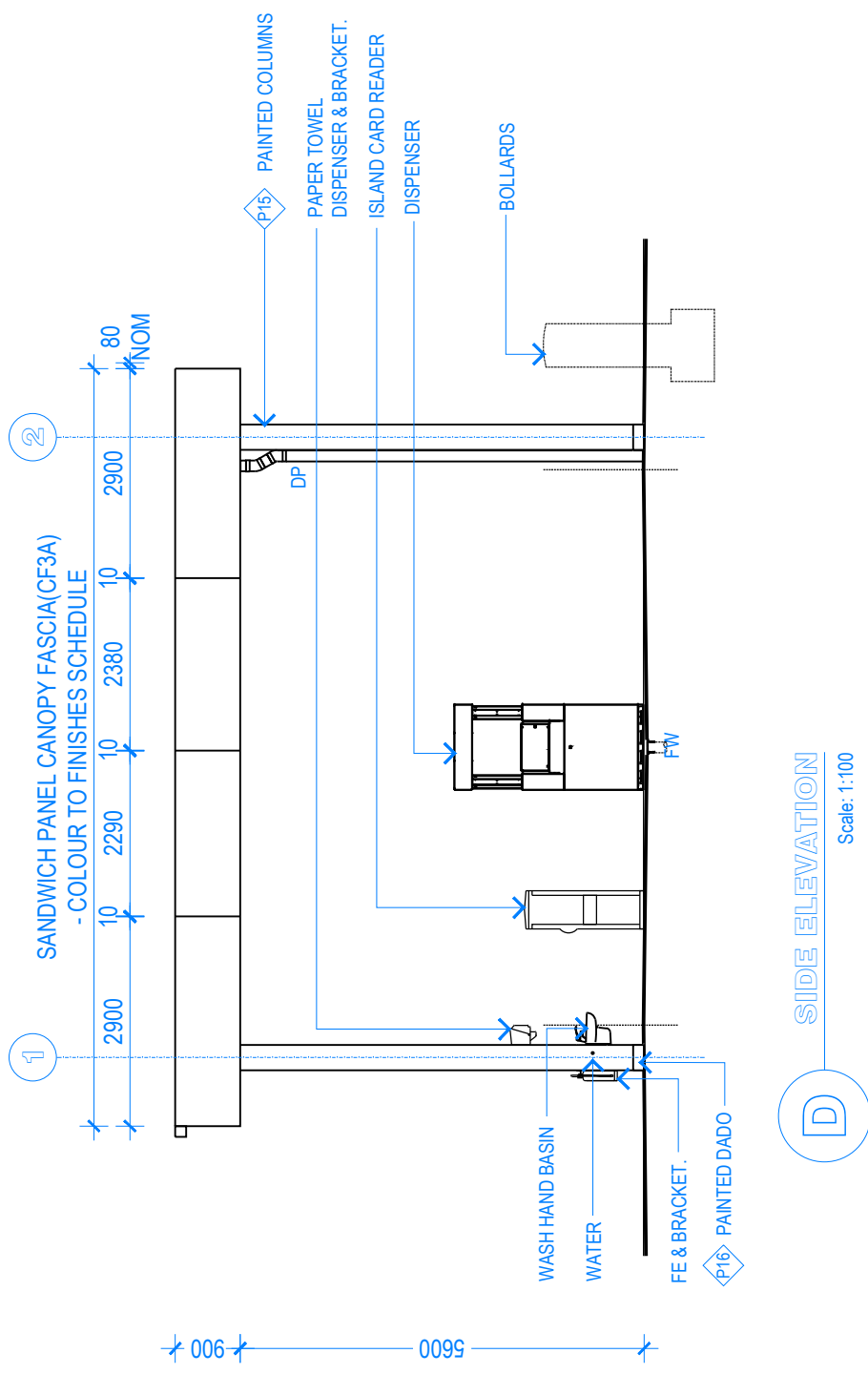
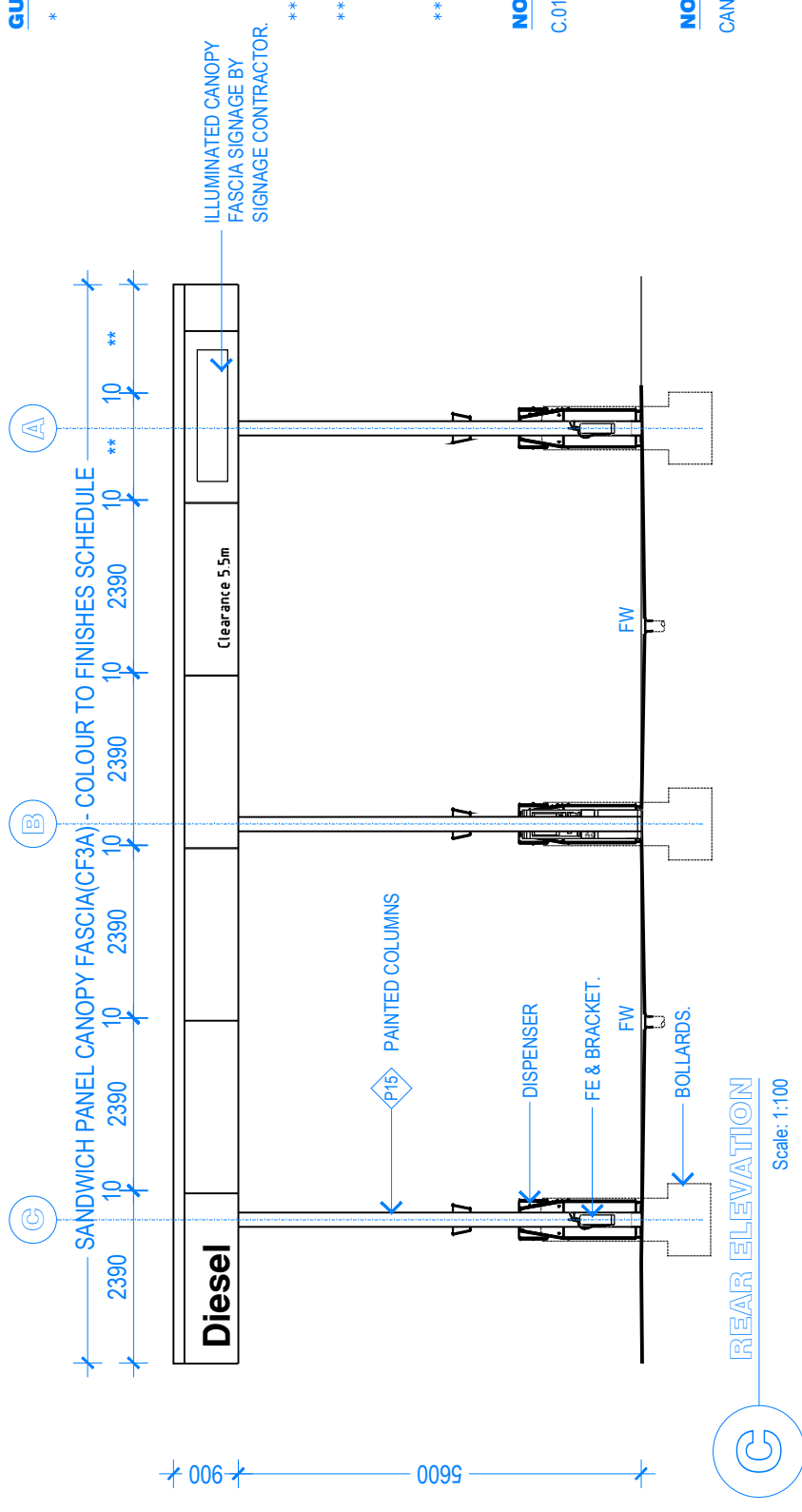
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SHEET
ELEVATION
CANOPY
TRUCK
SHEET 1

REV. DESCRIPTION
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27.08.19

A205
1:100
A



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**** CRITICAL OH&S DIMENSION

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ELEVATION
CANOPY
TRUCK
SHEET 2

REV. DESCRIPTION
A Development Application

27.08.19

A2006

1:100

A

28.08.19

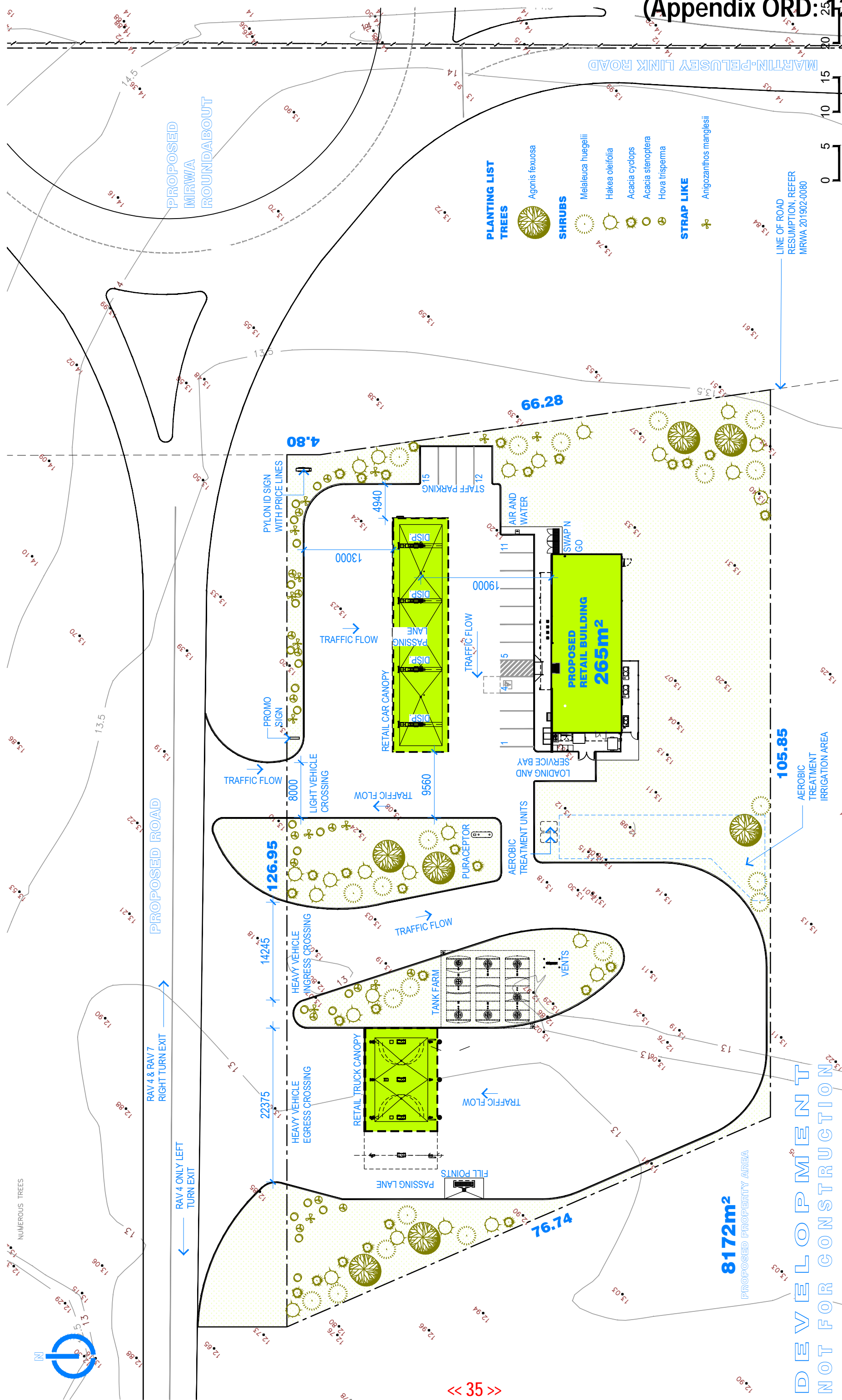
REV. DESCRIPTION
A Development Application

SHEET
PLAN
SITE
LANDSCAPING

PROJECT
EASTCOURT PROPERTY GROUP
PIGTON FUEL STATION
LOT 603 MARTIN PELUSEY ROAD
PIGTON EAST
1920-014

project consultants
DESIGN | DOCUMENT | DELIVER

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- PLANTING LIST**
- TREES**
 - Agonis flexuosa
 - SHRUBS**
 - Melaleuca huegelii
 - Hakea oleifolia
 - Acacia cyclops
 - Acacia stenoptera
 - Hoya trisperma
 - STRAP LIKE**
 - Anigozanthos manglesi

DEVELOPMENT
NOT FOR CONSTRUCTION

8172m²
PROPOSED PROPERTY AREA

105.85
AEROBIC TREATMENT IRRIGATION AREA

66.28

4.80

126.95

76.74

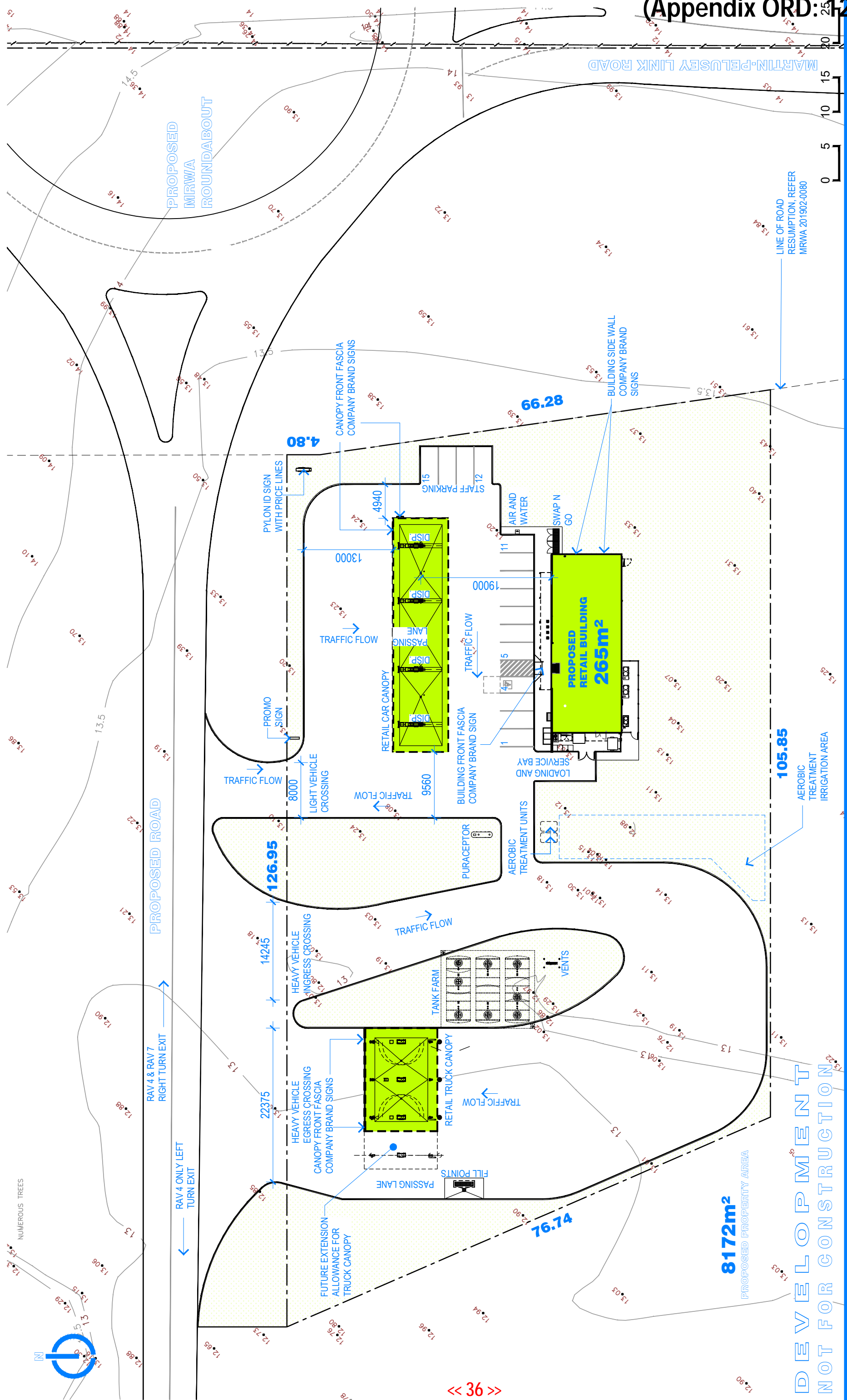
REV.	DESCRIPTION
A	Development Application

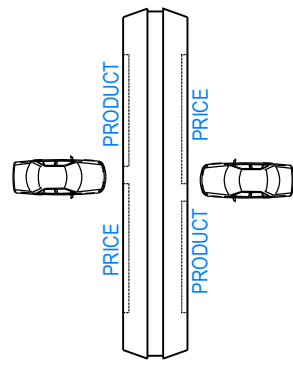
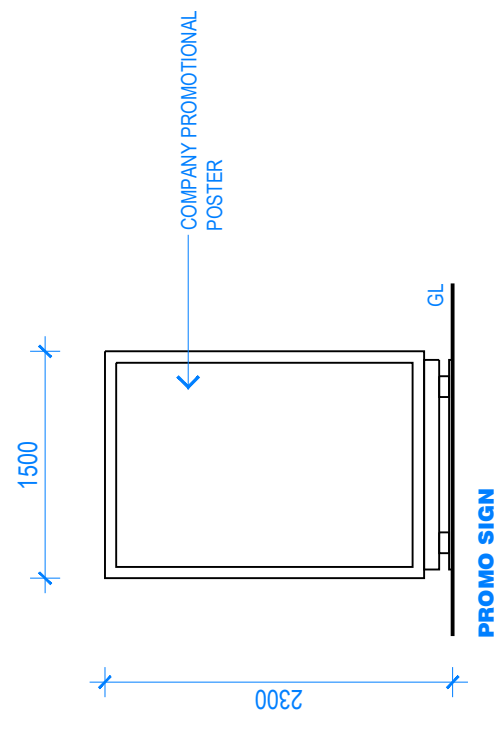
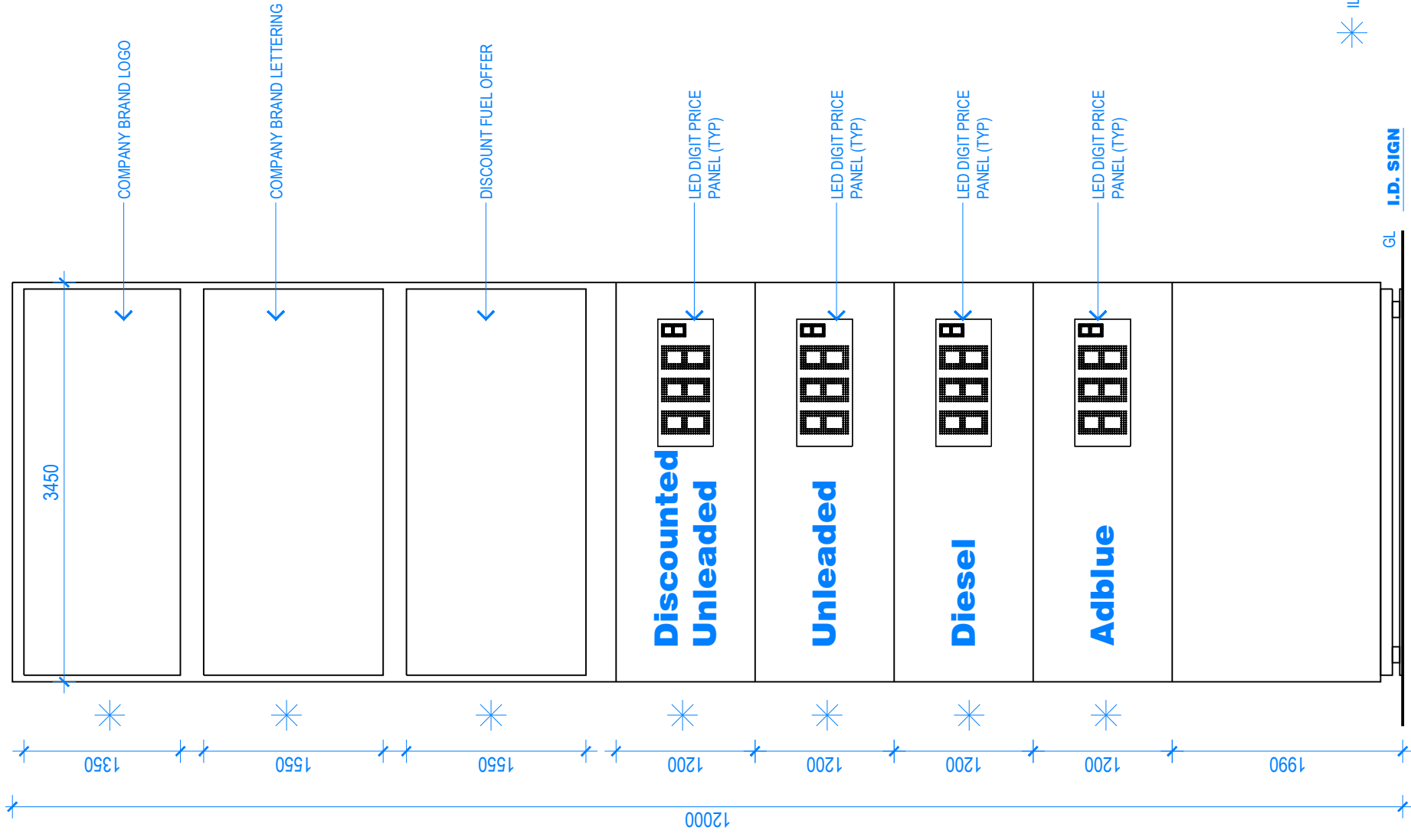
SHEET	DESCRIPTION
PLAN	Development Application
SITE	
SIGNAGE	

PROJECT
 EASTCOURT PROPERTY GROUP
 PIGTON FUEL STATION
 LOT 603 MARTIN PELUSEY ROAD
 PIGTON EAST
 1920-014

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* ILLUMINATED PANELS

DEVELOPMENT
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PROJECT
EASTCOURT PROPERTY GROUP
PIGTON FUEL STATION
LOT 603 MARTIN PELUSEY ROAD
PIGTON EAST
1920-014

SHEET
ELEVATION
SIGNAGE

REV. DESCRIPTION
A Development Application

26.08.19

S201

1:50

A

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28.08.19

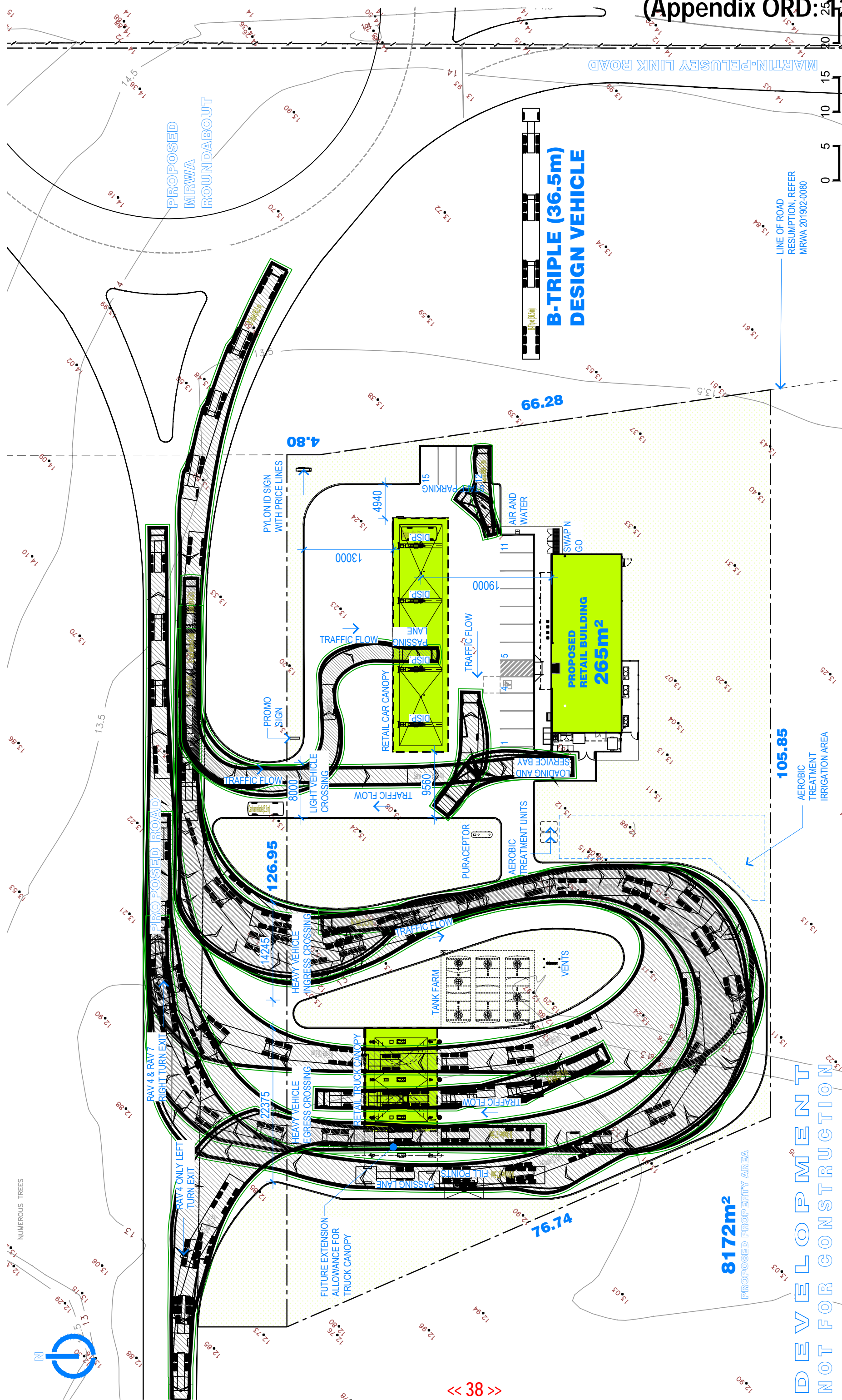
REV. DESCRIPTION
A Development Application

