



Shire of Dardanup

Sustainable Development
Directorate

APPENDICES

Booklet 3
Item 12.2.5

ORDINARY COUNCIL MEETING

To Be Held

Wednesday, 23rd of March 2022

Commencing at 5.00pm

At

e-Meeting via

www.dardanup.wa.gov.au

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Upon request.

[Appendix ORD: 12.2.5A]

LOT 303 (No. 15) ALBATROSS CRESCENT, EATON – SERVICE STATION DEVELOPMENT

Form 1 – Responsible Authority Report (Regulation 12)

DAP Name:	Regional JDAP	
Local Government Area:	Shire of Dardanup	
Applicant:	Peter Webb & Associates	
Owner:	West Coast Property Development Pty Ltd	
Value of Development:	\$2 million <input type="checkbox"/> Mandatory (Regulation 5) <input checked="" type="checkbox"/> Opt-In (Regulation 6)	
Responsible Authority:	Shire of Dardanup	
Authorising Officer:	Murray Connell, Manager Development Services Cecilia Muller, Principal Planning Officer	
LG Reference:	DAP – F0292154	
DAP File No:	DAP/22/02162	
Application Received Date:	7 January 2022	
Report Due Date:	23 February 2022	
Application Statutory Process Timeframe:	60 Days	
Attachments:	1. Application for Planning Approval Report 2. Development Plans: <ul style="list-style-type: none"> • Schedule Finishes A002 C • Site Plan Proposed A102L received on 4-03-2022 • Plan Building A103C • Canopy Plan A104 C • Elevation Building Sheet 1 A201 • Elevation Building Sheet 2 A202C • Elevation Canopy Car Sheet 1 A203C • Elevation Canopy Car Sheet 2 A204C • Elevation Building Sheet 3 A205E received on 4-03-2022 • Plan Site Signage S101F • Elevation Signage S201C • Stormwater Drainage Plan and Details C1 • Landscape Plan 01 • Plant Schedule 02 • Planting Specifications 03 • Vegetation Plan A105C received on 4-03-2022 3. Council Minutes	
Is the Responsible Authority Recommendation the same as the Officer Recommendation?	<input type="checkbox"/> Yes	Complete Responsible Authority Recommendation section
	<input type="checkbox"/> N/A	Complete Responsible Authority Recommendation section
	<input type="checkbox"/> No	Complete Responsible Authority and Officer Recommendation sections

Officer Recommendation:

It is recommended that Regional JDAP resolves to:

Approve DAP Application reference DAP/22/02162 and accompanying plans in accordance with Clause 68 of Schedule 2 (deemed provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of Clause 7.3 of the Shire of Dardanup Town Planning Scheme No. 3, subject to the following:

Conditions:

1. All development must be carried out in accordance with the approved plans listed below, unless amended with the written consent of the Shire. In the event of an inconsistency between the approved plans and a requirement of the conditions of this development approval, the requirement of the conditions prevail.
 - Schedule Finishes A002 C
 - Site Plan Proposed A102L received on 4-03-2022
 - Plan Building A103C
 - Canopy Plan A104 C
 - Elevation Building Sheet 1 A201
 - Elevation Building Sheet 2 A202C
 - Elevation Canopy Car Sheet 1 A203C
 - Elevation Canopy Car Sheet 2 A204C
 - Elevation Building Sheet 3 A205E received on 4-03-2022
 - Plan Site Signage S101F
 - Elevation Signage S201C
 - Stormwater Drainage Plan and Details C1
 - Vegetation Plan A105C received on 4-03-2022
2. The approved signs must at all times:
 - a. not be moving, pulsating, flashing, incorporate animation or movement into their design or structure.
 - b. only advertise services or products available on the subject lot.
 - c. have a minimum 1.5m setback to any part of any crossover.
 - d. be maintained to the approval of the Shire of Dardanup.
3. Prior to an application for a building permit, the Traffic Impact Statement must be updated to include the level of service for all intersections.
4. Should modifications to the road network be required as a result of the updated Traffic Impact Statement, these modifications shall be undertaken to the satisfaction of the Shire of Dardanup at the applicant's cost.
5. The approved fencing must be installed prior to the commencement of the use and thereafter maintained to the approval of the Shire of Dardanup.

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6. The landowner/proponent is responsible to ensure that the installations, activities and processes associated with the development are carried out at all times and in all respects in accordance with the Environmental Noise Impact Report prepared by Reverberate Consulting, dated 10 December 2021.
7. Prior to the works commencing, a landscaping plan for the subject site and the road verge along Eaton Drive, Blue Wren Drive and Albatross Crescent must be submitted to and approved by the Shire of Dardanup.

The approved landscaping plan must be fully implemented within 9 months of the use commencing, unless another date is specified in writing by the Shire of Dardanup, and must be maintained thereafter to the approval of the Shire of Dardanup.

8. Prior to the use commencing, an illumination report must be prepared by a suitably qualified person to demonstrate to the approval of the Shire of Dardanup that the completed development including signage complies with the requirements of '*AS/NZS 4282:2019 Control of the obtrusive effects of outdoor lighting*'.
lighting'.
9. All stormwater is to be disposed of on-site to the approval of the Shire of Dardanup.
10. The existing southern crossover onto Albatross Crescent as indicated on the approved plans shall be removed and the verge reinstated to the approval of the Shire of Dardanup.
11. All vehicle crossovers are to be installed and constructed to the specification of Council Policy '*Infr CP050 – Crossovers – Approvals, Standards and Subsidy*' to the approval of the Shire of Dardanup.
12. The area set aside for the parking of vehicles together with the associated access as shown on the approved plans shall:
 - a. be installed to the approval of the Shire of Dardanup prior to the use commencing;
 - b. be maintained thereafter to the approval of the Shire of Dardanup;
 - c. be made available for such use at all times and not used for any other purpose unless otherwise approved in writing by the Shire of Dardanup;
 - d. be properly formed to such levels that it can be used in accordance with the approved plans and approved use;
 - e. be drained and sealed with an all-weather seal coat to the approval of the Shire of Dardanup;
 - f. have the boundaries of all vehicle spaces clearly indicated on the ground in conformity with the approved plans; and
 - g. be designed in accordance with AS/NZS 2890 including the provision of accessible car parking for people with disabilities.