SHEET
PLAN
SITE
SWEEP PATHS

PROJECT
EASTCOURT PROPERTY GROUP
PIGTON FUEL STATION
LOT 603 MARTIN PELUSEY ROAD
PIGTON EAST
1920-014

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DEVELOPMENT
NOT FOR CONSTRUCTION

8172m²
PROPOSED PROPERTY AREA

76.74

105.85
AEROBIC TREATMENT IRRIGATION AREA

66.28

4.80

126.95

223.75

142.45

8000

13000

4940

19000

9560

142.45

126.95

142.45

223.75

142.45

126.95

142.45

126.95

142.45

126.95



Attachment Four

Service Station Site

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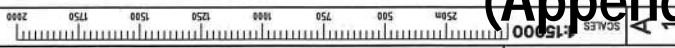


Attachment Five

Martin-Pelusey Road Design

LEGEND:

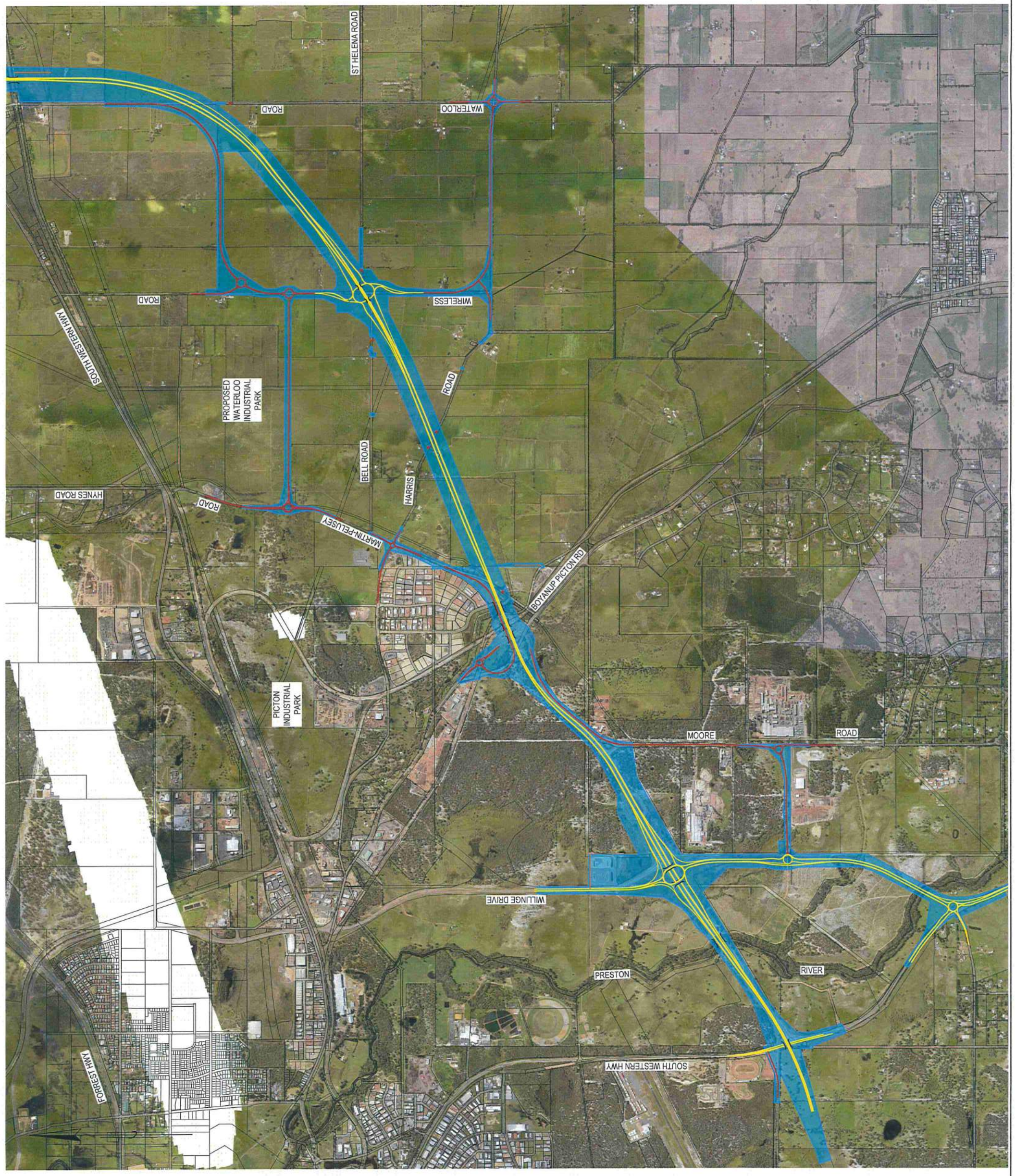
-  REFERRAL CORRIDOR
-  PROPOSED MAJOR ROAD
-  PROPOSED LOCAL ROAD
-  PROPOSED ACCESS ROAD

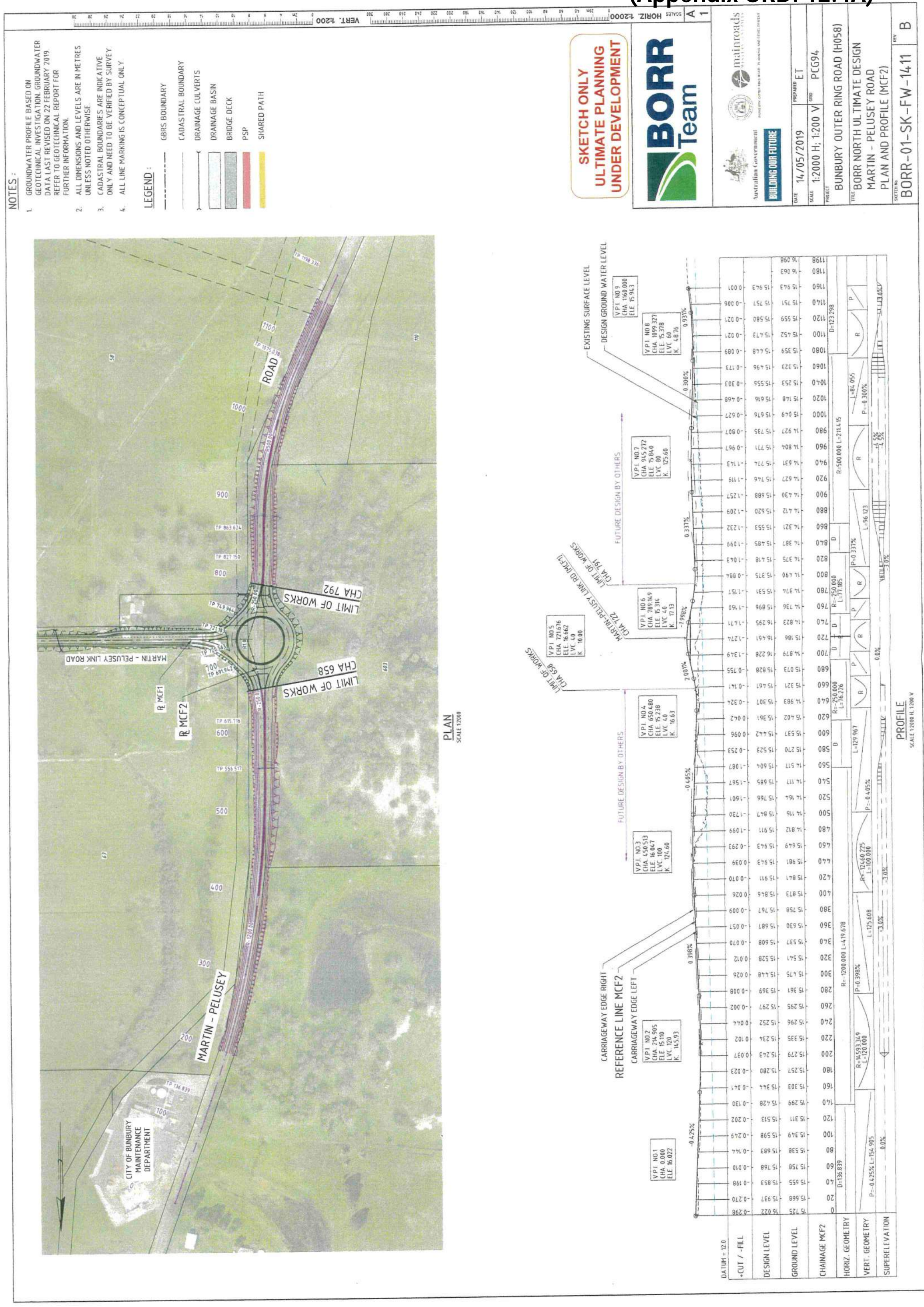


**SKETCH ONLY
ULTIMATE PLANNING
UNDER DEVELOPMENT**



DATE	15/04/2019	PREPARED	
SCALE	1:15000m @ A1	GRID	PCG 94
PROJECT	BUNBURY OUTER RING ROAD (H058)		
TITLE	BORR NORTH ACCESS STRATEGY PLAN SHEET 2 OF 2		
SKETCH NO.	BORR-01-SK-RD-0151		
REV	B		





- NOTES:**
- GROUNDWATER PROFILE BASED ON GEOTECHNICAL INVESTIGATION. GROUNDWATER DATA LAST REVISED ON 22 FEBRUARY 2019. REFER TO GEOTECHNICAL REPORT FOR FURTHER INFORMATION.
 - ALL DIMENSIONS AND LEVELS ARE IN METRES UNLESS NOTED OTHERWISE.
 - CADASTRAL BOUNDARIES ARE INDICATIVE ONLY AND NEED TO BE VERIFIED BY SURVEY.
 - ALL LINE MARKING IS CONCEPTUAL ONLY.

- LEGEND:**
- GBRS BOUNDARY
 - CADASTRAL BOUNDARY
 - DRAINAGE CULVERTS
 - DRAINAGE BASIN
 - BRIDGE DECK
 - PSP
 - SHARED PATH

**SKETCH ONLY
ULTIMATE PLANNING
UNDER DEVELOPMENT**



mainroads
AUSTRALIAN GOVERNMENT
BUILDING OUR FUTURE

DATE: 14/05/2019
PREPARED BY: ET
SCALE: 1:2000 H; 1:200 V
PROJECT: BUNBURY OUTER RING ROAD (H058)

TITLE: BORR NORTH ULTIMATE DESIGN
MARTIN - PELUSEY ROAD
PLAN AND PROFILE (MCF2)

REVISION: B
BORR-01-SK-FW-1411

PLAN
SCALE 1:2000

PROFILE
SCALE 1:2000 H, 1:200 V

GENERAL

- G001 DRAWING LIST
- G002 SCHEDULE FINISHES

ARCHITECTURAL

- A101 PLAN SITE EXISTING
- A102 PLAN SITE PROPOSED
- A103 PLAN BUILDING
- A104 PLAN CANOPY CAR
- A105 PLAN CANOPY TRUCK
- A201 ELEVATION BUILDING SHEET 1
- A202 ELEVATION BUILDING SHEET 2
- A203 ELEVATION CANOPY CAR SHEET 1
- A204 ELEVATION CANOPY CAR SHEET 2
- A205 ELEVATION CANOPY TRUCK SHEET 1
- A206 ELEVATION CANOPY TRUCK SHEET 2

TRAFFIC

- T101 PLAN SITE SWEEP PATHS

LANDSCAPING

- L101 PLAN SITE LANDSCAPING

SIGNAGE

- S101 PLAN SITE SIGNAGE
- S201 ELEVATION SIGNAGE

DEVELOPMENT
NOT FOR CONSTRUCTION



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PROJECT

EASTCOURT PROPERTY GROUP
PIGTON FUEL STATION
LOT 603 MARTIN PELUSEY ROAD
PIGTON EAST
1920-014

SHEET

DRAWING LIST

REV. DESCRIPTION

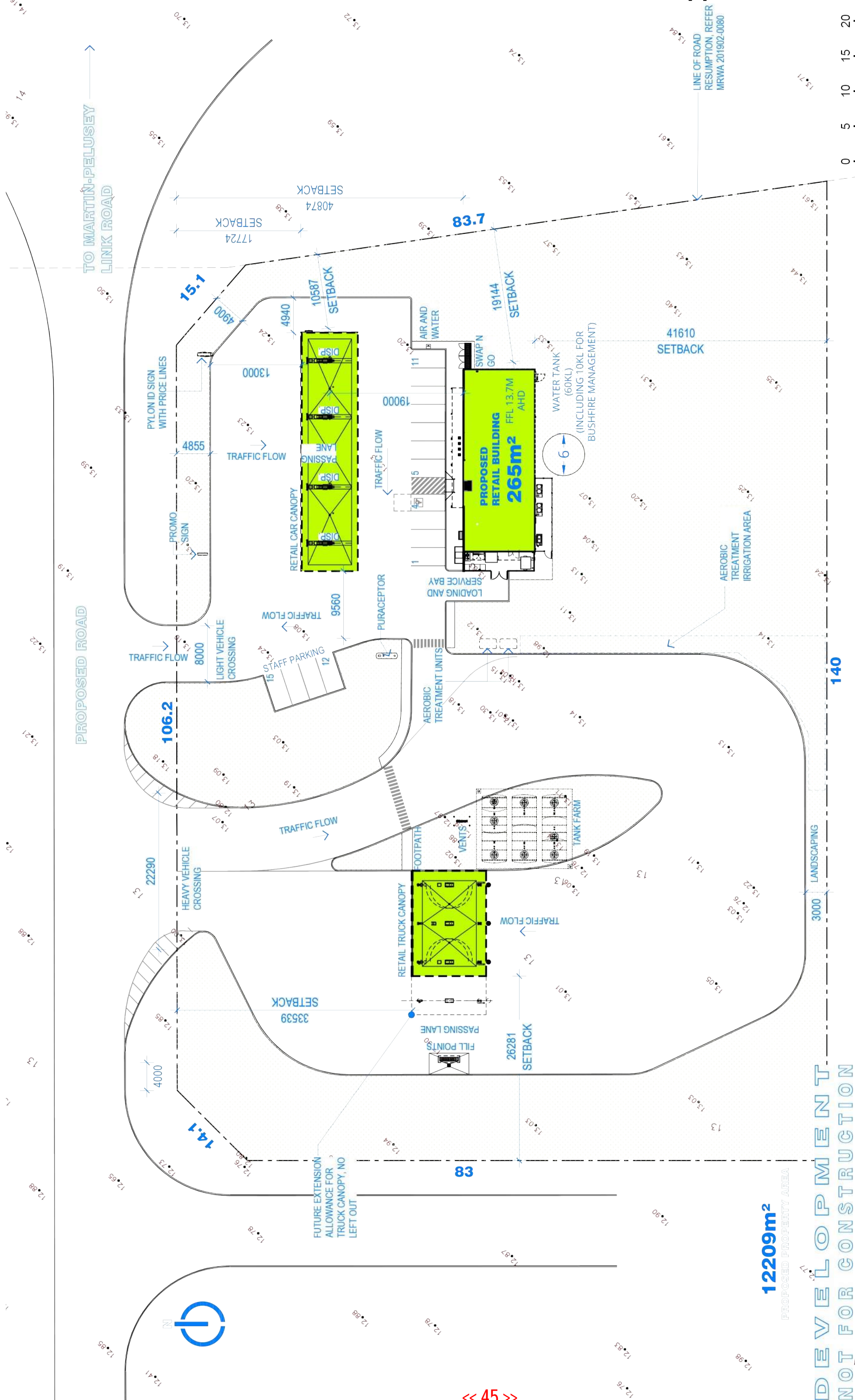
A Development Application

26.08.19

G001

NTS

A



A102
1:500
C

REV.	DESCRIPTION
A	Development Application
B	Carriageway increased to 10m, truck crossing and canopy moved east 4m
C	Truck canopy relocated, swept paths and kerbs revised to suit.

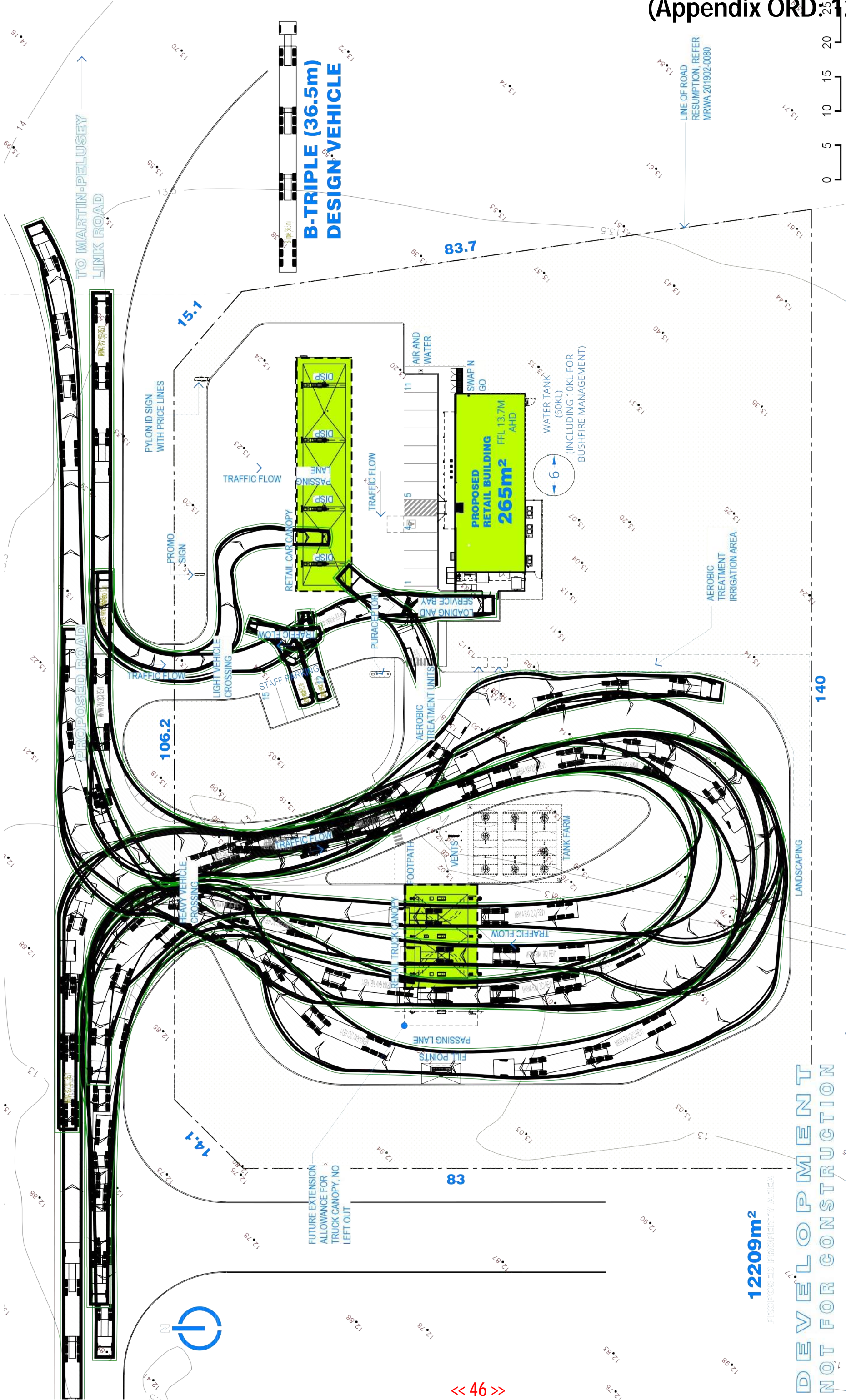
SHEET
PLAN
SITE
PROPOSED

PROJECT
EASTCOURT PROPERTY GROUP
PICOTON FUEL STATION
LOT 603 MARTIN PELUSEY ROAD
PICOTON EAST
1920-014

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DEVELOPMENT NOT FOR CONSTRUCTION



T1101
1:500 D

REV.	DESCRIPTION
A	Development Application
B	Boundary & roads revised to new concept plan, swept paths revised to suite
C	Carriageway increased to 10m, truck crossing and canopy moved east 4m
D	Truck canopy relocated, swept paths and kerbs revised to suit.

SHEET
PLAN
SITE
SWEEP PATHS

PROJECT
EASTCOURT PROPERTY GROUP
PICTON FUEL STATION
LOT 603 MARTIN PELUSEY ROAD
PICTON EAST
1920-014

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PROJECT
 EASTCOURT PROPERTY GROUP
 PICTON FUEL STATION
 LOT 603 MARTIN PELUSEY ROAD
 PICTON EAST
 1920-014

SHEET
 PLAN
 SITE
 LANDSCAPING

REV. DESCRIPTION

A	Development Application	28.08.19
B	Carriageway increased to 10m, truck crossing and canopy moved east 4m	05.12.19
C	Truck canopy relocated, swept paths and kerbs revised to suit.	27.05.20

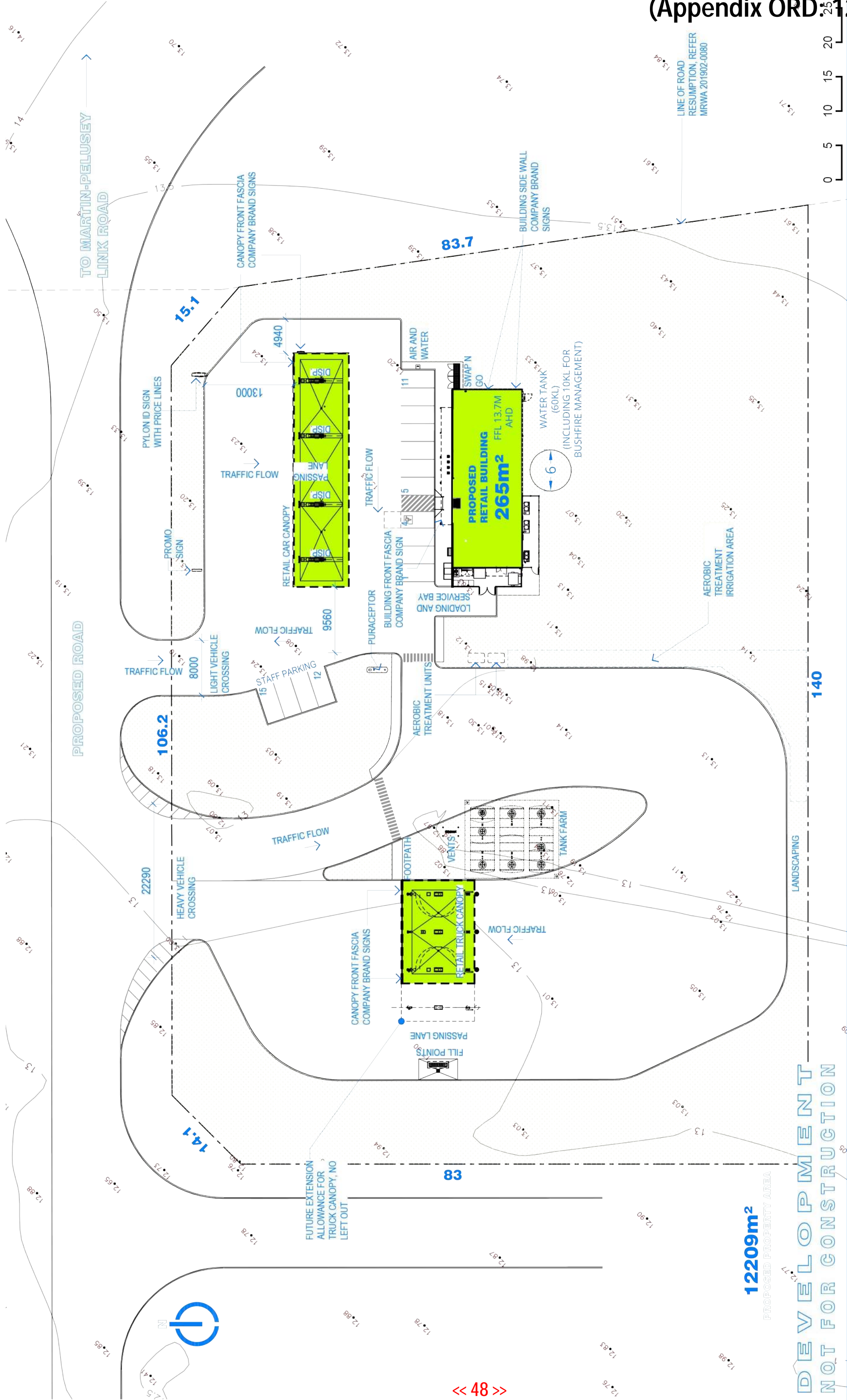
L101
1:500
C



TO MARTIN-PELUSEY LINK ROAD

09m²
 PROPERTY AREA

47



S101
1:500 | **C**

REV.	DESCRIPTION
A	Development Application
B	Carriageway increased to 10m, truck crossing and canopy moved east 4m
C	Truck canopy relocated, swept paths and kerbs revised to suit.

SHEET	DESCRIPTION
PLAN	
SITE	
SIGNAGE	

PROJECT
 EASTCOURT PROPERTY GROUP
 PICTON FUEL STATION
 LOT 603 MARTIN PELUSEY ROAD
 PICTON EAST
 1920-014

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(Appendix ORD: 12.4C)

From: NAUDE Daniel (RCPM) <Daniel.Naude@mainroads.wa.gov.au>
Sent: Monday, 16 March 2020 12:00 PM
To: Cecilia Muller
Subject: Reply: Proposed Roadhouse – LOT 603 (96) MARTIN PELUSEY ROAD, PICTON EAST

Dear Cecilia,

I refer to your recent correspondence regarding the proposed roadhouse at the above mentioned property and apologise for the delay in response.

Please be advised that Main Roads is not supportive of the proposal as it is considered that further strategic planning investigations is required to be undertaken for the locality, specifically regarding the primary road network. Comments regarding this aspect and recommendations regarding the proposed access configuration for the development are provided below.

The land is subject to a 'Primary regional road' planning reservation under the Greater Bunbury Region Scheme, as shown in red on the proposed development site plan. The land in question is also subject to the WAPC's endorsed Preston Industrial Park Southern Precinct District Structure Plan (PIPDSP). Road connectivity is provided to Martin Pelusey Road via an east / west road alignment along the southern common boundary with Lot 110. It appears that the intent of the road alignment depicted on the PIPDSP has been to facilitate staging and/or to coordinate internal road access between the lots in this sub-precinct and to Martin Pelusey Road.

The WAPC has also recently endorsed the Waterloo District Structure Plan (WDSP). The primary/integrator road network under the WDSP, in respect to the east / west connections to Martin Pelusey Road does, however, not integrate/align with the planned PIPDSP movement network. Main Roads, due to the severance impacts of the proposed BORR on local roads is proposing to realign and construct a number of local roads in the WDSP area. This includes provision for a roundabout on Martin Pelusey Road, which could provide the basis for consideration of a road access link (not a private access/driveway) from the subject land (and structure plan area) to the proposed roundabout, subject to DPLH/WAPC agreement and endorsement. Land areas associated with the roundabout on Martin Pelusey Road, as shown in blue on the proposed development site plan, is proposed to be acquired by Main Roads to construct the roundabout.

However, Main Roads does not at present intend to relinquish the existing GBRS reservation and is proposing to investigate and maintain a possible alignment for a district road link from Forrest Highway along the current BORR alignment (under the Greater Bunbury Region Scheme) over South Western Highway which could connect through the subject property to Martin Pelusey Road or via Columbus Drive. This could be fully or partly included within the current Primary Regional Road reservation under the Greater Bunbury Region Scheme.

Development in the manner and location proposed, and in the absence of further strategic planning for the locality, has the potential to prejudice further road planning/investigations which, in the opinion of Main Roads, needs to be reviewed for the locality.

It is noted that the proponent has submitted a draft local structure plan (Precinct 2A) , which is currently being progressed and broadly identifies land use and the primary/integrator road network. It is also noted that the proponent has also submitted a concept plan for subdivision of the overall lot and adjoining lots forming part of the sub-precinct, showing a second primary road connection through to Harris Road and other local roads. A Local Structure Plan has not however been approved

(Appendix ORD: 12.4C)

to guide future subdivision/development of the area. It is recommended that this process be concluded prior to approval of individual development applications. Revised development plans can then be prepared, if necessary, to address the requirements as outlined above.

Specific comment relating to the proposed Development Application:

It is noted access is proposed to be taken from a future road identified as an 'Integrator' road in the strategic planning framework for the locality which will provide primary access to the future industrial area. Multiple, and over-width crossovers in the manner proposed has the potential to create significant traffic conflict and safety issues as they present an uncontrolled form of access and egress to a road, particularly in relation to vehicle sight lines and 'masking' of through traffic. It is recommended that the crossover widths be reduced to address these aspects. There appears to be potential for the fuelling points be set back further and the width of the egress way/ crossover be reduced accordingly. To avoid conflict with future road investigations/requirements it is recommended that the proposed development be relocated outside to the existing Primary regional road reserve as a minimum.

The proposed development plan indicates a new local road linking to the proposed roundabout at Martin Pelusey Road. This new local road will need to be constructed by the proponent to facilitate access to the proposed development.

Given that the 'access road' is identified as an integrator-type road, it is recommended that no direct access be provided to the integrator road, rather a requirement be set for a side-road to be constructed and for access be taken from this side road. It is anticipated that the Shire would be imposing general access restrictions to the integrator road network to the east under the WDSP to maximise efficiency and safety on the road network, and it is considered appropriate for a similar level of access controls to be imposed for the integrator road west of Martin Pelusey, to avoid similar access issues to those on Harris Road from arising. The current design concept for the eastern leg of the Martin Pelusey Road under the WDSP includes an approximate 100 meter long raised median treatment to the roundabout.

Should the development be approved in its current form, it is recommended that the proposed driveway access/egress arrangements for the development be setback as far as possible, in the order of 300 metres from the proposed roundabout to minimise traffic impacts on the future road and operation of the roundabout. No access should be supported for the proposed development from Martin Pelusey Road.

If you have any queries please contact me on the number below.

Regards

Daniel Naude
ROAD CORRIDOR PLANNING MANAGER
Metropolitan and Southern Regions / South West
p: +61 9724 5724 | m: +61 4189 31078
w: www.mainroads.wa.gov.au



*We're working for
Western Australia.*





Government of **Western Australia**
Department of **Mines, Industry Regulation and Safety**

Your ref P105/19 - A3450 - DAP-R0699471

Our ref A0139/202001

Enquiries Steven Batty
9222 3104

Steven.BATTY@dmirs.wa.gov.au

Mr André Schönfeldt
Chief Executive Officer
Shire of Dardanup
Sent by Email : submissions@dardanup.wa.gov.au
Eaton WA 6232

Attention: Cecilia Muller

Dear Mr Schönfeldt

DEVELOPMENT APPROVAL - ROADHOUSE - LOT 603 96 MARTIN-PELUSEY ROAD PICTON EAST - SHIRE OF DARDANUP

Thank you for your letter dated 21 January 2020 inviting comment on the above proposed Development Approval - Roadhouse - Lot 603 96 Martin-Pelusey Road Picton East - Shire of Dardanup.

The Department of Mines, Industry Regulation and Safety has assessed this proposal with respect to mineral and petroleum resources, geothermal energy, and basic raw materials and makes the following comment(s).

- The eastern margin of the proposed Structure Plan - Precinct 2A Picton Industrial Park Southern Precinct is shown in the DMIRS Tengraph system as FNA 10611 - File Notation Area - Bunbury to Albany Gas Pipeline Investigation Corridor to Determine Final 50 m Alignment (Martin Pelusey Road Alternative). The FNA is currently a 200 m wide zone with the final 50 m pipeline corridor yet to be determined.
- It is noted that a proposed Primary Road in the strategic plan follows a similar route to the proposed Gas Pipeline along the eastern boundary of the Picton Industrial Park Southern Precinct. However on Figure 9013-CON-02_B – the Area marked as Primary Regional Road Reserve (Subject to Review) is in fact overlain by the footprint of the Roadhouse Development. We trust that the appropriate authorities regarding the pipeline investigation area and road reserve are being consulted.

Due to the planning approval involving installation of a service station we have forwarded your application to the Dangerous Goods Branch for their assessment.

000085.Steven.BATTY
Release Classification: - Addressee Use Only

Mineral House 100 Plain Street East Perth Western Australia 6004
Telephone +61 8 9222 3333 Facsimile +61 8 9222 3862
www.dmirs.wa.gov.au
ABN 69 410 335 356

Yours sincerely

Warren Ormsby

Warren Ormsby
Manager Land Use Planning
Minerals and Petroleum Resources Directorate
25 February 2020

From: WAPCreferrals <WAPCreferrals@pta.wa.gov.au>
Sent: Tuesday, 25 February 2020 9:26 AM
To: Kathleen Hoult <Kathleen.Hoult@dardanup.wa.gov.au>
Subject: RE: Ticket: EA2D1265E1542 Re: Development Application - 96 Martin-Pelusey

Hi Kathleen,

Thanks for your referral in regard to the proposed road house at Lot 603 Martin-Pelusey Road.

Given the distance between the proposed road house and the rail freight corridor PTA has no comments to make on this proposal.

Kind regards,

Andrew Grero

Land and Corridor Officer | IP&LS

Public Transport Authority of Western Australia
Public Transport Centre, West Parade, Perth, 6000
PO Box 8125, Perth Business Centre, WA, 6849
Tel: (08) 9326 2374

Email: wapcreferrals@pta.wa.gov.au | Web: www.pta.wa.gov.au



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Department of
**Primary Industries and
Regional Development**

Your reference: P105/19
Our reference: LUP 767
Enquiries: Leon van Wyk

Chief Executive Officer
Shire of Dardanup
PO Box 7016
EATON WA, 6232
submissions@dardanup.wa.gov.au

11 February 2020

Dear Mrs Muller

Application for Development Approval - Roadhouse - Lot 603 (96) Martin-Pelusey Road, Picton East

Thank you for the opportunity to comment on the application for a Roadhouse at Lot 603 (96) Martin-Pelusey Road, Picton East.

The Department of Primary Industries and Regional Development (DPIRD) does not object to the proposed Roadhouse at the abovementioned lot as this area has previously been identified for Industrial use in the *Greater Bunbury Region Scheme (GBRS)*.

If you have any queries regarding the comments, please contact Leon van Wyk at (08) 9780 6171 or leon.vanwyk@dpird.wa.gov.au.

Yours sincerely

A handwritten signature in blue ink that reads 'Melanie Strawbridge'.

Dr Melanie Strawbridge
**Director Agriculture Resource Management Assessment
Sustainability and Biosecurity**

3 Baron-Hay Court, South Perth 6151
Locked Bag 4 Bentley Delivery Centre 6983
Telephone +61 (0)8 9368 3333 enquiries@dpird.wa.gov.au
dpird.wa.gov.au

ABN: 18 951 343 745



Department of Planning,
Lands and Heritage

GOVERNMENT OF
WESTERN AUSTRALIA



Your ref: P105/19
Our ref: PLH2018P0093
Doc ref:
Enquiries: Ben Müller

Shire of Dardanup
PO Box 7016
EATON WA 6232

Dear Cecilia Müller

DEVELOPMENT APPLICATION – PROPOSED ROADHOUSE – LOT 603 MARTIN-PELUSEY ROAD, PICTON EAST

I refer to your letter dated 21 January 2020 requesting the Department of Planning, Land and Heritage (the Department) to comment on the above proposal.

Greater Bunbury Region Scheme

The following are noted under the Greater Bunbury Region Scheme (GBRS):

- Lot 603 Martin-Pelusey Road is partly zoned Industrial Deferred and partly reserved for Primary Regional Roads purposes;
- The lot is further situated within the Priority Agricultural Land Policy buffer area; and
- A Local Structure Plan (LSP) for the site has been drafted but is currently not endorsed.
- **A Development Application therefore under the GBRS is currently premature and should be deferred until such time as the LSP has been endorsed by the WAPC and the lifting of the Industrial Deferred Zone has been completed.**

The following assessment of the GBRS development application has been included with the understanding that the finalisation thereof will be after the LSP has been endorsed by the WAPC and the lifting of the Industrial Deferred Zone has been finalised.

Under Clause 24 of the GBRS, a person must not commence or carry out development of a kind or class specified in a resolution made by the Commission under Clause 27, unless that person has planning approval.

Under Clause 27 of the GBRS there are a number of scenarios under which a development application is required. The development on Lot 603 has been assessed in accordance with the provisions of the GBRS, and in particular with reference to the Notice of Resolution made under *Clause 27, Schedule 1, Paragraph 3: Land abutting a Regional Road Reserve and Paragraph 6: Development within the Priority Agricultural Land Policy area.*

- Under Paragraph 3, certain development types on land abutting a Regional Road Reserve requires planning approval under the GBRS except for a few exclusions. In this particular instance the proposed development is not excluded from obtaining planning approval under this section of the GBRS.
- Under Paragraph 6, a GBRS application can potentially be triggered due to the property's location within the GBRS Priority Agricultural Land Policy Area however in this case the proposed development is considered a compatible development under the policy. A GBRS application under Paragraph 6 is therefore not required.

On 22 April 2014, pursuant to section 16 of the Act, the WAPC resolved to DELEGATE to local governments, and to members and officers of those local governments, its functions in respect of the determination, in accordance with Parts 7 and 8 of the GBRS, of applications for approval to commence and carry out development as specified in schedules 1 and 2, within their respective districts, subject to the terms set out in schedule 3;

Under Schedule 2, Paragraph 2, development on land abutting Primary Regional Road Reserves, which requires planning approval under the GBRS is delegated to the local authority where -

- (a) the local government accepts the recommendation and any advice from Main Roads Western Australia, or
- (b) development for which the local government decides to refuse approval under the GBRS.

Further under Clause 24 of the GBRS, with regards to the encroachment onto the existing Primary Regional Road reserve, a person must not commence or carry out development unless that person has planning approval.

Under section 16 of the Planning and Development Act 2005 (the Act) the Western Australian Planning Commission (the WAPC) may, by resolution published in the Government Gazette, delegate any function to a local government, a committee established under the Local Government Act 1995 or an employee of a local government.

On 22 April 2014, pursuant to section 16 of the Act, the WAPC resolved to DELEGATE to local governments, and to members and officers of those local governments, its functions in respect of the determination, in accordance with Parts 7 and 8 of the GBRS, of applications for approval to commence and carry out development as specified in schedules 1 and 2, within their respective districts, subject to the terms set out in schedule 3;

This application has been assessed in accordance with the provisions of the GBRS, and in particular with reference to the Instrument of Delegation, *Schedule 1, Paragraph 1 and Schedule 3, Paragraph 4.*

- Under Schedule 1, Paragraph 1, development on land in a regional road reservation which requires planning approval under the GBRS is delegated to the local government where the local government accepts the recommendation and any advice of the advice agency.
- Schedule 3, Paragraph 4 stipulates that an application for development in or abutting a primary regional road reservation shall be referred within seven days to Main Roads Western Australia (MRWA) for advice and recommendation before being considered by the local government.

The proposed development on Lot 603 and within the current Primary Regional Roads Reserve must therefore be referred to Main Roads WA for advice.

Subject to the above, delegation to determine the GBRS application will be with the Shire of Dardanup after the LSP has been endorsed by the WAPC and the lifting of the Industrial Deferred Zone has been finalised.

Should you have any further queries please contact Ben Müller on 9791 0577.

Yours sincerely



Marion Dandridge
A/Planning Director
Regional South West

5 February 2020



Our Ref: AG:P12404-09
Your Ref: P105/19
Enquiries: Michelle Hayward
Hayley Daff
☎ (08) 9780 9517

30 January 2020

Chief Executive Officer
Shire of Dardanup
PO Box 7016
EATON WA 6232

Dear Mr Schonfeldt,

DEVELOPMENT APPROVAL – ROADHOUSE – LOT 603 (96) MARTIN-PELUSEY ROAD, PICTON EAST

In regards to the abovementioned Development Approval, Aqwest wish to advise the following condition(s) will apply:

- Please be advised that Aqwest has no comments in relation to the above application.

Yours faithfully

A handwritten signature in blue ink, appearing to read "Adam Gornall".

Adam Gornall
COORDINATOR WATER DISTRIBUTION

Development Services 629 Newcastle Street Leederville WA 6007 PO Box 100 Leederville WA 6902 T (08) 9420 2099 F (08) 9420 3193



Your Ref: P105/19
Our Ref: 57125495 (DEV365311)
Enquiries: Brett Coombes
Direct Tel: 9420 3165
Fax: 9420 3193

30 January 2020

Chief Executive Officer
Shire of Dardanup
P.O. Box 7016
EATON WA 6232

Attention: Mrs Cecelia Muller, Principal Planning Officer

Development - Roadhouse Lot 603 Martin Pelusey Rd, Picton East

Thank you for your letter of 21 January 2020 inviting comments on the above development application.

The site is remote from Water Corporation water or wastewater infrastructure. It is assumed that the proponents have approached AqWest regarding water servicing and that wastewater will be treated and disposed on site.

The Water Corporation manages several rural drains in the Picton area which will be impacted by the planned industrial development. Based on current ground levels, the land drains generally in a south-west direction towards the Picton Main Drain (see attached plan).

The overall drainage strategy for the proposed structure plan area needs to be addressed through the assessment and finalisation of the LWMS which accompanies the structure plan. In this regard it may be premature to approve the roadhouse development ahead of the finalisation of the LWMS. In the event that the roadhouse is granted approval, appropriate drainage conditions should be applied to ensure that the finished levels of the site and the on-site drainage volumes accord with the approved LWMS.

The Water Corporation will provide more detailed advice on the LWMS and the structure plan in due course.

If you have any queries, please contact me on Tel. 9420-3165.

A handwritten signature in black ink that reads "B. Coombes".

Brett Coombes
Senior Urban Planner
Development Services



(Appendix ORD: 12.4C)

From: Cathie Derrington <cathie.derrington@dwer.wa.gov.au>
Sent: Friday, 24 January 2020 10:50 AM
To: Cecilia Muller <Cecilia.Muller@dardanup.wa.gov.au>; Records <records@dardanup.wa.gov.au>
Cc: Krish Seewraj <krish.seewraj@dwer.wa.gov.au>
Subject: APPLICATION FOR DEVELOPMENT APPROVAL, ROADHOUSE – LOT 603 (96) MARTIN-PELUSEY ROAD, PICTON EAST

DATE: 24 January 2020

YOUR REF: P105/19

OUR REF: DWERT50~31, PA031934

TO: Shire of Dardanup

FROM: Department of Water and Environmental Regulation

Attention: Cecilia Muller
cecilia.muller@dardanup.wa.gov.au
records@dardanup.wa.gov.au

Dear Cecilia

APPLICATION FOR DEVELOPMENT APPROVAL, ROADHOUSE – LOT 603 (96) MARTIN-PELUSEY ROAD, PICTON EAST

Thank you for providing the Application for Development Approval for a Roadhouse, Lot 603 Martin-Pelusey Road, Picton East to the Department of Water and Environmental Regulation (Department) to consider.

The Department has identified that the Application for Development Approval for a Roadhouse, Lot 603 Martin-Pelusey Road, Picton East may impact on environment and / or water resource values and/or management. However the Department cannot assess the referral application at this time due to a lack of supportive information. Therefore the department objects to the proposal in its current form. Key information that is required by the department to assess this proposal are provided below. These matters are similar to the comments DWER has made on the Local Water Management Strategy for Lots 103, 110 and 603 Picton East. These matters must be addressed to the satisfaction of the Department:

1. Detailed information on sewerage treatment and management, including a nutrient management plan if irrigation of nutrient rich wastewater is proposed.
2. Demonstrating that the Department of Health have been consulted in the sewerage treatment and management.
3. Detailed information on the compliance of sewerage treatment with the Government Sewerage Policy (WAPC 2019).
4. Provide a site and soil evaluation to support the roadhouse proposal.
5. Detailed information on the highest known groundwater level for the site.
6. Detailed information on land filling and / or subsurface drainage. Including the demonstration of how surface and groundwater will be managed to meet future subdivision drainage configurations including all controls, receiving systems and proposed systems under the Local Water Management Strategy.
7. Provide approvals from the Water Corporation for any connection of drainage to the existing drainage networks.
8. Demonstrate water supply source and yearly water requirements.
9. Demonstrate the proposals meets best management practices including:
 - Water Quality Protection Note 56- Tanks for fuel and chemical storage near sensitive water resources (DWER 2018)
https://www.water.wa.gov.au/data/assets/pdf_file/0010/5131/82577.pdf and
 - Water Quality Protection Note 49 - Service Stations (DoW 2013)
https://www.water.wa.gov.au/data/assets/pdf_file/0005/4955/106040.pdf

In the event there are modifications to the proposal that may have implications on aspects of environment and/or water management, the Department should be notified to enable the implications to be assessed.

Should you require any further information on the comments please contact Cathie Derrington on 97264184.

Yours faithfully,

(Appendix ORD: 12.4C)

Cathie Derrington

Environmental Officer

Planning Advice /CAWS /WWC - South West Region

Department of Water and Environmental Regulation

35-39 McCombe Road, BUNBURY, WA 6230

PO Box 261, BUNBURY, WA 6231

T: (08) 9726 4184 | F: (08) 9726 4100 | Ext: 1184

E: cathie.derrington@dwer.wa.gov.au | www.dwer.wa.gov.au

Twitter: [@DWER_WA](https://twitter.com/DWER_WA)

Disclaimer: This e-mail is confidential to the addressee and is the view of the writer, not necessarily that of the Department of Water and Environmental Regulation, which accepts no responsibility for the contents. If you are not the addressee, please notify the Department by return e-mail and delete the message from your system; you must not disclose or use the information contained in this email in any way. No warranty is made that this material is free from computer viruses.

(Appendix ORD: 12.4C)

From: Snellin, Fiona <Fiona.Snellin@atco.com>
Sent: Thursday, 30 January 2020 2:02 PM
To: Submissions Planning <Submissions@dardanup.wa.gov.au>
Subject: LM20033_Development Approval_Lot 603_No. 96 Martin-Pelusey Rd Picton East_Shire of Dardanup

For the attention of Cecelia Muller

Good afternoon Cecelia,

RE: Application for Development Approval
Roadhouse – Lot 603 (96) Martin-Pelusey Road, Picton East
Our Reference: LM20033

Thank you for your recent correspondence, dated 21 January 2020, to ATCO Gas Australia Pty Ltd (ATCO) regarding the above mentioned development application.

ATCO Gas Australia (ATCO) has no objection to the proposed development application and change in landuse, based on the information and plan provided, to facilitate the development within Lot 603 Martin-Pelusey Road, Picton East for the purpose of :

- A development area of approximately 9769m²
- Retail building with a floor space of 265m²
- Retail car canopy
- Retail truck canopy
- Tank farm
- 15 car parking bays
- Poly ID sign and promo sign

ATCO's closest asset (a high pressure gas pipeline_DN150ST HP 1900kPa) is approximately 30 metres to the east within an easement within the properties adjacent to and on the opposite side of Martin-Pelusey Road.

Please accept this email as ATCO's written response.

Should you have any queries regarding the information above Cecelia, please contact us on 6163 5000 or eservices@atco.com.