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13. Prior to works commencing, engineering drawings and specifications are to be submitted to and approved by the Shire of Dardanup, and works undertaken in accordance with the approved plan for the provision of:
 - a. a shared path and pedestrian crossing point on Eaton Drive connecting the development with the existing footpath on the western side of Eaton Drive; and
 - b. a shared path along the northern side of Blue Wren Drive connecting to the existing footpath on Albatross Crescent.
14. All loading and unloading of goods must only be carried out within the lot boundaries and must not disrupt the circulation and parking of vehicles on the land.
15. Suitable arrangements must be made to the satisfaction of the Shire of Dardanup to effectively screen vehicle headlight impacts from the vehicle crossover on Blue Wren Drive to the residential properties opposite the application site.

Advice Notes:

- i. Detailed drawings are to be submitted with the building licence application identifying means of access from car parking areas to the entrance of the building and throughout the building, as required by AS1428.1-2009.
- ii. A demolition permit must be obtained from the Shire of Dardanup prior to the removal/demolition of the existing buildings.
- iii. Storage of dangerous goods must comply with the *Dangerous Goods Safety Act 2004* and its regulations and all relevant requirements of the Department of Mines, Industry Regulation and Safety.
- iv. The hardstand vehicle refuelling area is to be designed to ensure that hydrocarbons cannot enter the stormwater system.
- v. Any clearing permit that may be required are the responsibility of the landowner/proponent

Details: Outline of the development application

Region Scheme	Greater Bunbury Region Scheme
Region Scheme - Zone/Reserve	Urban
Local Planning Scheme	Shire of Dardanup Town Planning Scheme No. 3
Local Planning Scheme - Zone/Reserve	Mixed Business
Structure Plan/Precinct Plan	Eaton Fair Activity Centre Plan
Structure Plan/Precinct Plan - Land Use Designation	Mixed Business
Use Class and permissibility:	Service Station – 'P' use
Lot Size:	2,113m ²
Existing Land Use:	Community purpose and office
State Heritage Register	No
Local Heritage	<input checked="" type="checkbox"/> N/A
Design Review	<input checked="" type="checkbox"/> N/A
Bushfire Prone Area	No
Swan River Trust Area	No

Proposal:

The application seeks approval for the construction of a 7-Eleven service station on Lot 303 (No. 15) Albatross Crescent, Eaton on the corners of Blue Wren and Eaton Drives. It is proposed that the facility will operate 24 hours a day.

Key aspects of the application are as follows:

- 215m² convenience store building.
- Fuel bowsers with 6 refuelling points for light vehicles only.
- 2 x 90,000L underground tanks.
- Associated car parking, signage and landscaping.

The application contains the following technical reports:

- Stormwater design;
- Landscape plan;
- Traffic impact statement;
- Dangerous goods technical note; and
- Acoustic assessment.

Background:

The subject site is located on the corners of Eaton Drive, Blue Wren Drive and Albatross Crescent. Access is currently via Albatross Crescent and a new 10m wide crossover to Blue Wren Drive is proposed.

The site currently is occupied by a community facility and office building for South West Community Care and is located 200m south of the Eaton Fair Shopping Centre, which is a large district shopping centre.

Surrounding land uses to the north and east include a medical centre and automotive tyre facility. To the south land is used for residential purposes and a group dwelling development is directly opposite the site to the south.

Legislation and Policy:

Legislation

Planning and Development Act 2005
Planning and Development (Local Planning Schemes) Regulations 2015
Shire of Dardanup Town Planning Scheme No. 3.

Structure Plans/Activity Centre Plans

Eaton Fair Activity Centre Plan
Eaton Commercial Centre Design Guidelines

Local Planning Policies

SDev CP084 – Advertising Signage

Consultation:

Public Consultation

A 'Service Station' is a permitted use and no consultation is required.

Referrals/consultation with Government/Service Agencies

No referrals were undertaken.

Planning Assessment:

The following matters have been identified as key considerations for the determination of this application:

- Zoning and Land Use Class Permissibility
- Eaton Fair Activity Centre Plan
- Noise
- Signage
- Traffic
- Lighting

The application has been assessed under cl.67 (2) of the deemed provisions of the *Planning and Development (Local Planning Schemes) Regulations 2015* with those matters relevant to the application detailed below.

(a) The aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area.

The following aims and provisions of the Shire of Dardanup Town Planning Scheme No. 3 (TPS3) are relevant to this application:

1.3 *Objects of the Scheme*

1.3 (b) *to secure the amenity, health and convenience of the Scheme Area and inhabitants thereof.*

1.4 (a) *to consolidate the urban areas of Dardanup, Burekup ad Eaton and to control the building on those areas of new structures between or adjacent to existing buildings.*

2.2 *Objectives of the Zones*

Mixed Business Zone

- *to provide for a variety of commercial activities such as warehouses, bulky goods showrooms, service industries, offices, wholesaling, motor repair facilities and extensive retail which by virtue of their scale and character are generally not appropriate to, or cannot conveniently or economically, be accommodated within the Business-Commercial or Industry zones.*
- *to ensure the design and landscaping of development provides a high standard of safety and amenity and contributes towards a sense of place and community within the service area.*

It is considered that the proposed development is of a design standard that will contribute to the overall amenity of the service area and is consistent with the objective for the Mixed Business zone. The application has demonstrated that any impacts on nearby residential areas can be appropriately managed.

2.3 Zones and Uses

The application seeks approval for a 'Service Station' use class which is defined as:

service station means premises used for –

- (a) *The retail sale of petroleum products, motor vehicle accessories and goods of an incidental/convenience retail nature; and*
- (b) *The carrying out of greasing, tyre repairs and minor mechanical repairs to motor vehicles, but does not include premises used for a transport depot, panel beating, spray painting major repairs or wrecking.*

The Zoning Table of TPS3 lists 'Service Station' as a 'P' use under the 'Mixed Business' zone which means that the use is permitted providing the use complies with the relevant development standards and the requirements of the Scheme.

(b) The requirements of orderly and proper planning including any proposed local planning scheme or amendment to this Scheme that has been advertised under the *Planning and Development (Local Planning Schemes) Regulations 2015* or any other proposed planning instrument that the local government is seriously considering adopting or approving.

Council has adopted a draft new Local Planning Scheme No. 9 however this is yet to be advertised and as such, the draft scheme is not yet considered a seriously entertained document. Notwithstanding, under the draft scheme it is proposed that the 'Mixed Business' zone will become the 'Service Commercial' zone and a 'Service Station' will become a discretionary use in that zone.