Kind regards

Fiona Snellin
Land Management Coordinator
Gas, Australia

E. Fiona.Snellin@atco.com
Mobile 0476 831 540
A. 81 Prinsep Road, Jandakot WA 6164
P. Locked Bag 2, Bibra Lake DC, WA, 6965
W. atcogas.com.au



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Our Ref: D13359
Your Ref: STP-R0700046

Suzanne Occhipinti
Shire of Dardanup
submissions@dardanup.wa.gov.au

Dear Ms Occhipinti

RE: PRECINCT 2A PICTON INDUSTRIAL PARK SOUTHERN PRECINCT - LOTS 103 AND 110 HARRIS RD, AND LOT 603 (96) MARTIN – PELUSEY ROAD, PICTON EAST - LOCAL STRUCTURE PLAN

I refer to your email dated 23 January 2020 regarding the submission of a Bushfire Management Plan (BMP) (Version A), prepared by Emerge and dated 13 November 2019, for the above Local Structure Plan. The BMP was included as technical appendix 4 in the report titled 'Local Structure Plan Precinct 2a – Picton Industrial Park Southern Precinct' dated 17 January 2020 prepared by Rowe Group Design.

It should be noted that this advice relates only to *State Planning Policy 3.7 Planning in Bushfire Prone Areas* (SPP 3.7) and the *Guidelines for Planning in Bushfire Prone Areas* (Guidelines). It is the responsibility of the proponent to ensure that the proposal complies with all other relevant planning policies and building regulations where necessary. This advice does not exempt the applicant/proponent from obtaining necessary approvals that may apply to the proposal including planning, building, health or any other approvals required by a relevant authority under other written laws.

Assessment

1. Policy Measure 6.3 a) (ii) Preparation of a BAL Contour Map

Issue	Assessment	Action
BMP administration	Footnote 1 of the BMP (refer to page 29) is not supported by DFES and should be removed from the BMP to ensure accuracy. The definition 'development site' cannot be applied at this level of planning, as it relates to development applications associated with 78B (1) (a) or (b) of the Planning and Development (Local Planning Schemes) Regulations 2015.	Modification required.

(Appendix ORD: 12.4C)

Vegetation Exclusions	Drainage areas – insufficient information The BMP has excluded vegetation associated with drainage areas (as depicted in the Concept Plan on page 31 of the Structure Plan report and Plot 19 within Figure 4 of the BMP) as being maintained as low threat vegetation as per AS3959. No evidence is provided to justify the vegetation exclusion. The BMP assumes the Shire of Dardanup (Shire) will maintain these areas as low threat vegetation (refer to 3.1.1.1 of the BMP). However, it is unclear if responsibility for management of these areas has been accepted by the Shire. Should these areas be left unmanaged or subject to any revegetation, the identified BAL ratings impacting adjacent lots may be inaccurate. Further refinement of this exclusion will be required at subsequent planning stages (subdivision & development).	Decision maker to be satisfied with vegetation exclusion of POS areas.
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Recommendation – compliant application

At the structure planning stage, consideration should be given to the intensification of land use and how this relates to identified bushfire hazards. DFES is satisfied that the bushfire hazard level assessment has adequately identified the bushfire risk and considered how compliance with the bushfire protection criteria can be achieved at subsequent planning stages.

Notwithstanding the above, modifications as indicated in the above table(s) to the BMP are required prior to subsequent planning stages to ensure compliance with the bushfire protection criteria. As these modifications are minor in nature and will not affect the LSP, these modifications should be undertaken to support subsequent stages of the planning process (subdivision & development applications).

If you require further information, please contact me on telephone number 6551 4032.

Yours sincerely



Craig Scott
LAND USE PLANNING OFFICER

3 March 2020



Mr Mark Chester
Chief Executive Officer
Shire of Dardanup
PO Box 7016
EATON WA 6232

Via email to: submissions@dardanup.wa.gov.au

Attention: Mrs Cecilia Muller

Dear Mr Chester

PROPOSED ROADHOUSE – LOT 603 (#96) MARTIN-PELUSEY ROAD, PICTON EAST

Thank you for your letter of 21 January 2020 requesting comments from the Department of Health (DOH) on the above proposal. The DOH provides the following comment:

Water Supply and Wastewater Disposal

The development is to be in accordance with the *Government Sewerage Policy (2019)*. For non-scheme water connected areas, the development is to have access to a sufficient supply of potable water that is of the quality specified under the *Australian Drinking Water Quality Guidelines 2011*. The necessary requirements may be referenced and downloaded from:

http://ww2.health.wa.gov.au/Articles/A_E/Drinking-water-quality-management

http://ww2.health.wa.gov.au/Articles/A_E/Drinking-water-guidelines-and-standards

On-Site Wastewater Disposal

Suitable provision for an adequate onsite effluent disposal area is to be accommodated in any planning approval. For on-site wastewater disposal systems to be approved, a winter site-and-soil evaluation is required and be in accordance with Australian New Zealand Standard 1547.

For more details please refer to the attached fact sheet '*Guidance on Site-and-soil evaluation for Onsite Sewage Management*'. The fact sheet also provides information on the stages in the planning process and scale of development and determines the timing and the level of detail of investigation and reporting.

Food Act Requirements

All food related areas such as kitchen, preparation areas, etc. are to comply with the provisions of the *Food Act 2008* and related code, regulations and guidelines. Details available for download from:

http://ww2.health.wa.gov.au/Articles/F_I/Food-regulation-in-WA

Should you require further information please contact Vic Andrich on 9222 2000 or at ehinfo@health.wa.gov.au

Yours sincerely



Dr Michael Lindsay
**A/EXECUTIVE DIRECTOR
ENVIRONMENTAL HEALTH DIRECTORATE**

6 March 2020

Att.



Government of **Western Australia**
Department of **Mines, Industry Regulation and Safety**
Resources Safety

Your ref: P105/19
Our ref: A0139/202001
Enquiries: Eric Gruber - Ph 9358 8037
Email: eric.gruber@dmirs.wa.gov.au

CECILIA MULLER
Principal Planning Officer
PO Box 500
Collie WA 6225

Dear Cecilia,

DEVELOPMENT APPLICATION – ROADHOUSE LOT 603 (96) MARTIN-PELUSEY ROAD, PICTON EAST

A request for comment was received from the Shire of Dardanup in relation to the above planning application and the proposal to establish a Roadhouse, at the above address.

The establishment of a Roadhouse will require application for a Dangerous Goods Storage licence before dangerous goods can be stored on site. There is significant detail relating to the proposed Dangerous Goods and their storage, which is not included in the documents that make further comment difficult until a licence application is submitted. The specific detail relating to the Dangerous Goods Site Licence application will be assessed against relevant Australian Standards for compliance.

If you have any queries regarding this letter, please contact me on 9358 8037.

Yours faithfully,

Eric Gruber

Eric Gruber
Team Leader Dangerous Goods
Dangerous Goods and Critical Risks Directorate
04 March 2020

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
1. Department of Mines, Industry Regulation and Safety 26/02/2020	<p>The Department of Mines, Industry Regulation and Safety has assessed this proposal with respect to mineral and petroleum resources, geothermal energy, and basic raw materials and makes the following comment(s).</p> <p>a. The eastern margin of the proposed Structure Plan - Precinct 2A Picton Industrial Park Southern Precinct is shown in the DMIRS Tengraph system as FNA 10611 - File Notation Area - Bunbury to Albany Gas Pipeline Investigation Corridor to Determine Final 50 m Alignment (Martin Pelusey Road Alternative). The FNA is currently a 200 m wide zone with the final 50 m pipeline corridor yet to be determined.</p>	<p>Applicant Comments</p> <p>Noted. The Pipeline Investigation Corridor was acknowledged in the Structure Plan. It is an investigation area only and does not prevent progress of the Structure Plan as proposed. Future development would be assessed having regard to any final corridor alignment should that eventuate.</p> <p>Officer Comments</p> <p>Noted. Officers are not supportive of the proposed development located within the Primary Regional Road Reservation. An objection was received from Main Roads WA. The objection is discussed in the "Officer Comment" section of the Council report.</p>
2. Public Transport Authority Western Australia 25/02/2020	<p>b. It is noted that a proposed Primary Road in the strategic plan follows a similar route to the proposed Gas Pipeline along the eastern boundary of the Picton Industrial Park Southern Precinct. However on Figure 9013-CON-02_B – the Area marked as Primary Regional Road Reserve (Subject to Review) is in fact overlain by the footprint of the Roadhouse Development. We trust that the appropriate authorities regarding the pipeline investigation area and road reserve are being consulted.</p> <p>No comments</p>	<p>Applicant Comments</p> <p>The final role and function of Martin-Pelusey Road will be determined by DPLH and MRWA though the Structure Plan and Traffic Impact Statement accompanying acknowledge this. Refer comments below in relation to MRWA response, and the related Uloth & Associates advice in reply.</p> <p>Officer Comments</p> <p>The proposal was referred to Main Roads WA and an objection was received. The objection is discussed in the "Officer Comment" section of the Council report.</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
3. Department of Primary Industries and Regional Development 11/02/2020	No comments	Noted
4. Department of Planning, Lands and Heritage 05/02/2020	<p>Greater Bunbury Region Scheme</p> <p>The following are noted under the Greater Bunbury Region Scheme (GBRS):</p> <ol style="list-style-type: none"> a. Lot 603 Martin-Pelusey Road is partly zoned Industrial Deferred and partly reserved for Primary Regional Roads purposes; b. The lot is further situated within the Priority Agricultural Land Policy buffer area; and c. A Local Structure Plan (LSP) for the site has been drafted but is currently not endorsed. d. A Development Application therefore under the GBRS is currently premature and should be deferred until such time as the LSP has been endorsed by the WAPC and the lifting of the Industrial Deferred Zone has been completed. <p>The following assessment of the GBRS development application has been included with the understanding that the finalisation thereof will be after the LSP has been endorsed by the WAPC and the lifting of the Industrial Deferred Zone has been finalised.</p> <ol style="list-style-type: none"> e. Under Clause 24 of the GBRS, a person must not commence or carry out development of a kind or class specified in a resolution made by the Commission under Clause 27, unless that person has planning approval. f. Under Clause 27 of the GBRS there are a number of scenarios under which a development application is required. The development on Lot 603 has been assessed in accordance with the provisions of the GBRS, and in particular with reference to the Notice of Resolution made under Clause 27, Schedule 1, Paragraph 3: Land abutting a Regional Road Reserve and 	<p>Applicant Comments</p> <p>Noted. The application seeks approval to a use capable of being approved under the <u>existing</u> GBRS and TPS zoning. This does not appear to have been acknowledged.</p> <p>Application for Approval to Commence Development under the <u>existing</u> zoning of the land is provided for under the TPS and does not require prior Structure Planning.</p> <p>The DPLH confirms later in its advice with respect to the Priority Agricultural Land Policy that “...in this case the <i>proposed development is considered a compatible development under the policy...</i>”</p> <p>The application and associated Structure Plan adequately demonstrate the proposed use and site are not affected by any proposal contemplated by the Structure Plan. In short, the approval to a Roadhouse use at this time, on the site proposed will not prejudice the orderly and proper planning for the area for industrial purposes. The development application is equally also consistent with the existing zoning of the land and capable of approval.</p> <p>In e. to n. the DPLH provides standard advice regarding the process for delegation and/or assessment of an Application for Approval to Commence Development under the GBRS. These comments are noted as standard and known. They do not preclude the application from assessment and approval as proposed.</p> <p>Officer Comments</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
	<p>Paragraph 6: Development within the Priority Agricultural Land Policy area.</p> <p>g. Under Paragraph 3, certain development types on land abutting a Regional Road Reserve requires planning approval under the GBRs except for a few exclusions. In this particular instance the proposed development is not excluded from obtaining planning approval under this section of the GBRs.</p> <p>h. Under Paragraph 6, a GBRs application can potentially be triggered due to the property's location within the GBRs Priority Agricultural Land Policy Area however in this case the proposed development is considered a compatible development under the policy. A GBRs application under Paragraph 6 is therefore not required.</p> <p>i. On 22 April 2014, pursuant to section 16 of the Act, the WAPC resolved to DELEGATE to local governments, and to members and officers of those local governments, its functions in respect of the determination, in accordance with Parts 7 and 8 of the GBRs, of applications for approval to commence and carry out development as specified in schedules 1 and 2, within their respective districts, subject to the terms set out in schedule 3;</p> <p>j. Under Schedule 2, Paragraph 2, development on land abutting Primary Regional Road Reserves, which requires planning approval under the GBRs is delegated to the local authority where -</p> <ul style="list-style-type: none"> • the local government accepts the recommendation and any advice from Main Roads Western Australia, or <p>(b) development for which the local government decides to refuse approval under the GBRs.</p> <p>k. Further under Clause 24 of the GBRs, with regards to the encroachment onto the existing Primary Regional Road reserve, a person must not commence or carry out development unless that person has planning approval.</p> <p>l. Under section 16 of the Planning and Development Act 2005 (the Act) the Western Australian Planning Commission (the WAPC) may, by resolution published in the Government Gazette, delegate any function to a local government, a committee</p>	<p>The land is zoned 'Industrial Deferred' and partly reserved for Primary Regional Roads purposes under the GBRs. Approval is required under the GBRs. The submission from the DPLH indicated that a development application under the GBRs is premature and should be deferred until such time as the Local Structure Plan has been endorsed by the WAPC and the lifting of the 'Industrial Deferred' zone has been completed.</p> <p>Access to the Roadhouse is to be obtained from a link road via the proposed new roundabout in Martin Pelusey Road. The link road is identified on the Local Structure Plan that is yet to be considered and approved. Therefore development such as the Roadhouse which is dependent of the indicative link road for access should not be approved prior to the consideration and approval of the LSP by the WAPC.</p> <p>The proposed development on the part of the lot that is reserved for Primary Regional Road is not in line with the purpose of the reserve and not supportable.</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
	<p>established under the Local Government Act 1995 or an employee of a local government.</p> <p>m. On 22 April 2014, pursuant to section 16 of the Act, the WAPC resolved to DELEGATE to local governments, and to members and officers of those local governments, its functions in respect of the determination, in accordance with Parts 7 and 8 of the GBRs, of applications for approval to commence and carry out development as specified in schedules 1 and 2, within their respective districts, subject to the terms set out in schedule 3;</p> <p>n. This application has been assessed in accordance with the provisions of the GBRs, and in particular with reference to the Instrument of Delegation, Schedule 1, Paragraph 1 and Schedule 3, Paragraph 4.</p> <ul style="list-style-type: none"> • Under Schedule 1, Paragraph 1, development on land in a regional road reservation which requires planning approval under the GBRs is delegated to the local government where the local government accepts the recommendation and any advice of the advice agency. • Schedule 3, Paragraph 4 stipulates that an application for development in or abutting a primary regional road reservation shall be referred within seven days to Main Roads Western Australia (MRWA) for advice and recommendation before being considered by the local government. <p>o. The proposed development on Lot 603 and within the current Primary Regional Roads Reserve must therefore be referred to Main Roads WA for advice.</p> <p>p. Subject to the above, delegation to determine the GBRs application will be with the Shire of Dardanup after the LSP has been endorsed by the WAPC and the lifting of the Industrial Deferred Zone has been finalised.</p>	
5. Aqwest 30/01/2020	No comments	Noted.

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
<p>6. Water Corporation 30/01/2020</p>	<p>a. The site is remote from Water Corporation water or wastewater infrastructure. It is assumed that the proponents have approached AqWest regarding water servicing and that wastewater will be treated and disposed on site.</p> <p>b. The Water Corporation manages several rural drains in the Picton area which will be impacted by the planned industrial development. Based on current ground levels, the land drains generally in a south-west direction towards the Picton Main Drain (see attached plan).</p>	<p>Applicant Comment</p> <p>On-site effluent disposal is proposed for the proposed development in the absence of reticulated sewer being available.</p> <p>Office Comment</p> <p>Under separate legislation, the prescribed Treatment of Sewage application and the required fee is to accompany the building licence application for assessment by Environmental Health Services.</p> <p>Applicant Comments</p> <p>Noted. This is identified within the LWMS, which provides guidance on management of stormwater, groundwater and wastewater.</p> <p>As a temporary measure (i.e. until the full development of the area is progressed), development will be required to demonstrate that an appropriate basin has been provided within the subject land to hold (i.e. retain) stormwater from a 1 % AEP event before it is allowed to overland flow across existing terrain towards the drain.</p> <p>This can be addressed as a condition of development approval.</p> <p>Officer Comment</p> <p>Noted. If approved, a condition of approval is to be included requiring stormwater to be retained within the identified development area on Lot 603.</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
	<p>c. The overall drainage strategy for the proposed structure plan area needs to be addressed through the assessment and finalisation of the LWMS which accompanies the structure plan. In this regard it may be premature to approve the roadhouse development ahead of the finalisation of the LWMS. In the event that the roadhouse is granted approval, appropriate drainage conditions should be applied to ensure that the finished levels of the site and the on-site drainage volumes accord with the approved LWMS.</p>	<p>Noted. The LWMS is being updated through the LSP process and addressing comments raised by the various agencies, including Water Corporation comments. Addressing the requirements of the LWMS can be a condition of development approval in the normal manner.</p> <p>Officer Comment Noted that Water Corporation states that it may be premature to approve the roadhouse development ahead of the finalisation of the LWMS which forms part of the Local Structure Plan consideration.</p> <p>The applicant has amended the Development Plans and added a notation showing the Finished Floor Level FFL for the proposed retail building as being 13.7m AHD allowing the appropriate pad level above the anticipated site level (currently at 13m AHD and forecast in the LWMS to be at 12.9m AHD recognising the slight shaping of the site that will occur to ensure general drainage of the property following development).</p> <p>If approved, development would need to be conditioned to ensure the implementation of the approved LWMS at development stage.</p>
	<p>d. The Water Corporation will provide more detailed advice on the LWMS and the structure plan in due course.</p>	<p>Noted.</p>
<p>7. Department of Water and Environment Regulation (Environmental Officer) 24/01/2020</p>	<p>The Department has identified that the Application for Development Approval for a Roadhouse, Lot 603 Martin-Pelusey Road, Picton East may impact on environment and / or water resource values and/or management. However the Department cannot assess the referral application at this time due to a lack of supportive information. Therefore the department objects to the proposal in its current form. Key information that is required by the department to assess this proposal are provided below. These matters are similar to the comments DWER has made on the Local Water Management</p>	

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
	<p>Strategy for Lots 103, 110 and 603 Picton East. These matters must be addressed to the satisfaction of the Department</p> <ol style="list-style-type: none"> Detailed information on sewerage treatment and management, including a nutrient management plan if irrigation of nutrient rich wastewater is proposed. 	<p>Applicants Comment</p> <p>Demonstration that the wastewater system is appropriate can be addressed as a condition of development approval, and would be required as part of addressing the <i>Health (Treatment of Sewage and Disposal of Effluent and Liquid Waste) Regulations 1974</i>, which is a standard requirement for the approval of onsite wastewater disposal systems. If irrigation of nutrient rich wastewater is required, this is also required to be addressed as part of addressing the <i>Health (Treatment of Sewage and Disposal of Effluent and Liquid Waste) Regulations 1974</i>.</p> <p>Officer Comment</p> <p>If the development is approved, the effluent system will require application to install. A report from Shire Environmental Health Officer will need to be submitted with an application to Department of Health for issue. Flowrate information will be required for design purposes, and measurement and reporting of actual flowrates is recommended to ensure application rates are within recommended ranges. This information is usually provided to the applicant in the form of an advice note.</p>
	<ol style="list-style-type: none"> Demonstrating that the Department of Health have been consulted in the sewerage treatment and management. 	<p>Applicant Comment</p> <p>Noted. DOH have been consulted, as per their provision of comments.</p> <p>Officer Comment</p> <p>Same as Officer Comment in 1. above</p>
	<ol style="list-style-type: none"> Detailed information on the compliance of sewerage treatment with the Government Sewerage Policy (WAPC 2019). 	<p>Applicant Comment</p> <p>Demonstration that the wastewater system is appropriate can be addressed as a condition of development approval, as well as through the addressing the requirements of the <i>Health (Treatment of Sewage</i></p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
		<p><i>and Disposal of Effluent and Liquid Waste) Regulations 1974</i>, which is a standard requirement for the approval of onsite wastewater disposal systems.</p> <p>The updated LWMS (prepared in response to DWER and DOH comments) has outlined the relevant criteria that need to be satisfied by onsite wastewater disposal systems within the Structure Plan area, inclusive of the subject land. These criteria have been outlined below:</p> <ul style="list-style-type: none"> • Achieving 1.5 m separation to highest groundwater level. • This can be a condition of development approval, with groundwater levels documented within the LWMS. • As a minimum, a secondary treatment system with nutrient removal (i.e. ATU) should be utilised. This can be a condition of development approval. • Specifications for sand fill need to be based on hydraulic loading serviced, size of ATU system and characteristics of existing underlying soil. This is able to be addressed through an site and soil evaluation (SSE) as well as satisfying the requirements of the <i>Health (Treatment of Sewage and Disposal of Effluent and Liquid Waste) Regulations 1974</i>. This can be satisfied as a condition of approval. • Minimum lot size of 1 ha. The proposed subject land is a minimum of 1 ha satisfying this criteria. • Onsite sewage systems will not be located within 100 m of a drainage system that discharges directly into a waterway of significant wetland. There are no natural waterways or wetlands within 100 m of the Structure Plan area and therefore the subject land), accordingly onsite sewage systems within the subject land can be located within 100 m of future roadside swales and detention areas. • Onsite sewage will not be located in an area subject to inundation or flooding in a 10% annual exceedance probability (AEP) rainfall event. This can be a condition of development approval.

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
		<p>Addressing the above criteria can be a condition of development approval, needing to be satisfied prior to the submission of a building licence.</p> <p>Officer comment</p> <p>The applicant has amended the proposal to increase the development area to comply with the 1 hectare requirement. If approved, development would need to be conditioned to ensure the implementation of the approved LWMS at development stage.</p>
	<p>4. Provide a site and soil evaluation to support the roadhouse proposal.</p>	<p>Applicant Comments</p> <p>As per comment above, this can be addressed as a condition of development approval.</p> <p>Officer Comment</p> <p>If approved, development would need to be conditioned to ensure that a site and soil evaluation is completed prior to any works being commenced with.</p>
	<p>5. Detailed information on the highest known groundwater level for the site.</p>	<p>Applicants Comment</p> <p>This is provided in the LWMS, which is applicable to development.</p> <p>Officer Comment</p> <p>If approved, development would need to be conditioned to ensure the implementation of the approved LWMS at development stage.</p>
	<p>6. Detailed information on land filling and / or subsurface drainage. Including the demonstration of how surface and groundwater will be managed to meet future subdivision drainage</p>	<p>Applicant Comment</p> <p>Indicative minimum inverts of drainage and inferred minimum adjacent lot levels has been addressed in the LWMS. Installation of</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
	<p>configurations including all controls, receiving systems and proposed systems under the Local Water Management Strategy.</p> <p>7. Provide approvals from the Water Corporation for any connection of drainage to the existing drainage networks.</p>	<p>subsoil drains and control of groundwater can be addressed through the detailed design stage and will need to be in accordance with the LWMS.</p> <p>The LWMS outlines that all lots will be required to retain 2 m³ of runoff for every 65 m² of hardstand or roof, which is consistent with the Shire of Dardanup Policy No CP060 – Storm Water Discharge from Building.</p> <p>The requirement to comply with the LWMS can be addressed as a condition of development approval.</p> <p>Officer Comment</p> <p>If approved, development would need to be conditioned to ensure the implementation of the approved LWMS at development stage; and a detailed design of the onsite stormwater drainage system will be required to be submitted for approval and implementation.</p> <p>Specific requirement for approval from Water Corporation was not identified as part of their comments.</p> <p>As per response to Water Corporation comments, as a temporary measure (i.e. until the full development of the LSP area is progressed), development will be required to demonstrate that an appropriate basin has been provided within the subject land to hold (i.e. retain) stormwater from a 1 % AEP event before it is allowed to overland flow across existing terrain towards the drain.</p> <p>This can be addressed as a condition of development approval.</p> <p>Officer Comment</p> <p>If approved, development would need to be conditioned to ensure the a detailed design of the onsite stormwater drainage system is submitted for approval by the Shire and prior to implementation by the applicant.</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
	<p>8. Demonstrate water supply source and yearly water requirements.</p>	<p>Applicant Comment</p> <p>Noted. Use of groundwater is not proposed. The development can be readily serviced by storage of tankered water on-site.</p> <p>The AqWest reticulated water network, with extension, can also serviced the land, though this is not proposed in preference of rainwater tanks/carted water.</p> <p>This can be addressed as a condition of development approval and would be required prior to occupancy. The detailed design would be deal with in compliance with the condition and building licence.</p> <p>Officer Comment</p> <p>If approved, an adequate and potable water supply will be required for the facility. The facility will be included in the Shire water monitoring program, with regular water testing undertaken. Additional obligations may occur as a result of the review of legislation involving water supplies that is currently being undertaken.</p>
	<p>9. Demonstrate the proposals meets best management practices including:</p> <ul style="list-style-type: none"> Water Quality Protection Note 56- Tanks for fuel and chemical storage near sensitive water resources (DWER 2018) https://www.water.wa.gov.au/___data/assets/pdf_file/0010/513/1/82577.pdf and Water Quality Protection Note 49 - Service Stations (DoW 2013) https://www.water.wa.gov.au/___data/assets/pdf_file/0005/495/5/106040.pdf 	<p>Applicant Comment</p> <p>This can be addressed as a condition of development approval.</p> <p>Under Section 8 of the Dangerous Goods Safety Act 2004, 'a person who is involved directly or indirectly in storing, handling or transporting dangerous goods must take all reasonably practicable measures to minimise the risk to people, property and the environment from the goods'. Therefore, appropriate storage and handling of flammable and combustible materials (such as fuel) to minimise risk to the environment, is a requirement of separate legislation and is also guided by a number of Australian Standards (addressing which is also a requirement of this same legislation).</p> <p>Officer Comment</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
	In the event there are modifications to the proposal that may have implications on aspects of environment and/or water management, the Department should be notified to enable the implications to be assessed.	This information is usually provided to the applicant in the form of an advice note for consideration at the detailed design phase. Noted.
8. ATCO Gas Australia Pty Ltd 30/01/2020	<p>a. ATCO Gas Australia (ATCO) has no objection to the proposed development application and change in landuse, based on the information and plan provided, to facilitate the development within Lot 603 Martin-Pelusey Road, Picton East for the purpose of</p> <ul style="list-style-type: none"> • A development area of approximately 9769m² • Retail building with a floor space of 265m² • Retail car canopy • Retail truck canopy • Tank farm • 15 car parking bays • Poly ID sign and promo sign <p>b. ATCO's closest asset (a high pressure gas pipeline_DN150ST HP 1900kPa) is approximately 30 metres to the east within an easement within the properties adjacent to and on the opposite side of Martin-Pelusey Road.</p>	Noted.
9. Dangerous and Critical Risks Directorate 4/03/2020	The establishment of a Roadhouse will require application for a Dangerous Goods Storage licence before dangerous goods can be stored on site. There is significant detail relating to the proposed Dangerous Goods and their storage, which is not included in the documents that make further comment difficult until a licence application is submitted. The specific detail relating to the Dangerous Goods Site Licence application will be assessed against relevant Australian Standards for compliance.	<p>Applicant Comment</p> <p>Noted. This is a standard requirement for service stations and will be addressed prior to occupation/operation, as required by the Dangerous Goods Safety Act 2004.</p> <p>Officer Comment</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
10 Department of Health	<p>a. <i>Water Supply and Wastewater Disposal</i> The development is to be in accordance with the <i>Government Sewerage Policy (2019)</i>. For non-scheme water connected areas, the development is to have access to a sufficient supply of potable water that is of the quality specified under the <i>Australian Drinking Water Quality Guidelines 2011</i>. The necessary requirements may be referenced and downloaded from: http://ww2.health.wa.gov.au/Articles/A_E/Drinking-water-quality-management http://ww2.health.wa.gov.au/Articles/A_E/Drinking-water-guidelines-and-standards</p> <p>b. <i>On-Site Wastewater Disposal</i> Suitable provision for an adequate onsite effluent disposal area is to be accommodated in any planning approval. For on-site wastewater disposal systems to be approved, a winter site-and-soil evaluation is required and be in accordance with Australian New Zealand Standard 1547. For more details please refer to the attached fact sheet '<i>Guidance on Site-and-soil evaluation for Onsite Sewage</i>'</p>	<p>This information is usually provided to the applicant in the form of an advice note as it is separate legislation that the applicant would need to comply with.</p> <p>Applicant Comment Noted. Use of groundwater is not proposed. The development can be readily serviced by storage of tankered water on-site. The AqWest reticulated water network, with extension, can also service the land, though this is not proposed in preference of rainwater tanks/carted water. This can be addressed as a condition of development approval and would be required prior to occupancy. The detailed design would be dealt with in compliance with the condition and building licence. This is normal practice for the locality.</p> <p>Officer Comment If approved, an adequate and potable water supply will be required for the facility. The facility will be included in the Shire water monitoring program, with regular water testing undertaken. Additional obligations may occur as a result of the review of legislation involving water supplies that is currently being undertaken.</p> <p>Noted. As per comments further above, demonstration that the wastewater system is appropriate can be addressed as a condition of development approval, as well as through the addressing the requirements of the <i>Health (Treatment of Sewage and Disposal of Effluent and Liquid Waste) Regulations 1974</i>, which is a standard requirement for the approval of onsite wastewater disposal systems.</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
	<p><i>Management</i>. The fact sheet also provides information on the stages in the planning process and scale of development and determines the timing and the level of detail of investigation and reporting.</p>	<p>The updated LWMS (prepared in response to DWER and DOH comments) has outlined the relevant criteria that need to be satisfied by onsite wastewater disposal systems within the Structure Plan area, inclusive of the subject land. These criteria have been outlined below:</p> <ul style="list-style-type: none"> • Achieving 1.5 m separation to highest groundwater level. • This can be a condition of development approval, with groundwater levels documented within the LWMS. • As a minimum, a secondary treatment system with nutrient removal (i.e. ATU) should be utilised. This can be a condition of development approval. • Specifications for sand fill need to be based on hydraulic loading serviced, size of ATU system and characteristics of existing underlying soil. This is able to be addressed through an site and soil evaluation (SSE) as well as satisfying the requirements of the <i>Health (Treatment of Sewage and Disposal of Effluent and Liquid Waste) Regulations 1974</i>. This can be satisfied as a condition of approval. • Minimum lot size of 1 ha. The proposed subject land is a minimum of 1 ha satisfying this criteria. • Onsite sewage systems will not be located within 100 m of a drainage system that discharges directly into a waterway of significant wetland. There are no natural waterways or wetlands within 100 m of the Structure Plan area 9 and therefore the subject land), accordingly onsite sewage systems within the subject land can be located within 100 m of future roadside swales and detention areas. • Onsite sewage will not be located in an area subject to inundation or flooding in a 10% annual exceedance probability (AEP) rainfall event. This can be a condition of development approval. <p>Addressing the above criteria can be a condition of development approval, needing to be satisfied prior to the submission of a building licence.</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

(Appendix ORD: 12.4D)

Submitter	Submitter Comment	Response to comments
		<p>Officer Comment</p> <p>If the development is approved, the effluent system will require application to install. A report from Shire Environmental Health Officer will need to be submitted with an application to Department of Health for issue. Flowrate information will be required for design purposes, and measurement and reporting of actual flowrates is recommended to ensure application rates are within recommended ranges. This information is usually provided to the applicant in the form of an advice note.</p>
11	<p><i>Food Act Requirements</i> All food related areas such as kitchen, preparation areas, etc. are to comply with the provisions of the Food Act 2008 and related code, regulations and guidelines. Details available for download from: http://www2.health.wa.gov.au/Articles/F_1/Food-regulation-in-WA</p> <p>Please be advised that Main Roads is not supportive of the proposal as it is considered that further strategic planning investigations is required to be undertaken for the locality, specifically regarding the primary road network. Comments regarding this aspect and recommendations regarding the proposed access configuration for the development are provided below.</p> <p>The land is subject to a 'Primary regional road' planning reservation under the Greater Bunbury Region Scheme, as shown in red on the proposed development site plan. The land in question is also subject to the WAPC's endorsed Preston Industrial Park Southern Precinct District Structure Plan (PIDSP). Road connectivity is provided to Martin Pelusey Road via an east / west road alignment along the southern common boundary with Lot 110. It appears that the intent of the road alignment depicted on the PIDSP has been to facilitate staging and/or to coordinate internal road access between the lots in this sub-precinct and to Martin Pelusey Road.</p>	<p>Applicant Comment</p> <p>Noted. This can be addressed as a condition of development approval and would be a requirement of occupancy.</p> <p>Officer Comment</p> <p>This information is usually provided to the applicant in the form of an advice note.</p> <p>Applicant Comment</p> <p>Separate correspondence in response to Main Roads advice has been provided by Uloth & Associates (copy attached). In short the advice:</p> <ul style="list-style-type: none"> - Demonstrates there is no impediment to the DA being approved as proposed; - Recognises the specific access matters raised and recommends modification to the development plans in light of MRWA comments on this aspect. An updated Development Plan set has been prepared reflecting this. <p>Officer Comment</p> <p>With regard to the proposed link road, Main Roads WA indicated that consideration of the road access link to the proposed roundabout is subject to DPLH/WAPC approval and that it is recommended that the Local Structure Plan be considered and approved prior to individual development applications being considered. As stated in Item 12.8</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
	<p>The WAPC has also recently endorsed the Waterloo District Structure Plan (WDSP). The primary/integrator road network under the WDSP, in respect to the east / west connections to Martin Pelusey Road does, however, not integrate/align with the planned PIPDSP movement network. Main Roads, due to the severance impacts of the proposed BORR on local roads is proposing to realign and construct a number of local roads in the WDSP area. This includes provision for a roundabout on Martin Pelusey Road, which could provide the basis for consideration of a road access link (not a private access/driveway) from the subject land (and structure plan area) to the proposed roundabout, subject to DPLH/WAPC agreement and endorsement. Land areas associated with the roundabout on Martin Pelusey Road, as shown in blue on the proposed development site plan, is proposed to be acquired by Main Roads to construct the roundabout.</p> <p>However, Main Roads does not at present intend to relinquish the existing GBRS reservation and is proposing to investigate and maintain a possible alignment for a district road link from Forrest Highway along the current BORR alignment (under the Greater Bunbury Region Scheme) over South Western Highway which could connect through the subject property to Martin Pelusey Road or via Columbus Drive. This could be fully or partly included within the current Primary Regional Road reservation under the Greater Bunbury Region Scheme.</p> <p>Development in the manner and location proposed, and in the absence of further strategic planning for the locality, has the potential to prejudice further road planning/investigations which, in the opinion of Main Roads, needs to be reviewed for the locality.</p> <p>It is noted that the proponent has submitted a draft local structure plan (Precinct 2A) , which is currently being progressed and broadly identifies land use and the primary/integrator road network. It is also noted that the proponent has also submitted a concept plan for subdivision of the overall lot and adjoining lots forming part of the sub-precinct, showing a second primary road connection through to</p>	<p>relating to the proposed LSP, officers support the location for the northern link road as it aligns with the future road network on the eastern side of Martin-Pelusey Road. Approval of the Roadhouse is dependent on the location of the link road as identified in the LSP which is yet to be endorsed by the WAPC. Officer recommendation is that the Development Application is not to be considered until the LSP has been considered and approved by the WAPC.</p> <p>Main Roads WA stated that multiple, and over-width crossovers in the manner proposed has the potential to create significant traffic conflict and safety issues as they present an uncontrolled form of access and egress to a road, particularly in relation to vehicle sight lines and 'masking' of through traffic. The crossover widths are to be reduced to address these aspects. Officer recommendation is that the Development Application is not to be considered until the applicant has demonstrated that access to the development is set back sufficiently, in the order of 300 metres from the proposed roundabout to minimise traffic impacts on the future road and operation of the roundabout, alternatively access is to be taken from the proposed side road.</p> <p>The proposed development on the part of the lot that is reserved for Primary Regional Road is not in line with the purpose of the reserve and not supportable.</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
	<p>Harris Road and other local roads. A Local Structure Plan has not however been approved to guide future subdivision/development of the area. It is recommended that this process be concluded prior to approval of individual development applications. Revised development plans can then be prepared, if necessary, to address the requirements as outlined above.</p> <p>Specific comment relating to the proposed Development Application:</p> <p>It is noted access is proposed to be taken from a future road identified as an 'Integrator' road in the strategic planning framework for the locality which will provide primary access to the future industrial area. Multiple, and over-width crossovers in the manner proposed has the potential to create significant traffic conflict and safety issues as they present an uncontrolled form of access and egress to a road, particularly in relation to vehicle sight lines and 'masking' of through traffic. It is recommended that the crossover widths be reduced to address these aspects. There appears to be potential for the fuelling points be set back further and the width of the egress way/ crossover be reduced accordingly. To avoid conflict with future road investigations/requirements it is recommended that the proposed development be relocated outside to the existing Primary regional road reserve as a minimum.</p> <p>The proposed development plan indicates a new local road linking to the proposed roundabout at Martin Pelusey Road. This new local road will need to be constructed by the proponent to facilitate access to the proposed development.</p> <p>Given that the 'access road' is identified as an integrator-type road, it is recommended that no direct access be provided to the integrator road, rather a requirement be set for a side-road to be constructed and for access be taken from this side road. It is anticipated that the Shire would be imposing general access restrictions to the integrator road network to the east under the WDSP to maximise efficiency and safety on the road network, and it is considered appropriate for a similar level of access controls to be imposed for the integrator road west of Martin Pelusey, to avoid similar access issues to those on</p>	

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Submitter	Submitter Comment	Response to comments
12 Department of Biodiversity, Conservation and Attractions	<p>Harris Road from arising. The current design concept for the eastern leg of the Martin Pelusey Road under the WDSP includes an approximate 100 meter long raised median treatment to the roundabout.</p> <p>Should the development be approved in its current form, it is recommended that the proposed driveway access/egress arrangements for the development be setback as far as possible, in the order of 300 metres from the proposed roundabout to minimise traffic impacts on the future road and operation of the roundabout. No access should be supported for the proposed development from Martin Pelusey Road.</p> <p>The development plan (Project consultants, 1920-014) depicts a “line of road resumption” which relates to the proposed Bunbury Outer Ring Road (BORR). The BORR is currently being assessed by the Environmental Protection Authority (EPA). DBCA notes the location of the proposed roadhouse may be directly impacted by the final location of the BORR. DBCA understands consideration of development applications near the proposed BORR, should occur after the EPA has finalized its assessment and provided advice for the BORR. It would also need to consider any service development requirements associated with the BORR from Main Roads WA. DBCA has no further comment at this stage of the BORR assessment.</p>	<p>Applicant Comment</p> <p>Noted.</p> <p>The subject land is not located within the recently determined Planning Control Area (PCA) for the BORR, which we understand is indicative of the land being considered for the use as part of the BORR.</p> <p>No vegetation within the subject land has been identified for retention either within the Structure Plan, or as part of previous assessments by the EPA (i.e. EPA Bulletin 1282 for the Preston Industrial Park, which is inclusive of the subject land).</p> <p>Officer Comment</p> <p>The applicant indicate in the application letter that clearing of vegetation has recently occurred in preparation of additional grazing and also for the preparation of the proposed development. Officers agree with the applicants statement that no vegetation within the development area has been identified for retention within the Local Structure Plan that is under consideration or as part of previous assessments by the EPA (i.e. EPA Bulletin 1282 for the Preston Industrial Park, which is inclusive of the subject land).</p>

Schedule of Submissions – Proposed Roadhouse, Martin-Pelusey, Picton South

Submitter	Submitter Comment	Response to comments
<p>Department of Fire and Emergency Services</p>	<p>The following submission was received from DFES relating to the Bushfire Management Plan (BMP) (Version A), prepared by Emerge and dated 13 November 2019, for the above Local Structure Plan. The BMP was included as technical appendix 4 in the report titled 'Local Structure Plan Precinct 2a – Picton Industrial Park Southern Precinct' dated 17 January 2020 prepared by Rowe Group Design.</p> <p>The recommendation from DFES states that <i>“At the structure planning stage, consideration should be given to the intensification of land use and how this relates to identified bushfire hazards. DFES is satisfied that the bushfire hazard level assessment has adequately identified the bushfire risk and considered how compliance with the bushfire protection criteria can be achieved at subsequent planning stages.</i></p> <p><i>Notwithstanding the above, modifications as indicated in the above table(s) to the BMP are required prior to subsequent planning stages to ensure compliance with the bushfire protection criteria. As these modifications are minor in nature and will not affect the LSP, these modifications should be undertaken to support subsequent stages of the planning process (subdivision & development applications).”</i></p>	<p>Applicant Comment</p> <p>The BMP has been updated as per DFES request. In particular:</p> <ul style="list-style-type: none"> • The footnote regarding 'development site' has been removed. • This has been with the definition for 'developable land', as defined by the DPLH position statement on Element 1 and 2 (and is a definition that is applicable to this stage of planning). • Additional text included regarding management of drainage reserves, and potential for assumptions to be different, depending upon Shire agreement. Any changes to assumptions can be appropriately addressed through subdivision and/or development approval given the development type (industrial) and the size of future lots, which will mean as a worst case in lot setbacks can be easily accommodated. <p>Habitable buildings within the subject land have been located so that BAL-29 or less is able to be achieved. This can be demonstrated as a condition of development and as part of building licence.</p> <p>Officer Comment</p> <p>The applicant has submitted further information that demonstrate that the proposal satisfy the bushfire protection criteria.</p>

RISK ASSESSMENT TOOL

OVERALL RISK EVENT: Application for Development Approval for a Roadhouse – Lot 603 (96) Martin Pelusey Road, Picton East

RISK THEME PROFILE:

- 3 - Failure to Fulfil Compliance Requirements (Statutory, Regulatory)
6 - Engagement Practices

RISK ASSESSMENT CONTEXT: Operational

CONSEQUENCE CATEGORY	RISK EVENT	PRIOR TO TREATMENT OR CONTROL			RISK ACTION PLAN (Treatment or controls proposed)	AFTER TREATMENT OR CONTROL		
		CONSEQUENCE	LIKELIHOOD	INHERENT RISK RATING		CONSEQUENCE	LIKELIHOOD	RESIDUAL RISK RATING
HEALTH	No risk event identified for this category.	Not Required - No Risk Identified	N/A	N/A	Not required.	Not required.	Not required.	Not required.
FINANCIAL IMPACT	No risk event identified for this category.	Not Required - No Risk Identified	N/A	N/A	Not required.	Not required.	Not required.	Not required.
SERVICE INTERRUPTION	No risk event identified for this category.	Not Required - No Risk Identified	N/A	N/A	Not required.	Not required.	Not required.	Not required.
LEGAL AND COMPLIANCE	No risk event identified for this category.	Not Required - No Risk Identified	N/A	N/A	Not required.	Not required.	Not required.	Not required.
REPUTATIONAL	Risk of approval where land use and works are not in line with the purpose of the Primary Road Reservation. This may set a precedent. Applicant is likely to appeal the Council decision.	Moderate (3)	Almost Certain (5)	Moderate (5 - 11)	Not required.	Not required.	Not required.	Not required.
ENVIRONMENT	No risk event identified for this category.	Not Required - No Risk Identified	N/A	N/A	Not required.	Not required.	Not required.	Not required.

Clause 67 Deemed Provisions Table

Clause No	Matter to be Considered	Yes	No	N/A	Comment
(a)	the aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area	Yes			<p>The general objectives of TPS3 are:</p> <ul style="list-style-type: none"> • to zone the Scheme Area for the purposes in the Scheme described; • to secure the amenity, health and convenience of the Scheme Area and the inhabitants thereof; • to make provisions as to the nature and location of buildings and the size of lots when used for certain purposes; • the preservation of places of natural beauty, of historic buildings and objects of historical and scientific interest; and • to make provision for other matters necessary or incidental to town planning and housing. <p>The proposed development on the subject land has been assessed against the abovementioned objectives of TPS3. Shire Officers consider the application to be generally consistent with the objectives of the Scheme. However, it is noted that the proposed Roadhouse is a discretionary use in the "General Farming" zone.</p> <p>The TPS3 objectives for the 'General Farming' zone are as follows:</p> <ul style="list-style-type: none"> • to provide for a wide variety of productive farming activities, ranging from broad acre grazing to horticulture, which are compatible with the capability of the land and retain the rural character and amenity of the locality. • to protect areas of significant agricultural value, particularly those in irrigation districts, from conflicting land uses. • to facilitate low-key tourist development where it is incidental to the use of the land for farming purposes and where land use conflict can be minimised. <p>Further to the objectives of the 'General Farming' Zone TPS3 under cl.3.13 states that in considering development applications on land in Non-Urban Zones the Council shall have regard to the need:</p> <ul style="list-style-type: none"> • to protect the economic viability of the rural land use generally; • to preserve the rural character and a rural appearance of the area; • to ensure that the existing standard of roads, water and electricity supply and other services is sufficient for the additional demands that the proposed development would create; and • to ensure that in general all buildings are at least twenty meters from any lot boundary adjoining a street in order to preserve the rural character of the area. <p>Officers consider the viability of the rural land will not be negatively impacted as the Roadhouse development is proposed on 3.11% of the total area of Lot 603. The eastern and southern elevations of the retail building consist mainly of precast concrete panels that will be painted with a low sheen acrylic paint finish. Given the location of the building in a paddock there is an opportunity to consider architectural materials and detail to minimise bulk of the building and to reflect the rural aspect of the site. If approved, a condition of approval is to be included requiring comprehensive details of the design of the eastern and southern elevations of the retail building to be submitted for consideration prior to the commencement of works and that these finished should be implemented prior to the occupation of the building. Existing services is discussed further in the "Officer Comment" section of the Council report. The proposed development does not comply with the "General Farming" zone requirements which requires a 20m setback from the indicative lot boundaries. However, the applicant states that technically</p>

	<p>it complies with the boundary setbacks of the larger Lot 603 within which it is located as the development is set back well in excess of the required 20 setback from lot boundaries.</p> <p>It is also to be noted that the Local Planning Strategy identifies future land use on the subject land as Industrial, and the development area where the proposed Roadhouse is to be constructed forms part of the Draft 'Picton Industrial Park Precinct 2A Local Structure Plan' (LSP). Under the Draft LSP the development area for the Roadhouse is identified for "General Industrial" use. A Roadhouse is also a discretionary use in the "General Industry" zone under TPS3.</p> <p>The TPS3 objective for the 'General Industry' zone is as follows:</p> <ul style="list-style-type: none"> To provide for a wide range of industrial and associated activities, which can be undertaken without undue constraints on operational performance, so as to meet the needs of the wider community for industrial services and facilities. <p>Shire Officers consider the application to be consistent with the objectives of the 'General Industry' zone under the Scheme.</p>				
(b)	<p>the requirements of orderly and proper planning including any proposed local planning scheme or amendment to this Scheme that has been advertised under the Planning and Development (Local Planning Schemes) Regulations 2015 or any other proposed planning instrument that the local government is seriously considering adopting or approving</p>	Yes			
(c)	<p>any approved State planning policy</p>	Yes			SPP 3.7 Planning in Bushfire Prone Areas – the proposal contains a Bushfire Attack Level Contour map. The applicant has submitted further information that demonstrate that the proposal satisfy the bushfire protection criteria.
(d)	<p>any environmental protection policy approved under the Environmental Protection Act 1986 section 31(d);</p>			N/A	
(e)	<p>any policy of the Commission;</p>			N/A	
(f)	<p>any policy of the State</p>			N/A	
(g)	<p>any local planning policy for the Scheme area</p>	Yes			CP060 – Storm water discharge from buildings. Under the policy all stormwater run-off from the development will need to be contained on the development site. Stormwater drainage plans would need to be submitted for consideration and approval by the Shire prior to the commencement of works. This will be a condition of approval.