(d) Any environmental protection policy approved under the *Environmental Protection Act 1986* section 31(d)

The EPA's *Guidance for the Assessment of Environmental Factors – Separation Distances between Industrial and Sensitive Land Uses* recommends a generic 50m buffer between services stations and sensitive land uses (including residential uses). The closest dwelling is approximately 23m to the south of the development.

The Guidance identifies potential impacts from gas, odour and noise emissions. These matters are commonly considered under the Dangerous Goods Storage Licence requirements by Department of Mines, Industry Regulation and Safety. A 'Dangerous Goods Technical Note' has been submitted with the application which demonstrates that the proposed development will comply with all the applicable dangerous goods regulatory requirements.

With regard to noise, an 'Acoustic Assessment' has been submitted with the application which demonstrates that the proposed development will comply with the *Environmental Protection (Noise) Regulations 1997* if structural items are installed.

These items include a 2.5m high pre-cast concrete wall along Blue Wren Drive with lower fencing along Albatross Crescent. The applicant has submitted a design for the pre-cast wall (refer to Elevation Building Sheet 3 A205E). The design is nature based and of a high quality, and the design and colour scheme is supportable.

It is also noted that some ongoing noise management controls must be implemented when the premises is operational and it is recommended that this be imposed as a condition on the development approval.

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(fa) Any local planning strategy for this Scheme endorsed by the Commission

The WAPC endorsed the Shire of Dardanup Local Planning Strategy on 4th May 2015. The site is within an area identified as 'District Centre'. The Strategy recommends land along Eaton Drive to be used for non-retail, commercial uses and the proposed development of a service station is therefore consistent with the Strategy.

(g) Any local planning policy for the Scheme area

The Shire's policy '*SDev CP084 – Advertising Signage*' provides guidance on the assessment of advertisement signs. Under the policy, a signage strategy is required for multiple signs. Where a sign is not listed or defined in the policy, it must be considered against the provisions of a sign definition and criteria of a 'nearest fit'.

The application contains a signage strategy as per plans 'Plan Site Signage S101F' and 'Elevation Signage S201C'. The following assessment is provided for the signs that are seeking some variations to the requirements of the policy.

Sign type	Requirement	Proposed	Assessment
Sign S1 Although attached to the roof, the nearest fit for this sign is a 'Monolith Sign'	Height: 6m	10m	Although the sign itself is only 7m tall, the overall height of the sign as attached to the building is 10m. The increase in height is considered acceptable in the context of surrounding developments. The height is lower than other approved signs along Eaton Drive, such as the McDonald's sign that is 12m high and the Chicken Treat sign that is 15m high.
Sign S2 Monolith Sign	Setback: 1.5m from any part of a crossover	Undefined	It has not been demonstrated that the sign will not impede vehicular sightlines and therefore a condition is recommended to ensure that the sign is setback 1.5m from any part of a crossover.
Sign S3 The nearest fit for this sign is a 'Freestanding Banner Sign'	Not within the street setback line	Within the 9m setback.	The sign located on the north east corner fronting Albatross Crescent. This sign is within the 9m primary setback, however it does not impede any sightlines and there are other signs along the street frontage.
	Setback: 1.5m from any part of a crossover	Undefined	The sign located adjacent to the proposed crossover onto Blue Wren Drive. It has not been demonstrated that the sign will not impeded vehicular sightlines and therefore a condition is recommended to ensure that the sign is setback 1.5m from any part of a crossover

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Sign type	Requirement	Proposed	Assessment
Sign S4 Awning Sign	Height: 500mm	1m for all awning signs	The canopy and awning for the building is 1m wide and the proposed signs will not protrude above the roof line. The signs blend in with the corporate colours of the development and are proportionate to the structures to which they will be attached.
	Only located near the entrance to the tenancy.	Located at the building entrance and also on the fuel bowser canopy.	

The overall signage strategy is commensurate with other service station developments and acknowledges the corporate branding of the development. Signage is not considered excessive and conditions are recommended to ensure that any illumination of the signs is in accordance with the relevant Australian Standard for the control of the obtrusive effects of outdoor lighting.

(h) Any structure plan or local development plan that relates to the development

The site is within the Eaton Fair Activity Centre Plan (EFAC) and is located in the 'Mixed Business Precinct' of the EFAC. The guiding principles and objectives most relevant to this application area:

- (b) *Allow appropriate businesses to locate and develop in close proximity to the established Eaton urban area for the convenience of its residents.*
- (d) *Provide efficient vehicle access and circulation with pedestrian focus a priority.*
- (h) *Seamlessly connect all areas of the EFAC to the surrounding urban area by a safe and convenient pedestrian and cycle network.*

The objective of the Mixed Business Precinct is to provide for the development of the Eaton Commercial Centre in accordance with the Mixed Business zone of TPS3 and the Eaton Commercial Centre Design Guidelines. The following land uses are preferred in the Mixed Business Precinct:

- Bulky goods showroom
- Health service
- Office
- Eating and drinking
- Light and service industry

The following assessment is provided against the Eaton Commercial Centre Design Guidelines and TPS3.

Provision	Requirement	Proposal	Assessment
Front Setbacks	9m	26.76m setback from Albatross Crescent	Complies
Rear Setbacks	Nil	300mm setback from Eaton Drive	Complies
Secondary Street Setbacks	3m	2.18m setback from Blue Wren Drive	Variation of 0.82m sought.