(h)	any structure plan, activity centre plan or local development plan that relates to the development	Yes			As per (b) above.
(i)	any report of the review of the local planning scheme that has been published under the Planning and Development (Local Planning Schemes) Regulations 2015		N/A		
(j)	in the case of land reserved under this Scheme, the objectives for the reserve and the additional and permitted uses identified in this Scheme for the reserve	Yes			The development area is partly reserved for Primary Regional Roads purposes under the GBRs. The purpose of the reserve is to provide a regional road network to accommodate current and future transport needs on roads declared under the <i>Main Roads Act 1930</i> . The applicant states that the reserve is now superseded. However, it is still a gazette reserve under the GBRs. Officers consider the development of a car canopy and a retail building associated with the Roadhouse development is not in line with the purpose of the reserve and not supportable under TPS3. The proposal was referred to Main Roads WA, the advice agency under the GBRs. Main Roads WA submitted an objection stating that the development in the manner and location proposed and in the absence of further strategic planning for the locality, has the potential to prejudice further road planning/investigations which, in the opinion of Main Roads, needs to be reviewed for the locality. To avoid conflict with future road investigations / requirements Main Roads WA recommended that the proposed development be relocated outside of the existing Primary Regional Road Reservation.
(k)	the built heritage conservation of any place that is of cultural significance		N/A		
(l)	the effect of the proposal on the cultural heritage significance of the area in which the development is located		N/A		
(m)	the compatibility of the development with its setting including the relationship of the development to development on adjoining land or on other land in the locality including, but not	Yes			As per (a) above. Given the location of the building in a paddock there is an opportunity to consider including other architectural materials and detail to minimise bulk of the building and to reflect the rural aspect of the site. The proposed retail building is 5.15m in height, the proposed retail car canopy is 5.5m in height and the proposed retail truck canopy is 6.5m in height. Although the heights are consistent for a two storey building it may appear bulk as there are no surrounding structures in close proximity to it. This may be softened with the provision of appropriate landscaping. If approved, a condition of approval is to be included requiring a landscaping plan to be submitted for further consideration and approval. It is also recommended that if approval is granted a condition be included requiring comprehensive details of the design of the eastern and southern

	limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development			elevations of the retail building to be submitted for consideration prior to the commencement of works and that these finished should be implemented prior to the occupation of the building.
(n)	the amenity of the locality including the following — (i) environmental impacts of the development; (ii) the character of the locality; (iii) social impacts of the development;	Yes		The applicant indicate in the application letter that clearing of vegetation has recently occurred in preparation of additional grazing and also for the preparation of the proposed development. Officers consider that the proposed development will provide a service to the local community and no adverse social impacts are expected as a result of the proposed development. Matters regarding the rural character of the locality has already been addressed in (a) above. The Roadhouse will fit in with the future planning for the area which is to have a "General Industrial" character.
(o)	the likely effect of the development on the natural environment or water resources and any means that are proposed to protect or to mitigate impacts on the natural environment or the water resource;	Yes		This matter is considered further in the Officer Comment section of the report.
(p)	whether adequate provision has been made for the landscaping of the land to which the application relates and whether any trees or other vegetation on the land should be preserved;	Yes		The application states that landscaping will be provided to the front and sides of the development. Landscaping will also be provided within the centre of the site to provide separation from vehicles using the retail truck canopy or retail car canopy. Although the heights of the proposed retail building and are consistent for a two storey building it may appear bulk as there are no surrounding structures in close proximity to it. Officers consider it needs to be softened with the provision of appropriate landscaping. If approved, a landscaping plan is to be conditioned to ensure appropriate landscaping of the development occurs.
(q)	the suitability of the land for the development taking into account the possible risk of flooding, tidal inundation, subsidence, landslip, bush fire, soil erosion, land degradation or any other risk;	Yes		These matters relates to the submissions received from various agencies and departments and are considered in the "Officer Comment" section of the Council report.
(r)	the suitability of the land for the development taking into account the possible risk to human health or safety;	Yes		These matters relates to the submissions received from various agencies and departments and are considered in the "Officer Comment" section of the Council report.
(s)	the adequacy of —	Yes		With regard to the proposed link road, Main Roads WA indicated that consideration of the road access link to the proposed roundabout is subject to DPLH/WAPC approval and that it is recommended that the Local Structure Plan be considered and

	<p>(i) the proposed means of access to and egress from the site; and (ii) arrangements for the loading, unloading, manoeuvring and parking of vehicles;</p>			<p>approved prior to individual development applications being considered. As stated in Item 12.8 relating to the proposed LSP, officers support the location for the northern link road as it aligns with the future road network on the eastern side of Martin-Pelusey Road. Approval of the Roadhouse is dependent on the location of the link road as identified in the LSP which is yet to be endorsed by the WAPC.</p> <p>Main Roads WA stated that multiple, and over-width crossovers in the manner proposed has the potential to create significant traffic conflict and safety issues as they present an uncontrolled form of access and egress to a road, particularly in relation to vehicle sight lines and 'masking' of through traffic. The crossover widths are to be reduced to address these aspects.</p> <p>The applicant has amended to Development Plans to reduce the number of crossovers and provided a response by Uloth & Associates Consultants in Traffic Engineering & Transport Planning on 14 May 2020. This information was referred to Main Roads WA and it replied stating the plans submitted have not been modified to address Main Roads concerns.</p> <p>Further comments regarding this matter is contained in the "Officer Comment" section of the Council report.</p>
(t)	<p>the amount of traffic likely to be generated by the development, particularly in relation to the capacity of the road system in the locality and the probable effect on traffic flow and safety;</p>	Yes		<p>The transport assessment report for the LSP identifies the east west road as a local distributor road with a 25m road reserve expected to carry less than 7000 vehicles per day.</p>
(u)	<p>the availability and adequacy for the development of the following — (i) public transport services; (ii) public utility services; (iii) storage, management and collection of waste; (iv) access for pedestrians and cyclists (including end of trip storage, toilet and shower facilities); (v) access by older people and people with disability</p>	Yes		<p>These matters are to be conditioned if the DA is to be granted.</p>
(v)	<p>the potential loss of any community service or benefit resulting from the development other than potential loss that may result</p>		N/A	<p>The Roadhouse will provide a service to the community.</p>

	from economic competition between new and existing businesses;				
(w)	the history of the site where the development is to be located;			N/A	
(x)	the impact of the development on the community as a whole notwithstanding the impact of the development on particular individuals;	Yes			No objections were received from the community when the proposal was advertised.
(y)	any submissions received on the application;	Yes			Submissions on the application are discussed in further detail in the Schedule of Submissions and the "Officer Comment" section of the Council report.
(za)	the comments or submissions received from any authority consulted under clause 66;	Yes			Submissions on the application are discussed in further detail in the Schedule of Submissions and the "Officer Comment" section of the Council report.
(zb)	any other planning consideration the local government considers appropriate.			N/A	



CONSULTANTS IN
**TRAFFIC ENGINEERING
& TRANSPORT PLANNING**

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580 Hay Street, Perth WA 6000
www.uloth.com.au (08) 9321 4841

14 May 2020

Rod Dixon,
Manager Land Development,
Rowe Group
3/369 Newcastle Street,
NORTHBRIDGE WA 6003

Dear Rod,

RE: PROPOSED ROADHOUSE - LOT 603 MARTIN PELUSEY ROAD, PICTON EAST
RESPONSE TO MRWA COMMENTS

As requested, I now provide our responses to the email comments issued by Main Roads WA (Daniel Naude) to Shire of Dardanup (Cecilia Muller) on 16 March 2020, regarding the proposed roadhouse on Lot 603 (96) Martin Pelusey Road, Picton East.

It is noted that the comments from Main Roads WA include concerns regarding the overall road planning, specifically in relation to *“a possible alignment for a district road link from Forrest Highway along the current BORR alignment (under the Greater Bunbury Region Scheme) over South Western Highway...”*. Specific comments are also provided relating to the proposed Development Application, regarding access being proposed from an Integrator Road, and multiple/over-width crossovers creating traffic conflict and safety issues.

Responses to the various issues raised are detailed below:

1. STRATEGIC PLANNING CONCERNS

- As noted in the Main Roads WA comments, WAPC has recently endorsed the Waterloo District Structure Plan (together with the Wunju District Structure Plan). These plans include the east-west District Distributor Road connection towards the east from the proposed roundabout in Martin Pelusey Road, together with the proposed realignment of Martin Pelusey Road north of Lot 603 (also as a District Distributor Road) to align with a future overpass of South Western Highway at Hynes Road.
- In my phone conversation with Main Roads WA, Daniel Naude expressed concerns regarding the future funding of the overpass at South Western Highway as part of the District Structure Plans, and instead suggested that the overpass may need to be State-funded (and therefore part of the Primary Regional Road network under the Greater Bunbury Region Scheme). His concern then, is that this creates uncertainty regarding the currently proposed Development Application.
- However, from the point of view of the proposed roadhouse application, as well as the currently proposed Local Structure Plan over Lots 103, 110 and 603 Harris Road and Martin Pelusey Road, there is already a proposed District Distributor Road (Martin Pelusey Road) along the eastern boundary of the Local Structure Plan, with a road reserve width of 40 metres at the southern end of Lot 603, widening to over 100 metres at the proposed roundabout.

(Appendix ORD: 12.4G)

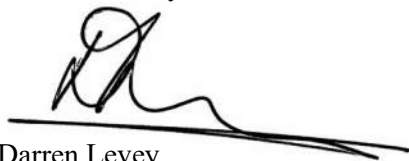
- This is sufficient to cater for the currently proposed District Distributor Road function extending north across South Western Highway towards Forrest Highway, as already indicated in both the Waterloo and Wanju District Structure Plans.
- If, as suggested by Main Roads WA, there is a requirement for a Primary Regional Road reservation to be included for the future overpass at South Western Highway, the attached Figure 1 (updated from Figure 2 in our November 2019 Transport Assessment Report) shows how this could be incorporated into the 2 District Structure Plans, with no impact at all on the proposed Development Application (or Local Structure Plan) at Lot 603.
- Figure 2 then shows how this would translate to the overall future road hierarchy in the vicinity of the proposed structure plans, including the retention of the existing Martin Pelusey Road alignment at South Western Highway prior to the construction of the overpass, while Figure 3 confirms that the proposed realignment of Martin Pelusey Road has no further impact on Lot 603 (following the construction of the currently proposed roundabout).

2. PROPOSED DEVELOPMENT APPLICATION

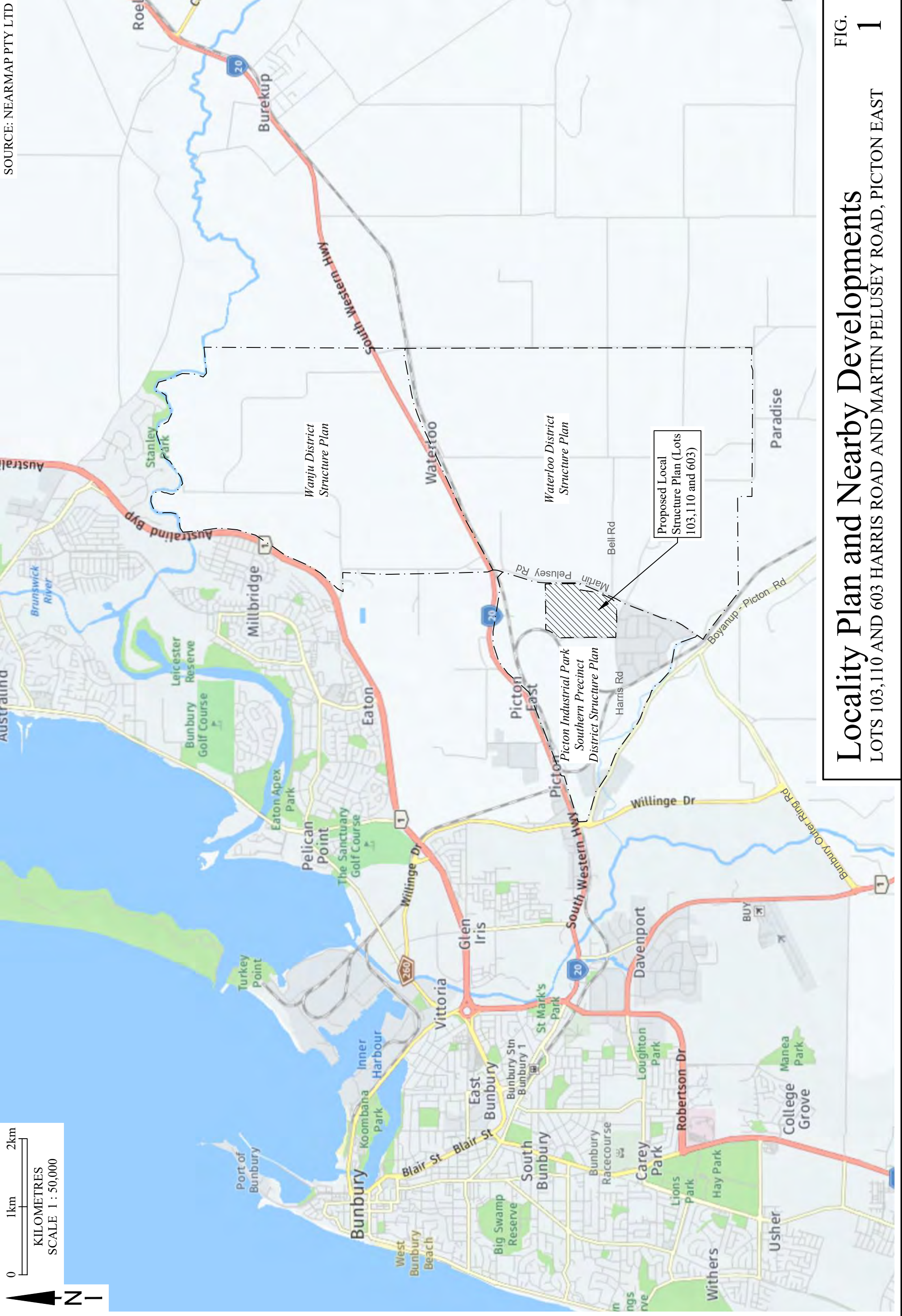
- In regard to the proposed Development Application, it is noted that the Main Roads WA comments are predicated on the east-west road linking to Martin Pelusey Road being “an ‘Integrator’ road in the strategic planning framework for the locality...”. It is also noted, however, that the November 2019 Transport Assessment Report for the proposed Local Structure Plan identifies the east-west road as a Local Distributor Road (with a 25 metre road reserve) expected to carry less than 7,000 vehicles per day, since there is no direct route for the District Distributor Road east of Martin Pelusey Road to extend further west towards Harris Road.
- The comments regarding the multiple and over-width crossovers, however, are valid, and the proposed Development Application site plan has been revised to take these comments into account. The revised plan is therefore shown in the attached Figure 4, together with an overlay of the proposed roundabout at Martin Pelusey Road.
- The revised plan proposes a separation of 55 metres from the property boundary at Martin Pelusey Road to the centreline of the eastern access crossover, ensuring that the Approach Sight Distance for a speed of 50 kilometres per hour exiting the roundabout is easily provided, which is sufficient for property access off a local road. The plan has also been modified to provide greater separation between the truck fuelling points and the street, in order to reduce the overall crossover width for the truck stop area.
- It is agreed that the new local road linking the development to Martin Pelusey Road will need to be constructed by the proponent to facilitate access via the proposed roundabout. It is also agreed that there should be no direct access from the site onto Martin Pelusey Road.

I trust that the above responses (together with the revised Development Application Plan) are sufficient to address the issues raised by Main Roads WA. However, please do not hesitate to contact me if you required anything further.

Yours sincerely,



Darren Levey



SOURCE: NEARMAP PTY LTD

FIG. 1

Locality Plan and Nearby Developments

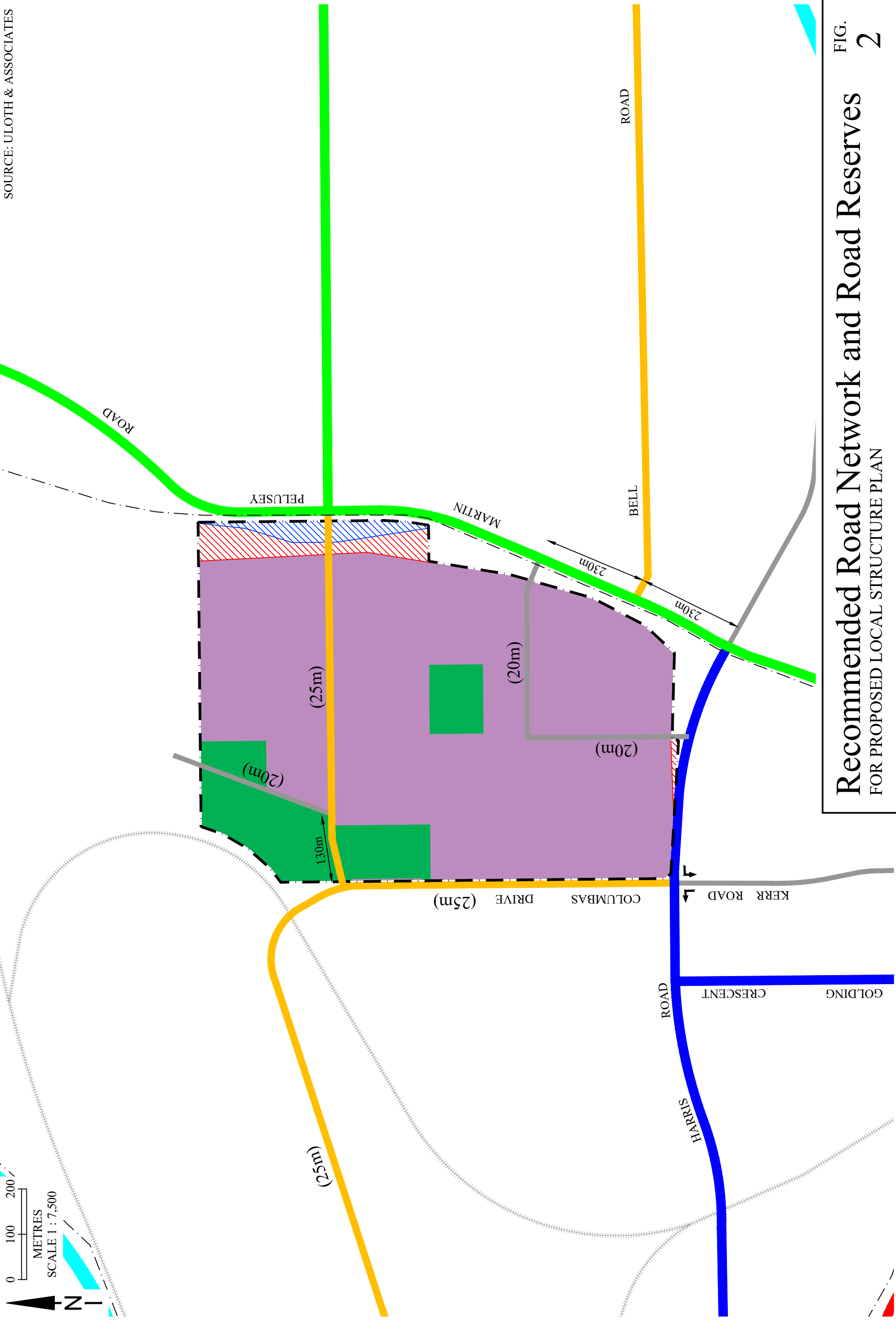
LOTS 103,110 AND 603 HARRIS ROAD AND MARTIN PELUSEY ROAD, PICTON EAST

SOURCE: ULOTH & ASSOCIATES

FIG. 2

Recommended Road Network and Road Reserves

FOR PROPOSED LOCAL STRUCTURE PLAN



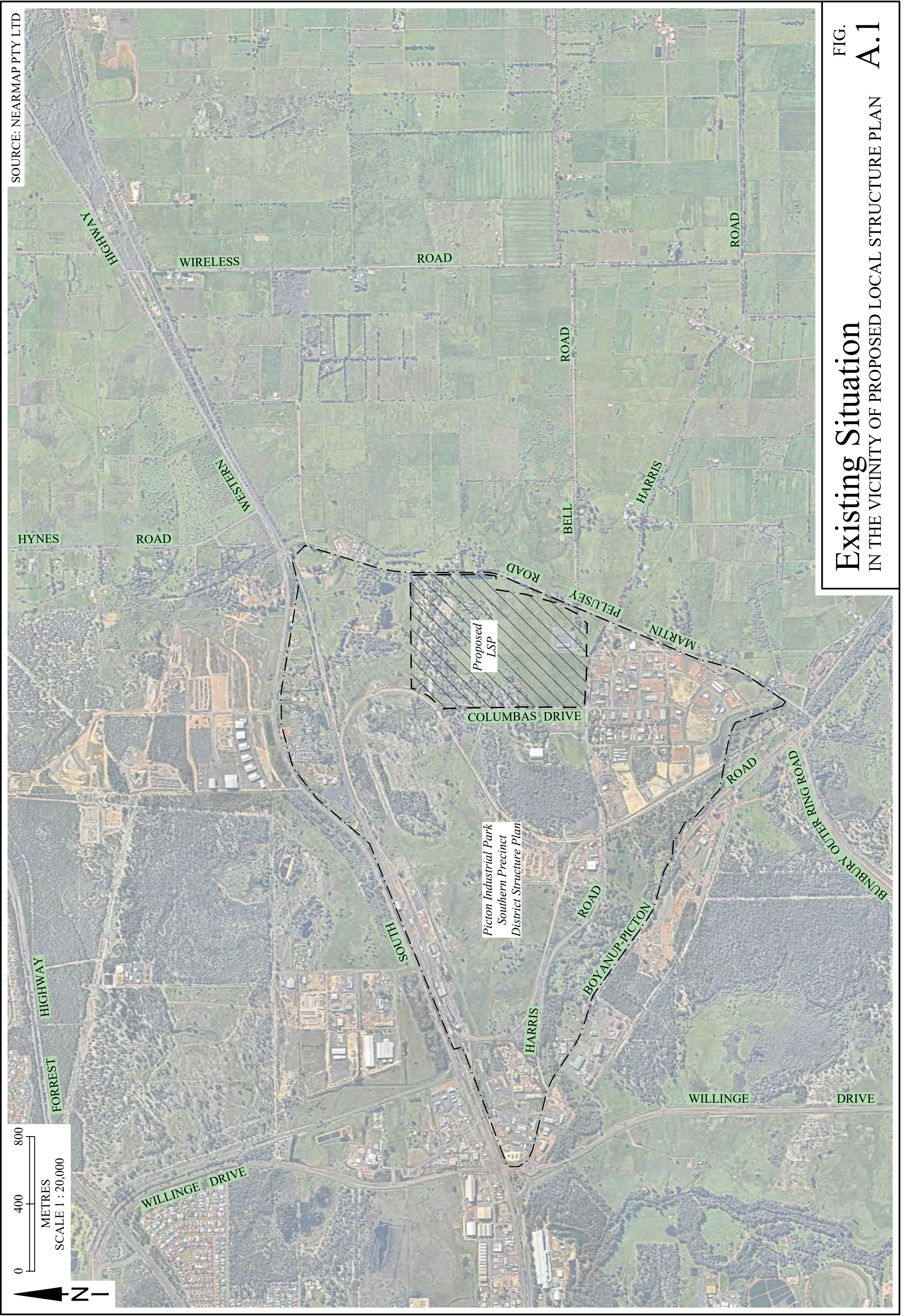
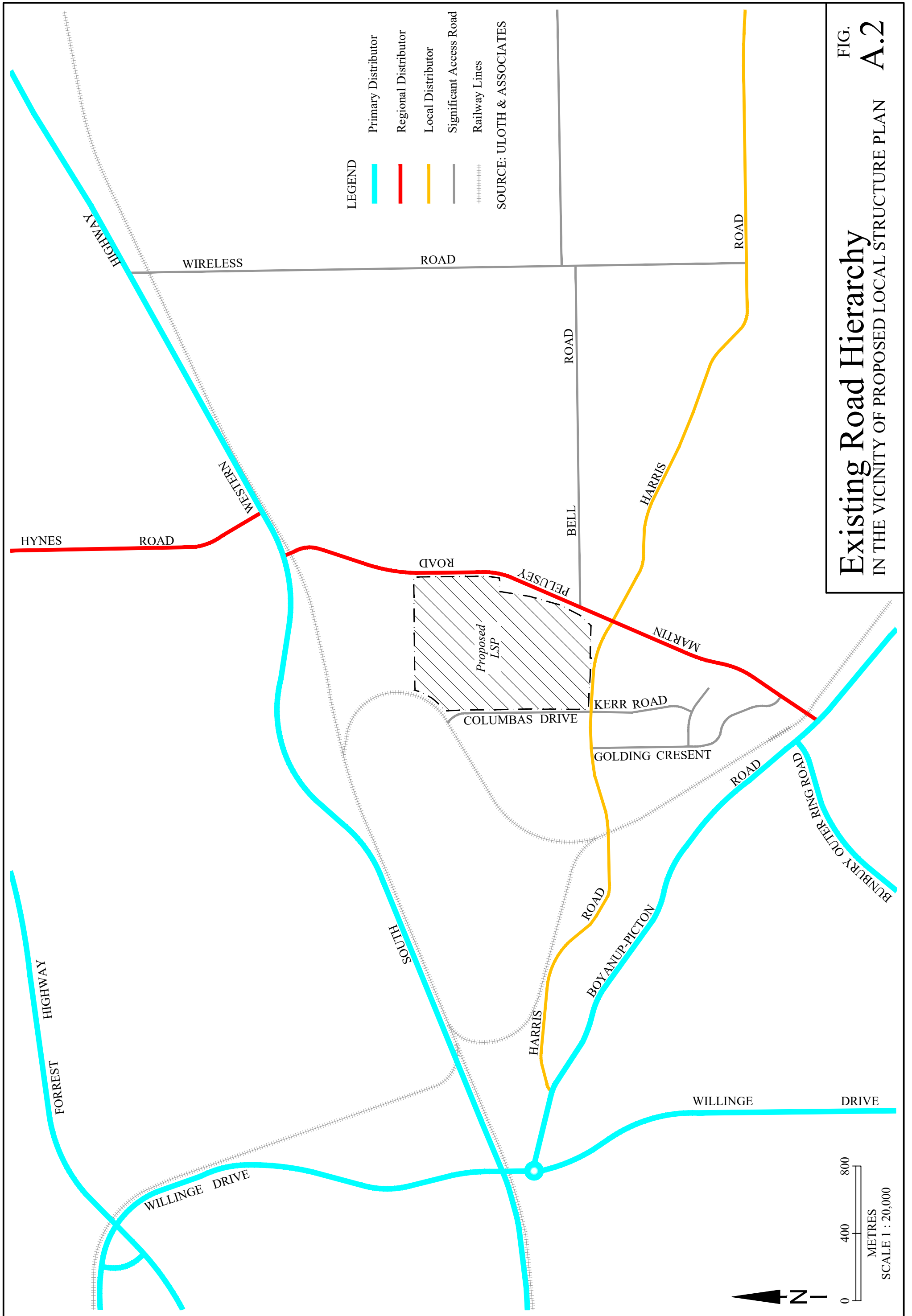


FIG. A.1
Existing Situation
 IN THE VICINITY OF PROPOSED LOCAL STRUCTURE PLAN



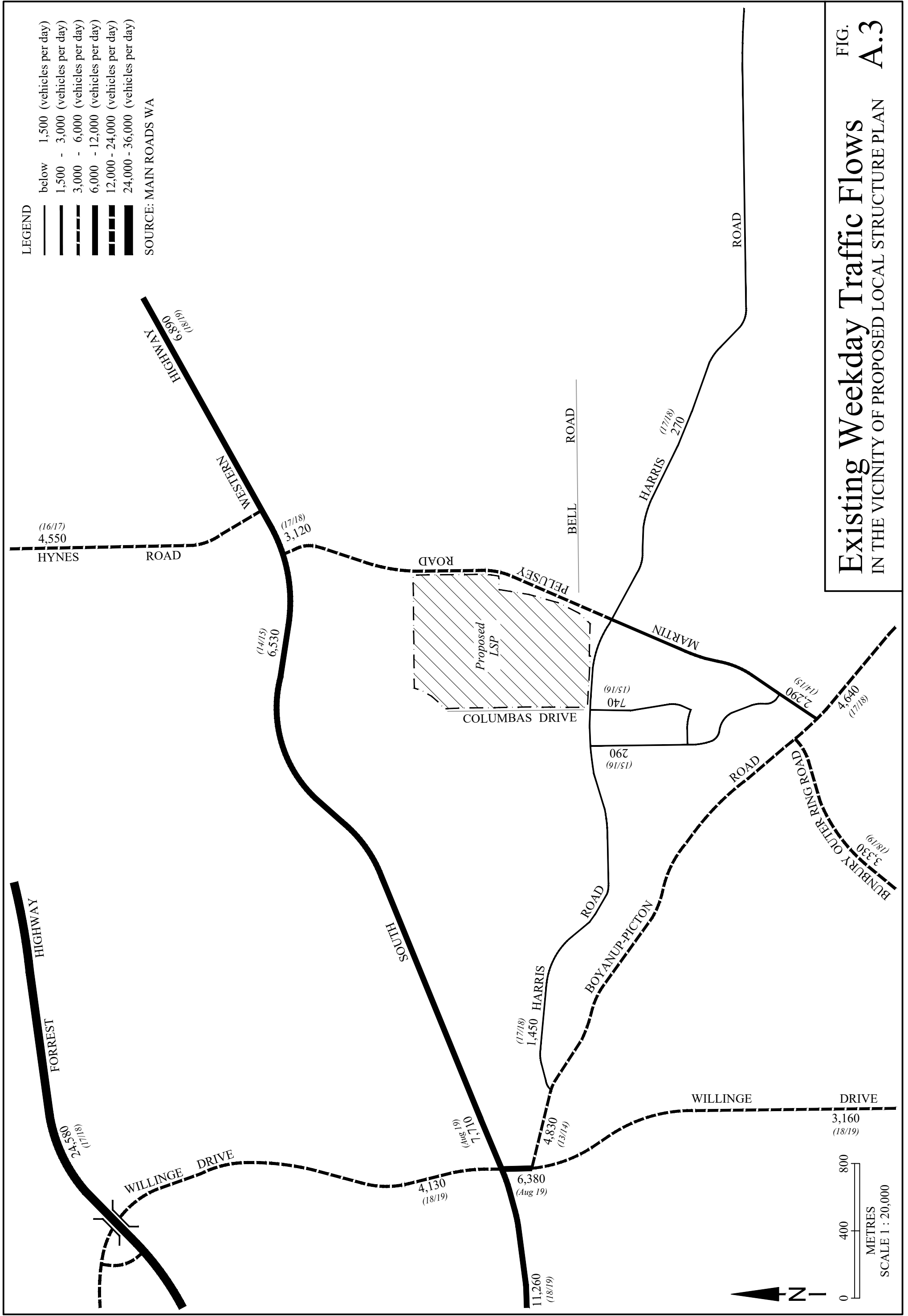
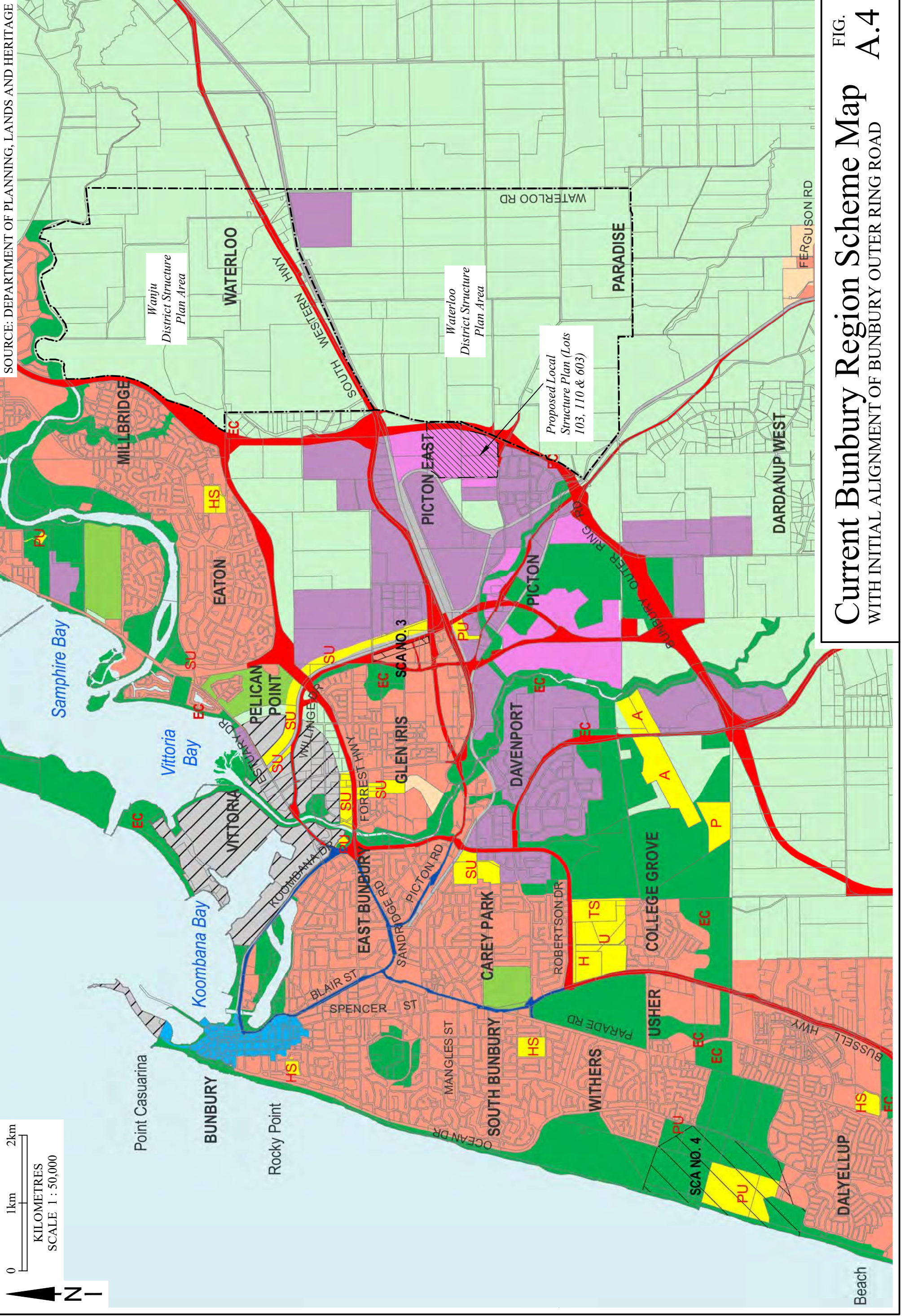


FIG. A.3
Existing Weekday Traffic Flows
IN THE VICINITY OF PROPOSED LOCAL STRUCTURE PLAN



SOURCE: DEPARTMENT OF PLANNING, LANDS AND HERITAGE

FIG. A.4
Current Bunbury Region Scheme Map
WITH INITIAL ALIGNMENT OF BUNBURY OUTER RING ROAD

SOURCE: MAIN ROADS WA

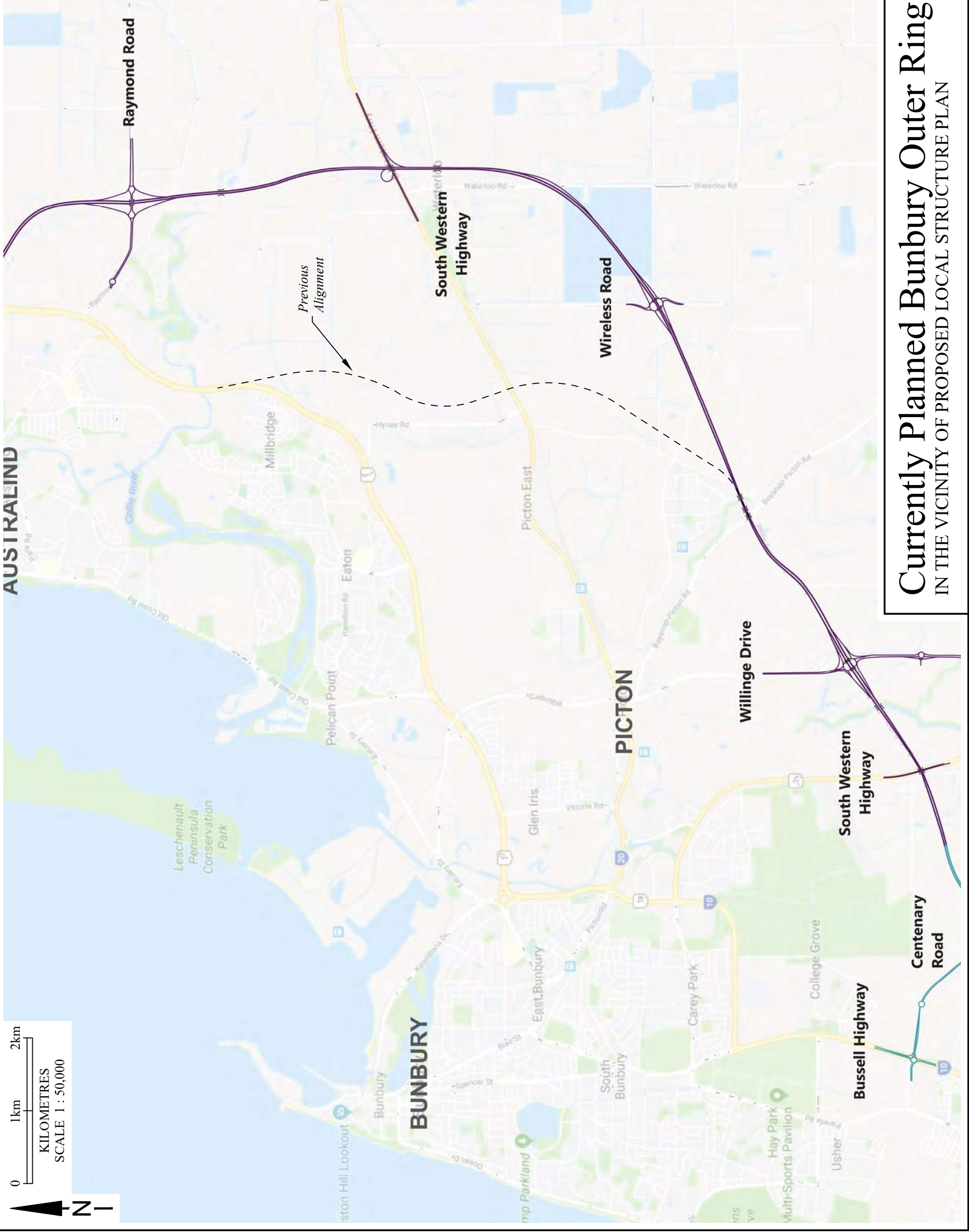


FIG. A.5
Currently Planned Bunbury Outer Ring Road
 IN THE VICINITY OF PROPOSED LOCAL STRUCTURE PLAN

SOURCE: ULOTH & ASSOCIATES

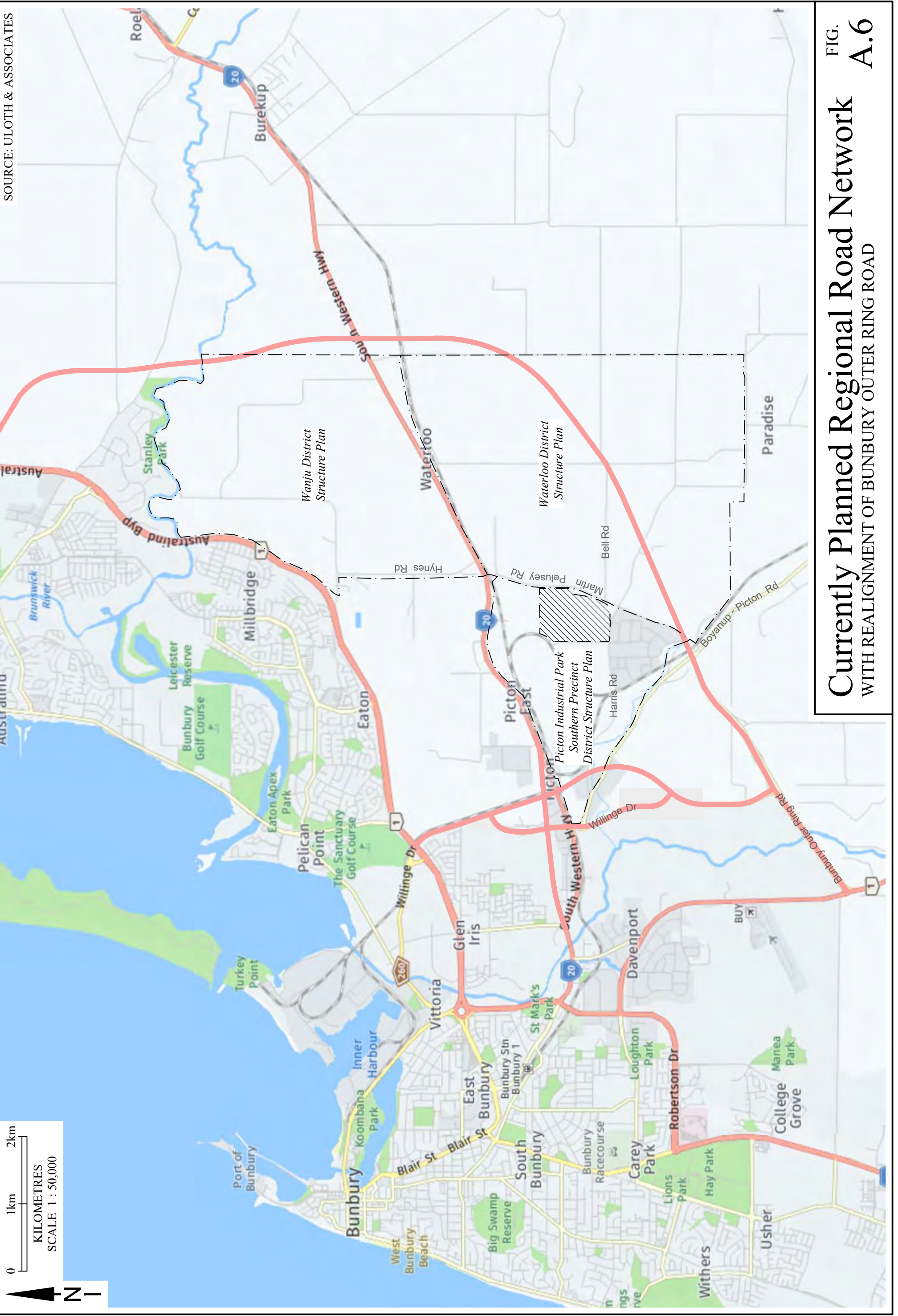
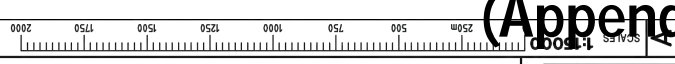


FIG. A.6
Currently Planned Regional Road Network
WITH REALIGNMENT OF BUNBURY OUTER RING ROAD



LEGEND:





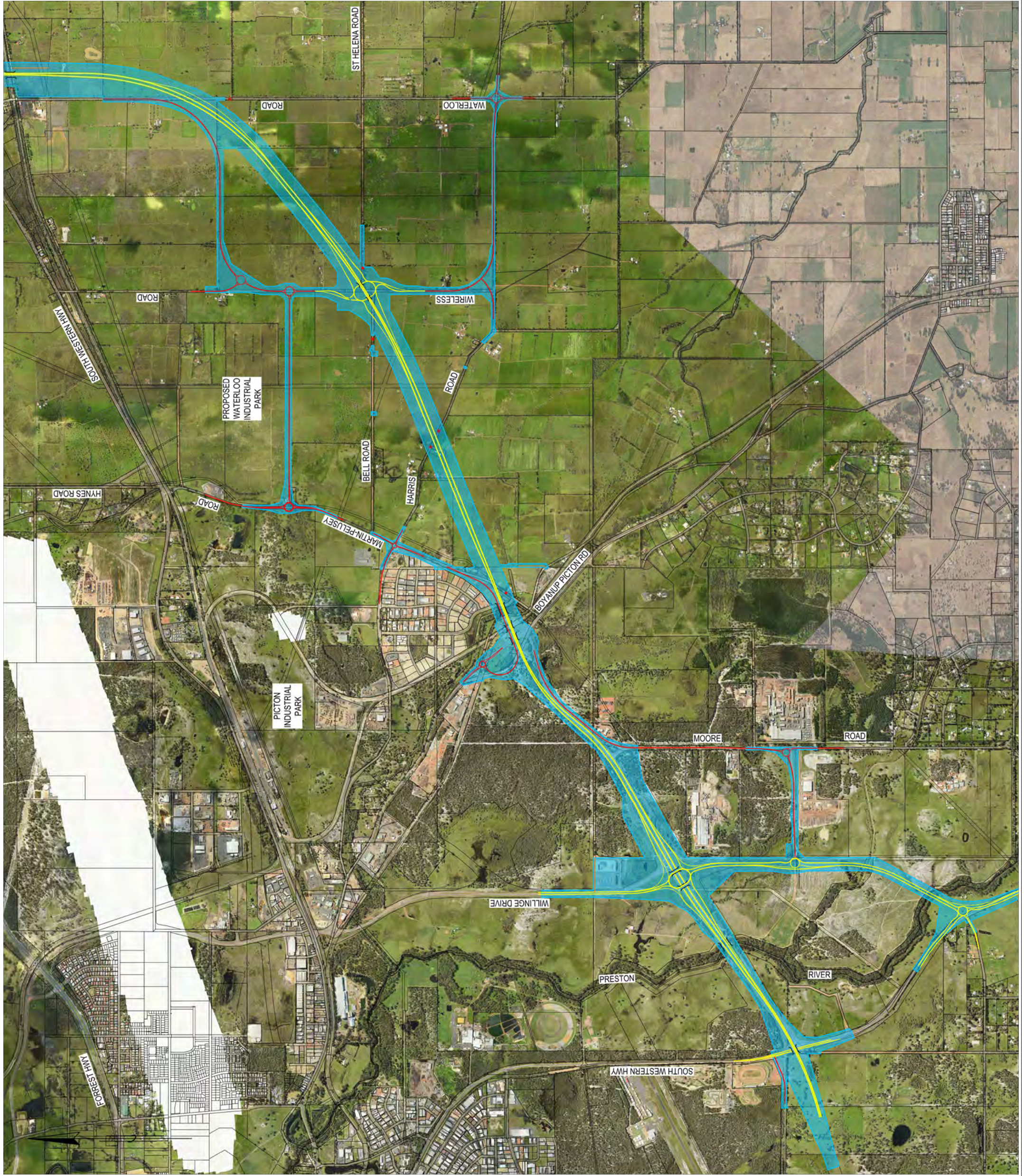
-  REFERRAL CORRIDOR
-  PROPOSED MAJOR ROAD
-  PROPOSED LOCAL ROAD
-  PROPOSED ACCESS ROAD

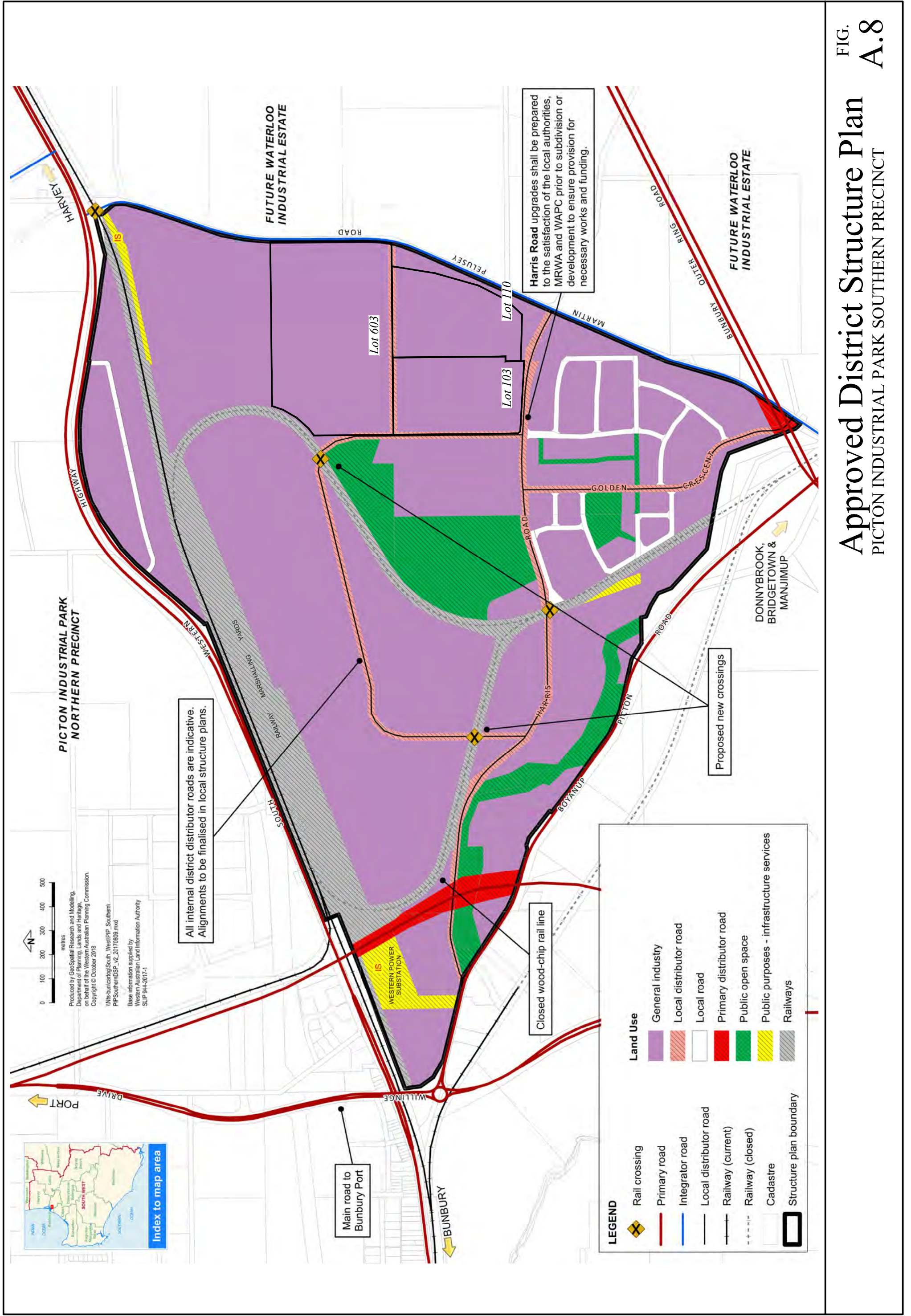
FIG. A.7

SKETCH ONLY
ULTIMATE PLANNING
UNDER DEVELOPMENT



DATE	15/04/2019	PREPARED	
SCALE	1:15000m @ A1	GRID	PCG 94
PROJECT	BUNBURY OUTER RING ROAD (H058)		
TITLE	BORR NORTH ACCESS STRATEGY PLAN SHEET 2 OF 2		
SKETCH NO.	BORR-01-SK-RD-0151		
REV	B		