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Provision	Requirement	Proposal	Assessment
			The variation is considered minor and only for a small portion of the building. A number of nearby developments have a reduced setback such as at Lot 7 (No. 4) Albatross Crescent and 17 (No. 9) Cassowary Bend.
Side Setback	One or both sides nil	4.9m	Complies
Plot Ratio and Coverage	Plot Ratio 1.5 Coverage 70%	Plot Ratio 0.20 Coverage 21%	Complies
Height	Two storeys with a wall height of 7.5m	Single storey Wall height (inclusive of screen) is 6.6m	Complies
	Signage above the ridge height not be permitted	Signage device protrudes above the roofline by 3.4m	Refer to the assessment of signage contained previously in this report
Parking and Access	One space for every 15m ² net lettable area 14.3 bays are required for a building of 215m ² Due to verge parking provided, one car parking bay per lot may be included in the car parking requirement Therefore 13.3 bays required	13 bays provided (inclusive of the air/water bay) An additional 6 bays are available for refuelling	Complies
	Site line truncations apply to vehicular access ways	Fencing reduced to 750mm within 1.5m of the crossover along Blue Wren Drive	Complies
Landscaping	8%	13.85%	Complies
Canopy shade trees	One tree for every four open-air parking bays	5 trees proposed on-site	Complies
	4 trees required		
Lighting	The area in front of the setback shall be	Given the proposal is for a 24-hour service station	Complies

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Provision	Requirement	Proposal	Assessment
	lit at night for security	operation, the nature of the development is such that it will be lit at night.	
Fencing	1.8m high permeable fencing required along Eaton Drive and Blue Wren Drive	2.5m high acoustic wall proposed along Blue Wren Drive, and fencing along the truncation and Albatross Crescent for noise mitigation purposes	Refer to the assessment of noise contained previously in this report
	Side boundary fencing within the 9m setback from the verge shall be no higher than 1.2m with the remainder to be 1.8m high	The fence along the northern boundary would be retained in its current form	Complies

- (m) The compatibility of the development with its setting, including –
- (i) the compatibility of the development with the desired future character of its setting; and
 - (ii) the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development.
- (n) The amenity of the locality including the following –
- (i) environmental impacts of the development
 - (ii) the character of the locality
 - (iii) social impacts of the development

The site is located within an established commercial pocket adjacent to the Eaton Fair Activity Centre. The proposed service station is a permitted use in the zone and is compatible with surrounding development and the character of the area. Although abutting the site to the south is a residential area the application has demonstrated that any impacts are minimal and can be appropriately managed. The application report does not address the potential impact of vehicle headlights on residents across the exit on Blue Wren Drive. Therefore, it is recommended that the development approval be conditioned to ensure that suitable arrangements are made to eliminate impacts from vehicle headlights.

As the facility will provide convenience goods to residents in the locality it is considered appropriate that new and upgraded pedestrian paths be provided to connect to the overall network. This is supported by the EFAC Plan which places considerable emphasis on pedestrian priority and network connections.

(p) Whether adequate provision has been made for the landscaping of the land to which the application relates and whether any trees or other vegetation on the land should be preserved

The application proposes the removal of the existing vegetation on the Eaton Drive verge with the exception of 4 trees and the subsequent replacement with landscaping as per the landscaping plan. This is most likely to enable greater exposure of the site to Eaton Drive.

Landscaping of verge areas is supported under the Eaton Commercial Centre Design Guidelines with developers being required to give an ongoing commitment to its establishment and maintenance. The applicant has agreed to this and indicated that the trees to be removed within the Eaton Drive verge will be replaced with local native varieties which grow to a lower height.

(s) The adequacy of —

(i) the proposed means of access to and egress from the site; and

(ii) arrangements for the loading, unloading, manoeuvring and parking of vehicles

(t) The amount of traffic likely to be generated by the development, particularly in relation to the capacity of the road system in the locality and the probable effect on traffic flow and safety

A 'Transport Impact Statement' (TIS) has been submitted with the application which demonstrates the adequacy of vehicle access, turning movements and car parking. Whilst traffic flows have been provided the TIS fails to clearly demonstrate the level of service at each of the intersections and it is recommended that this information be supplied as a condition of the development approval.

Conclusion

The proposed service station is a permitted use in the zone and is compatible with surrounding development and the character of the area. The site is located within an established commercial pocket and the application has demonstrated that any impacts on nearby residential areas will be minimal and can be appropriately managed.

The application complies with the planning framework applicable to the site and the requirements of orderly and proper planning for the area. Conditional approval is therefore recommended.

Application for Planning Approval

Proposed 7-Eleven “Service Station”

Lot 303 (#15) Albatross Crescent, Eaton

Shire of Dardanup / Regional JDAP

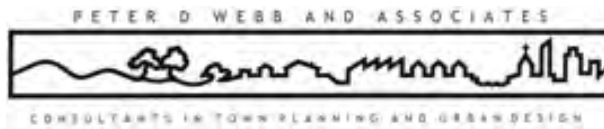
21 December 2021



This Planning Report has been prepared by **Peter Webb & Associates** for the proposed 7-Eleven Service Station at Lot 303 (#15) Albatross Crescent, Eaton

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DOCUMENT HISTORY & DETAILS

Author	Revision	Approved by	Date
Nik Hidding	R01	N Hidding	21/12/2021

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Project: 7-Eleven Service Station - Eaton
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ANNEXURES:

- Annexure 1:** Certificate of Title
 - Annexure 2:** Development Plans (Project Consultants)
 - Annexure 3:** Stormwater Design (JCCE)
 - Annexure 4:** Landscape Plan (Urban Retreat Garden Design)
 - Annexure 5:** Traffic Impact Statement (Transcore)
 - Annexure 6:** Dangerous Goods Technical Note (Cadre Engineering)
 - Annexure 7:** Acoustic Assessment (Reverberate Consulting)
-

1.0 INTRODUCTION

This Planning Report has been prepared by Peter Webb & Associates (**PWA**) on behalf of Tomahawk Property as part of an Application for Planning Approval for the development and use of Lot 303 (#15) Albatross Crescent, Eaton (**Subject Site**) for a proposed 7-Eleven “Service Station”.

This report provides a Planning assessment of the proposed development against the relevant State and local Planning framework. The information contained in this report confirms that the proposed “Service Station” use is appropriate for the site and reflects the applicable planning framework.

1.1 DEVELOPMENT ASSESSMENT PANEL (DAP) DETERMINATION

As the anticipated construction cost of the project is **\$2 million**, the Applicant has “opted in” to the Development Assessment Panel (**DAP**) process and therefore, this Development Application will be required to be determined by the Regional Joint Development Assessment Panel (**JDAP**).

Accordingly, please find **attached** our completed Shire of Dardanup Application for Development Approval Form, GBRS Form 1 and DAP Form 1, all signed by the current owner.

1.2 PRE-APPLICATION MEETING

A pre-application meeting was held on 3 August 2021 between Tom Carmody (Tomahawk Property), Regan Harray (Development Manager - 7-Eleven) and Cecilia Muller (Principal Planning Officer - Shire of Dardanup) to discuss the proposed Service Station development.