SOURCE: ULOTH & ASSOCIATES

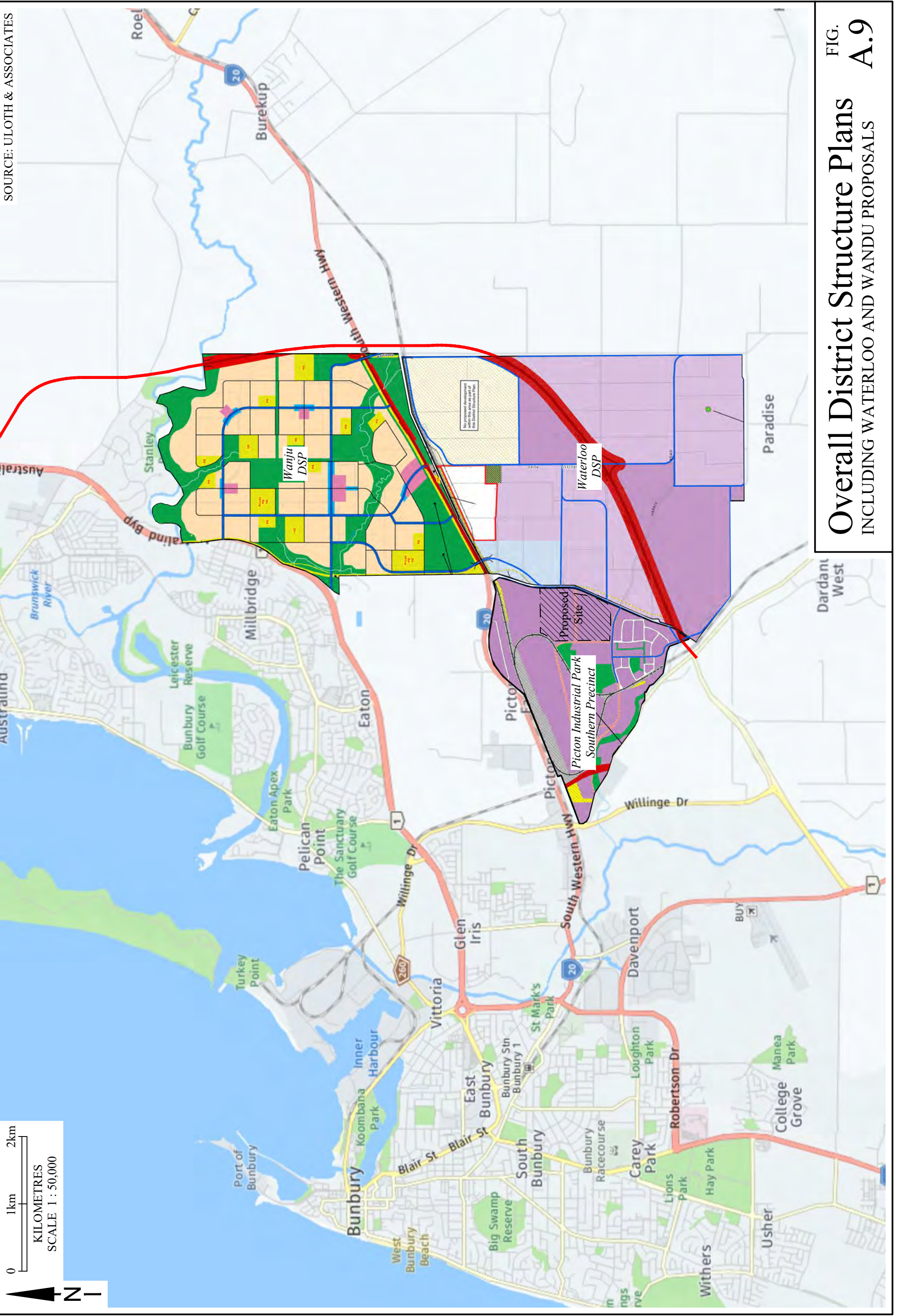


FIG. **A.9**
Overall District Structure Plans
 INCLUDING WATERLOO AND WANDU PROPOSALS

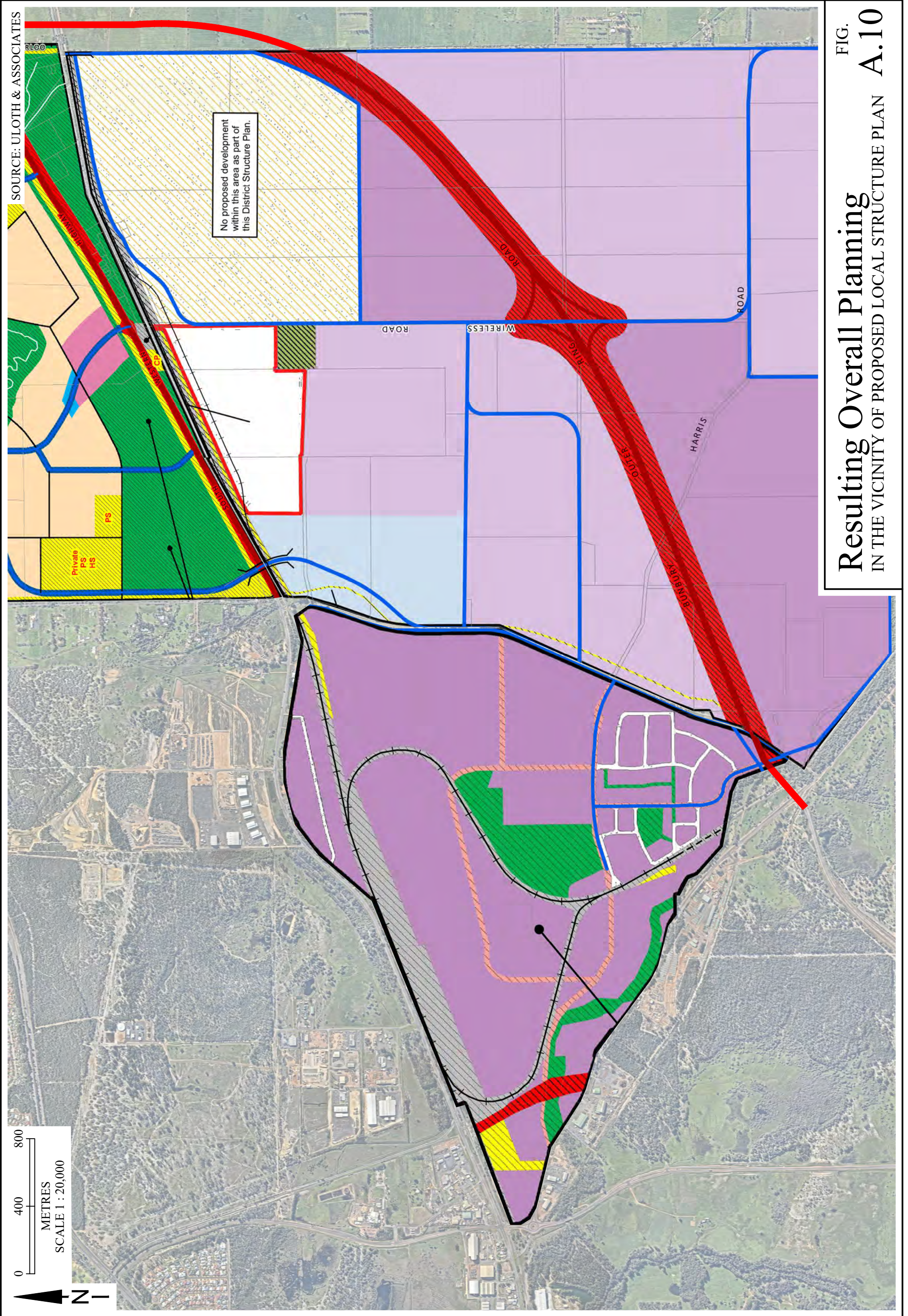


FIG. A.10
Resulting Overall Planning
IN THE VICINITY OF PROPOSED LOCAL STRUCTURE PLAN

SOURCE: ULOTH & ASSOCIATES

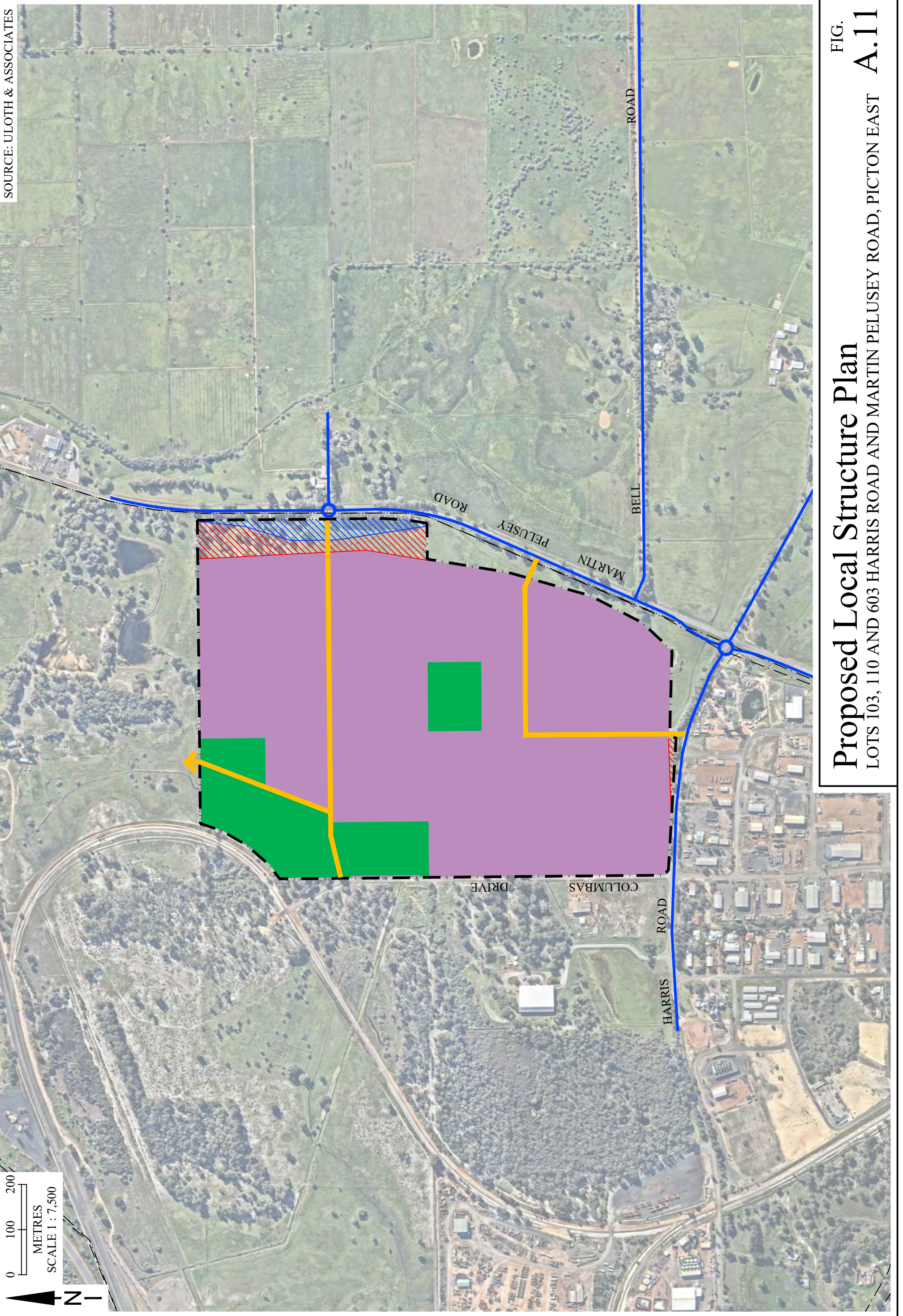
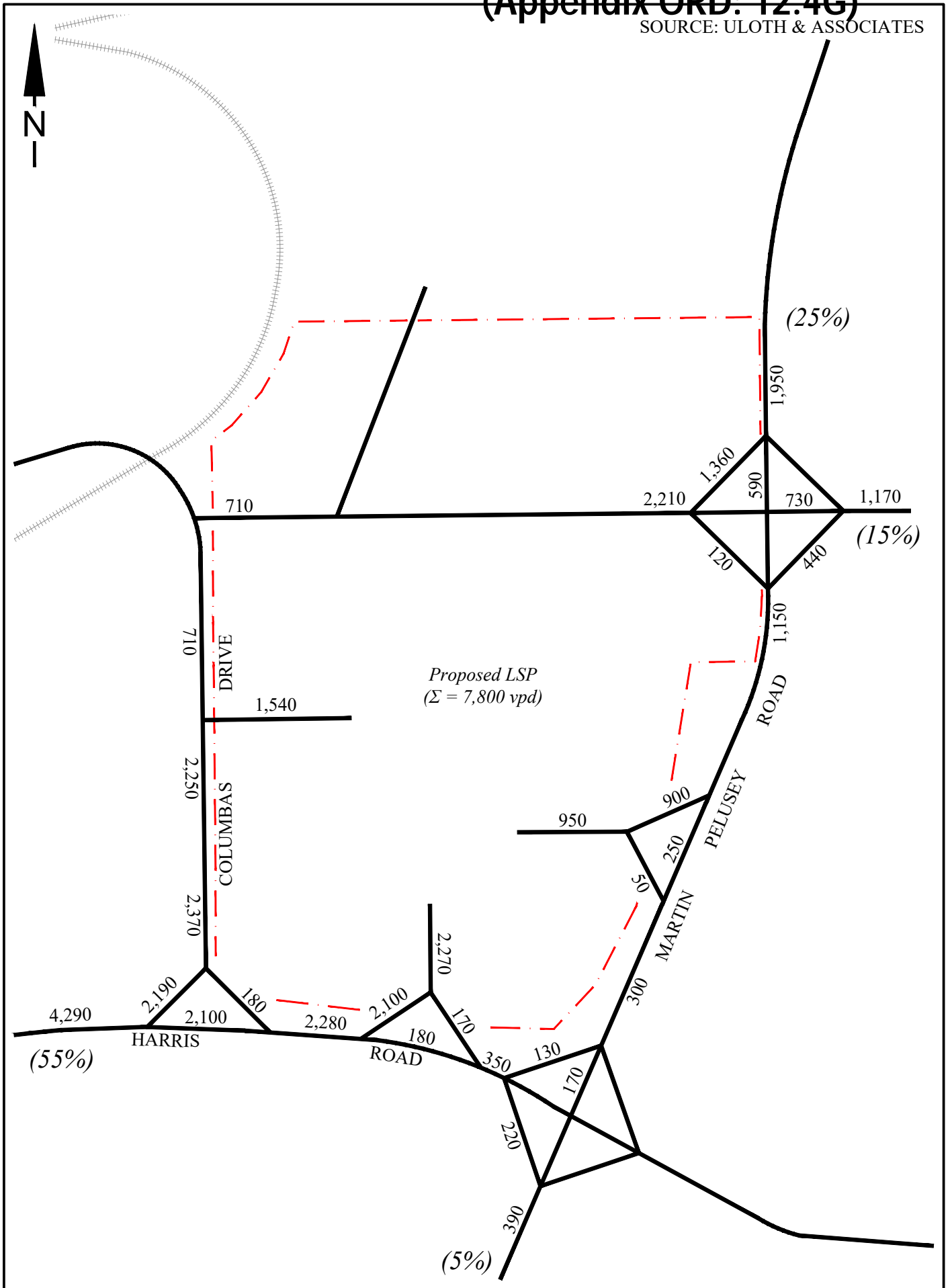
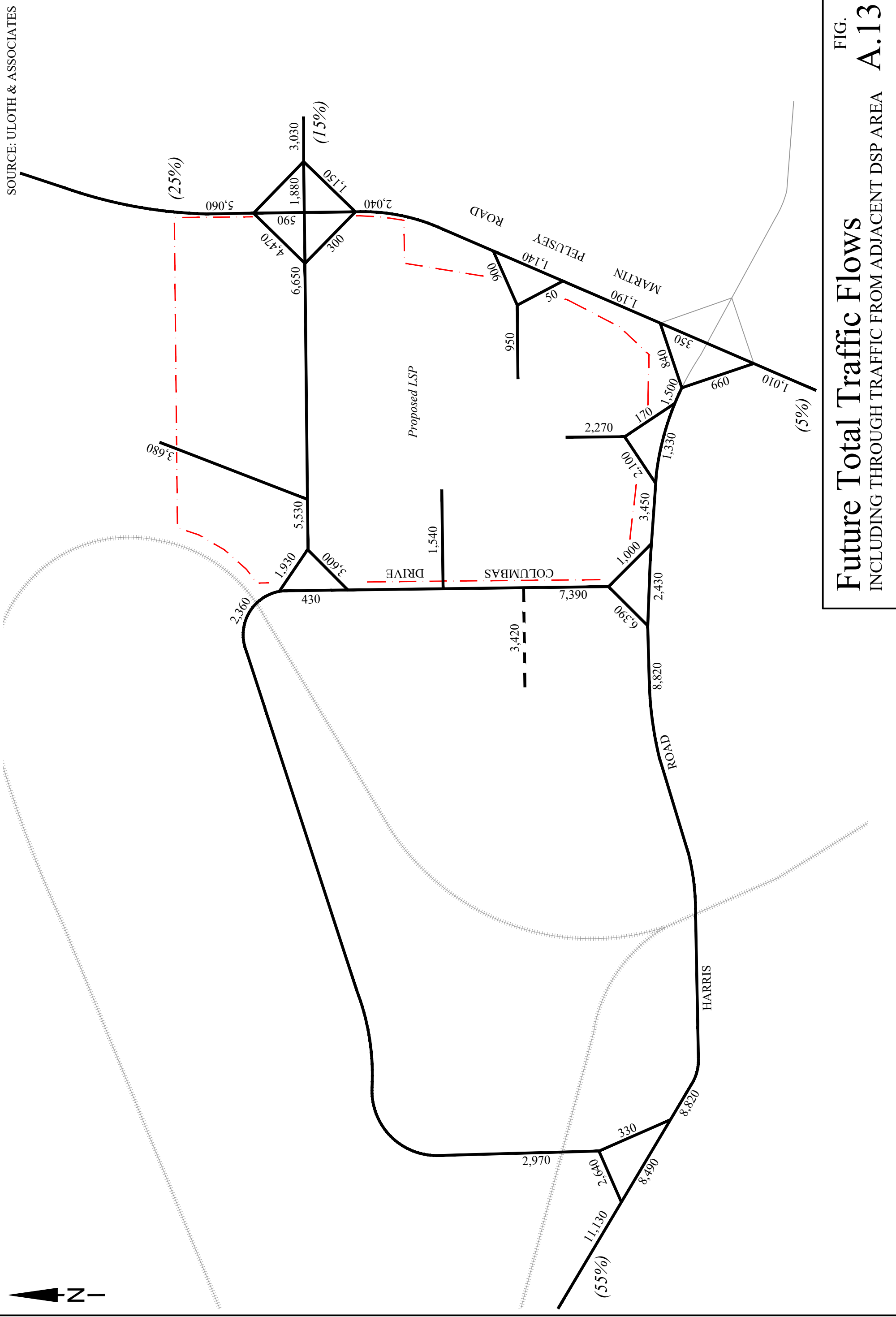


FIG. A.11
Proposed Local Structure Plan
 LOTS 103, 110 AND 603 HARRIS ROAD AND MARTIN PELUSEY ROAD, PICTON EAST



Future Development Traffic Flows
FOR PROPOSED LOCAL STRUCTURE PLAN

FIG.
A.12



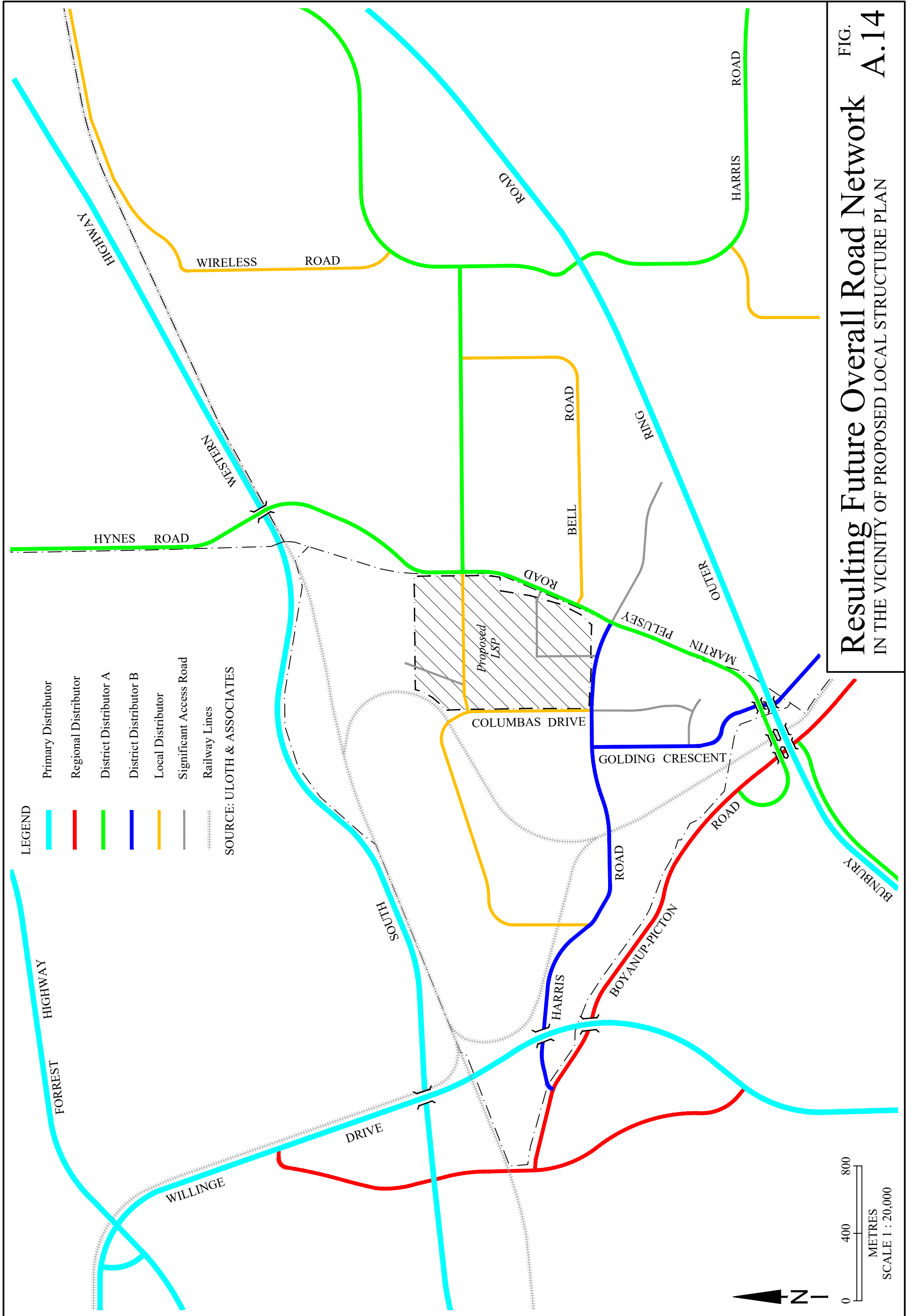


FIG. A.14
Resulting Future Overall Road Network
IN THE VICINITY OF PROPOSED LOCAL STRUCTURE PLAN