2.0 OVERVIEW, LEGAL DESCRIPTION & SITE DETAILS

2.1 OVERVIEW

Applicant:	Peter Webb & Associates (PWA)
Landowner:	West Coast Property Developments Pty Ltd
Local Government:	Shire of Dardanup
Scheme:	Local Planning Scheme No. 3 (LPS3)
Zoning:	LPS3: "Mixed Business"
	GBRS: "Urban"
Current Uses:	Existing Community Centre
Proposed Use:	"Service Station" ("P" use)
Construction Cost:	\$2 million

2.2 LEGAL DESCRIPTION OF LAND

This development application refers to Lot 303 (#15) Albatross Crescent, Eaton, the details of which are provided in **Table 1** below.

Table 1: Legal Description of Land

Lot	Plan	Vol/Folio	Area	Address	Proprietors
303	38275	2597/585	2,113m ²	15 Albatross Crescent, Eaton	West Coast Property Developments Pty Ltd

The Certificate of Title for the subject site is attached at **Annexure 1**.

2.3 SITE DETAILS

The Subject Land comprises Lot 303 (#15) Albatross Crescent, Eaton. The site is located on the corners of Blue Wren Drive, Eaton Drive and Albatross Crescent. The site is within the Shire of Dardanup local government area in a Mixed Business zone.

The subject land has a total land area of 2,113m² and is currently used as a Community Centre and office facility.

The subject land is located 200m south of the Eaton Fair Shopping Centre, which is a large regional shopping centre.

The site has a frontage of approximately 28.54m to Eaton Drive as well as a truncation of 8.34m at the corner of the site (cnr of Eaton Drive and Blue Wren Drive), a frontage of 50.02m to Blue Wren Drive with a truncation of 8.6m at the corner of Blue Wren Drive and Albatross Crescent and a frontage of 28.45m to Albatross Crescent.

Surrounding uses include the two storey Eaton Medical Centre to the north of the subject land, and the Dapco Tyre & Auto Service Facility which is located on the opposite corner of Albatross Crescent and Blue Wren Drive. The service bays of the Dapco facility face directly out towards Blue Wren Drive.

Aerial Photographs (wide view and zoomed view) of the subject land are included at **Figure 1**.



Figure 1: Aerial Photographs (PlanWA)

Photographs of the site, adjacent roads and surrounding properties are included below to provide context of the subject site.



Photo 1: The subject site as viewed from the corner of Blue Wren Drive and Albatross Crescent



Photo 2: The existing building on the subject land



Photo 3: The existing car park on the subject land



Photo 4: The rear of the existing building on the subject land.



Photo 5: Eaton Medical Centre on the adjacent property to the north of the subject land.



Photo 5: Dapco Tyre & Auto Service Facility on the opposite corner of Albatross Cres and Blue Wren Dr.

3.0 THE PROPOSAL

The proposal is to develop a 7-Eleven Service Station on the subject land.

Plans of the proposed Service Station development (prepared by Project Consultants) are included at **Annexure 2** of this Report.

The plans included at Annexure 2 include:

- Site Plan
- Floor Plan & Fuel Canopy Plan
- Building Elevations
- Fuel Canopy Elevations
- Overall Elevations
- Site Signage Plan
- Signage Elevations
- External Finishes Schedule

A Feature Survey of the subject land is also included at Annexure 2.

3.1 SUPPORTING PLANS & REPORTS

Consultant reports and other supporting information has been prepared to assist in the assessment of this application, including Civil Drawings, Landscape Plan, a Traffic Impact Assessment, an Acoustic Report and a Dangerous Goods Technical Note.

The reports and documentation which are provided in support of this application are detailed in **Table 2** below and are attached as **Annexures** to this Report.

Table 2: Supporting Plans & Reports

Consultant	Plan/Document	Annexure
Project Consultants	Development Plans + Feature Survey	2
JCCE	Stormwater Design	3
Urban Retreat Garden Design	Landscaping Plan	4
Transcore	Transport Impact Statement	5
Cadre Engineering	Dangerous Goods Technical Note	6
Reverberate Consulting	Acoustic Report	7

3.2 BUILDING & DESIGN

The proposed Service Station development comprises of the following components:

- 215m² convenience store building;

- Quality building design incorporating a 6.60m high screening feature located around the corner part of the building;
- Balance of building at 4.50m height;
- Blade feature to 8.00m height on building incorporating 7-Eleven logo;
- Fuel bowsers with canopy with 6 refuelling points for light vehicles;
- Pylon sign incorporated into the corner of the building to 10.00m height;
- Refuse Store / Loading bay;
- Associated car parking (12 bays + 1 air/water bay); and
- 292.6m² of landscaping areas (representing 13.85% of the site area).

The design of the proposed development is typically commercial in appearance and will adopt the corporate branding associated with 7-Eleven.

The convenience store building is well designed with a 4.76m high wall height, plus feature blade to 8.00m height. The main entry to the convenience store building is from the forecourt area, which includes a large glazed shopfront.

The convenience store building proposes to incorporate an internal area of 215m², which will include a point-of-sale and offer for sale of goods generally expected in a convenience store, including food and drink products and other associated items for the convenience of motorists, workers and residents in the area. The convenience store building will also include standard amenities, administration office, coolroom, store room and food preparation areas. Refer to the Floor Plan for additional detail.

New car parking bays (inclusive of a disabled bay) are proposed directly in front of the convenience store building for ease of access.

The convenience store building is set back 300mm from the Eaton Drive boundary and 2.185m from the Blue Wren Drive boundary.

The light vehicle fuel canopy is set back approximately 6.00m from Blue Wren Drive and is sited perpendicular to it.

An internal pedestrian footpath extends from in front of the convenience store building into the verge area of Blue Wren Drive. No footpath currently exists in Blue Wren Drive.

3.3 LANDSCAPING & FENCING

A total landscaping area of 292.6m² (or 13.85% of the site) is proposed and includes landscaping along the northern boundary and along the Blue Wren Drive boundary. A consolidated landscaping area is proposed in the corner of the site adjacent to Albatross Crescent and Blue Wren Drive, enabling the planting of trees. A total of 5 trees are proposed to be planted in the landscaping areas on the site.

The verge area of Eaton Drive is proposed to be completely landscaped as part of the proposal, enabling a quality landscaping area to be planted forward of the proposed development.

A Landscaping Plan has been prepared by Urban Retreat Garden Design in support of this application, which is included at **Annexure 4**. The Landscaping Plan details the plant species and plant numbers.

It is considered that amount of landscaping proposed is acceptable and complies with the Shire's requirements.

Fencing treatments include a 2.50m high masonry wall along Blue Wren Drive on the eastern side of the proposed driveway (reduced to 750mm within 1.50m of the crossover), and a lower 1.20m - 1.80m high Colorbond fence along the truncation and part way along the Albatross Crescent street boundary, as shown in the plans.

The 1.80m high Colorbond fence along the northern boundary (shared with #13 Albatross Cres) will be retained.

No fence is proposed along the Eaton Drive boundary, as the proposed convenience store building is intended to interface directly with Eaton Drive, and at the corner with Blue Wren Drive.

3.4 ACCESS & MOVEMENT

All roads adjacent to the site area local roads under the care of the Shire of Dardanup.

The development is proposed to be accessed via an existing crossover to Albatross Crescent which is proposed to be slightly modified. A second existing crossover to Albatross Crescent, located closer to the intersection of Blue Wren Drive will be removed (refer Site Plan).

A new 10.00m wide crossover to Blue Wren Drive is proposed in order to provide adequate circulation of vehicles including the fuel tanker.

Fuel tankers are proposed to enter the site from Blue Wren Drive and circulate through the site around the fuel canopy in a clockwise manner. To exit, it is intended that the fuel tanker leave the site back to Blue Wren Drive.

A Transport Impact Statement (**TIS**) has been prepared by Transcore in support of the proposed development (refer **Annexure 5**).

Transcore was commissioned to undertake a TIS to address the adequacy of the vehicle access arrangements, vehicle and tanker turning requirements and on-site parking. The internal turning requirements for the fuel tanker in and around the site have been checked, and it is confirmed that the proposed crossover arrangements are satisfactory.

The car parking proposed for the development has also been reviewed and it is confirmed that the parking bays numbers are satisfactory.

3.5 CAR PARKING BAYS & LOADING BAY

The development proposes to provide a total of 12 marked car parking bays (plus one (1) air/water bay) and one (1) loading bay to support the proposed Service Station.

In addition to the marked car parking bays, there is opportunity for six (6) cars to use the fuel bowsers to remain parked after refuelling.

3.6 SIGNAGE

This application proposes an illuminated Pylon Sign located on top of the proposed convenience store building with a total height of 10.00m. The actual height of the pylon sign structure is 7.00m, but it is attached to the building.

There is also a proposed pylon price sign at the Albatross Crescent frontage which is proposed at a height of 6.00m.

There are a range of other smaller signs proposed on buildings and on the fuel canopy (refer Elevations & Signage Plans).

3.7 HOURS OF OPERATION & STAFF

The development is proposed to be operated 24 hours a day, 7 days a week consistent with many service station and convenience developments across Western Australia.

The proposed Service Station will normally have up to 3 staff on site at peak times; 1-2 at non-peak times; and 1 overnight. During the overnight hours a single staff member will attend to all petrol and other internal sales.

3.8 FUEL TYPES, DISPENSING & FUEL DELIVERY

The proposed Service Station will provide standard fuels (ULP, ULP95, ULP98, Diesel) under the fuel canopy. The dispensing arrangements are through standard bowsers drawing from underground storage tanks.

All fuel storage and delivery activities will be undertaken in a manner which complies with Australian Standard 1940 – *The Storage and Handling of Combustible Liquids*.

The largest service vehicle which is expected to use the site is a 17.25m fuel tanker for fuel deliveries. Fuel delivery will occur at a rate of two to three deliveries per week on average, as required.

Transcore has undertaken a fuel tanker turn path assessment and the TIS includes the turn paths for a 17.25m fuel tanker (included at Appendix B of the TIS at Annexure 5).

The fuel will be stored in underground horizontal cylindrical tanks positioned in proximity to the fill point.

3.9 ENVIRONMENTAL CONSIDERATIONS

The risk of contamination and pollution of the local environment is considered minimal. Service Station uses are highly regulated land uses and designers use industry best practices to minimise any fuel or other contaminant access to stormwater drains. Design, operational and management measures will include:

- The connection of washrooms and toilets to sewer system;
- The use of double-contained fuel storage tank systems with a leak monitoring space;
- Fuel tanks will be established in stable compacted soils;
- On-site retention and treatment of all stormwater using a SPEL Purceptor Stormwater Treatment and Hydrocarbon Capture system;
- Stage 1 Vapour Recovery Systems compliant with the *Protection of the Environment Operation (Clean Air) Regulation 2002* (NSW Guidance Document);
- Fuel spill kit compliant with the latest Australian Standards and Federal National Occupational Health and Safety Council (NOHSC Codes) or Practice that pertains to the handling, storage, clean-up and disposal of Dangerous Goods and Hazardous Substances: NOHSC: 2007(1994), 1005(1994), & 1015(2001) and AS/NZS 3816:1998, AS1940-2004, AS3780-1994, & AS2507-1998;
- Fuel distribution and leak detection infrastructure compliant with all the relevant Australian Standards, Regulations and Industry Best Practices;
- Site operators will be trained personnel to effectively handle incidents such as fuel and oil spills; and
- Equipment will be installed on site to use in the clean up of any fuel, oil or chemical spills.

In support of this application, the proponent has commissioned a Dangerous Goods Technical Note (refer **Annexure 6**) to address all relevant issues associated with the operation of the service station. The report concludes that the proposed service station development will comply with all the applicable dangerous goods regulatory requirements.

3.10 NOISE MANAGEMENT

Reverberate Consulting was engaged to prepare an Acoustic Assessment in support of the proposed development (refer **Annexure 7**).

Of note, there is will be a 2.50m high masonry barrier wall located along part of the Blue Wren Drive, east of the proposed crossover to Blue Wren Drive as shown on the plans. The proposed masonry barrier wall assist in providing acoustic compliance for the operation of the proposed Service Station. In addition, there are lower height Colorbond fences proposed along the truncation boundary at the corner of Blue Wren Drive and Albatross Crescent (at 1.80m height), and part way along Albatross Crescent (at 1.20m height).

[Appendix ORD: 12.2.5B]

From the analysis undertaken by Reverberate Consulting, noise emissions from the proposed development (with management measures set out in a noise management plan) has been assessed to comply with the requirements of the *Environmental Protection (Noise) Regulations 1997* and no other major noise mitigation is required.

4.0 PLANNING ASSESSMENT

4.1 GREATER BUNBURY REGION SCHEME

The subject land is zoned “Urban” under the Greater Bunbury Region Scheme (**GBRS**).

4.2 SHIRE OF DARDANUP LOCAL PLANNING SCHEME NO. 3

4.2.1 Zoning & Land Use

The subject site is zoned “Mixed Business” in the Shire of Dardanup Local Planning Scheme No. 3 (**LPS3**) - refer LPS 3 Scheme Map Extract below at **Figure 2**.

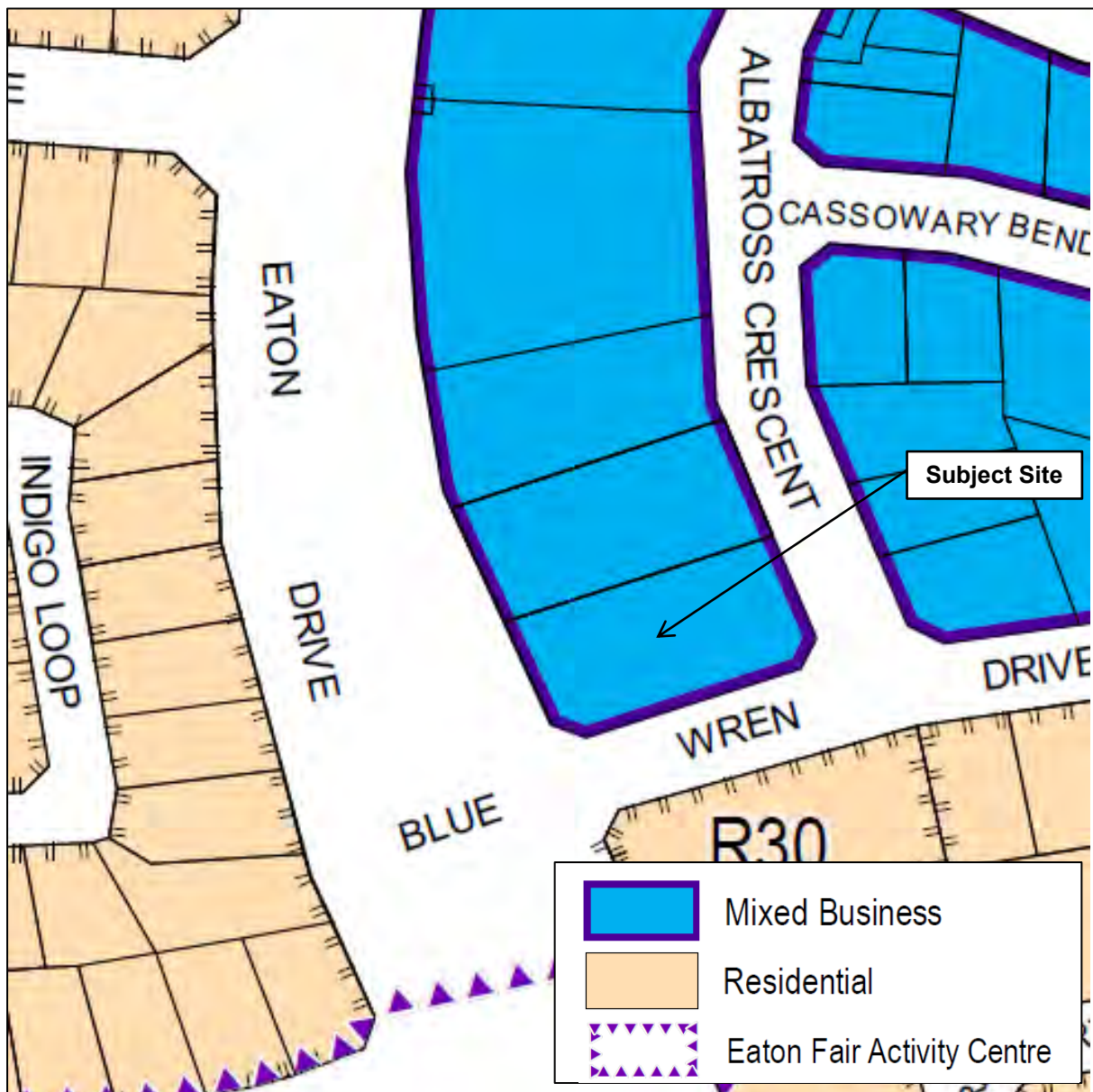


Figure 2: LPS 3 Scheme Map Extract

The subject land is also within the Eaton Fair Activity Centre area, as notated on the Scheme Map.

The objectives of the “Mixed Business” zone are set out in Table 2 of LPS8 and are as follows:

- *To provide for a variety of commercial activities such as warehouses, bulky goods showrooms, service industries, offices, wholesaling, motor repair facilities and extensive retail which by virtue of their scale and character are generally not appropriate to, or cannot conveniently or economically, be accommodated within the Business-Commercial or Industry zones.*
- *To ensure the design and landscaping of development provides a high standard of safety and amenity and contributes towards a sense of place and community within the service area.*

The proposed “Service Station” accords with the objectives of the Mixed Business zone as it is a use which is considered to be complementary to the commercial activities described in the objective, and will result in a quality development with a high level of landscaping.

A “Service Station” use is an “P” (Permitted) use in the “Mixed Business” zone, as set out in Appendix 1 – Zoning Table of LPS3, and therefore it is considered that the proposed use is something that is appropriate in this area.

Accordingly, the use class of “Service Station” is appropriate for the development/use proposed in this application.

4.2.2 Development Standards

Clause 3.1.1 of LPS3 describes that Appendix II – Development Table Part B – Non-Residential Land Uses provides development standards that apply to various land uses specified in that table.

The Development Table confirms that for development in the Mixed Business zone, front, rear and side setbacks are ‘as determined by Council’ meaning that there is no minimum setbacks applicable to the development, and that the Shire can assess the proposed development on its merits.

Accordingly, the 300mm setback to Eaton Drive, the 2.185m setback to Blue Wren Drive, the 26.762m setback from Albatross Crescent and the 4.90m side setback can all be considered by the Shire and are capable of approval.

The Development Table confirms that a minimum landscaping requirement of 3.00m wide landscape strips along all street boundaries, or as otherwise determined by Council, and one tree for every 4 open air parking bays.

A large landscaping area is proposed along Blue Wren Drive and around the corner of Albatross Crescent along with a 2.185m wide landscaping area located at the corner of Blue Wren Drive and Eaton Drive adjacent to the corner of the convenience store building. The total landscaping of the site totals 292.6m² or 13.85% of the site. Accordingly, there is not a uniform width landscaping area along all street boundaries.

However, the Shire is able to consider it, as the landscaping provision is confirmed as being able to be “otherwise determined by Council”. It is considered that the proposed landscaping is acceptable, and will also comply with Clause 4.2 of LPS3 which deals with general landscaping requirements.

As there are 13 open air car parking bays in the development, there is a requirement to provide 3.25 trees. The landscaping area has provision for the planting of at least 5 trees, and therefore, the tree provision has been met.

Clause 4.2.5 of LPS3 also requires the provision of 1 tree for every 10m² of landscaping area. As there is 292.6m² of landscaping area, there is a requirement under this provision to provide 29 trees, however it is noted that the Shire can relax these requirements. It would appear that planting 29 trees on the development site is excessive, so the Shire's discretion is sought in respect of this matter.

4.2.3 Car Parking

Appendix IIA – Car Parking of LPS3 sets out that there is a car parking requirement for the Service Station land use of 4 spaces for every service bay. As there are no workshop or service bays proposed in this development, there is no parking requirement under the Service Station land use.

However, it is considered reasonable to also apply a Convenience Store land use parking requirement of 1 space for every 15m² net lettable area to ensure that there is sufficient car parking for customers to the convenience store component of the development.

As the convenience store building has 214m² NLA, there is a requirement to provide 14.26 parking bays.

The development proposes to provide a total of 13 marked car parking bays (inclusive of the air/water bay).

The six (6) light vehicle bowser pump bays should be considered by the Shire as being able to be added to the car parking calculation, since in practice, those bays are highly likely to be used to park cars while customers pay for fuel and access the convenience store building. This is a standard being applied across many local governments in the consideration of Convenience Store/Service Station developments.

In these circumstances, it is considered that the proposed car parking arrangements for the site complies with the Shire's requirements.

4.2.4 Eaton Fair Activity Centre Plan

Clause 3.19 of LPS3 states that the local government shall have due regard to any activity centre plan(s) endorsed by the WAPC for applications for development approval in the area identified on the Scheme Map as 'Eaton Fair Activity Centre'.

The subject land is within the Eaton Fair Activity Centre Plan (EFACP) – refer **Figure 3** below.

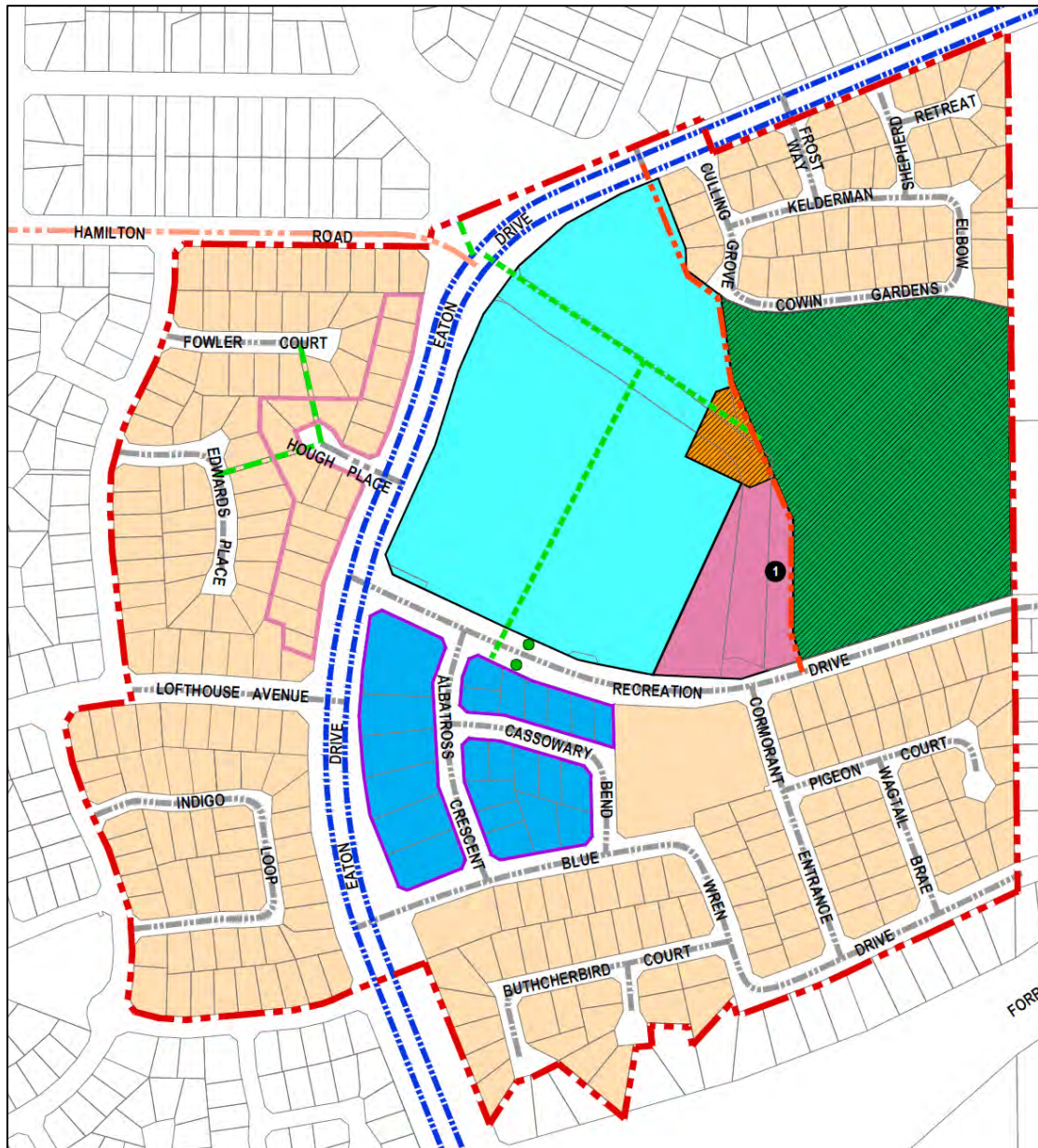


Figure 3: Eaton Fair Activity Centre Area

The subject land is within the Mixed Business Precinct of the EFACP. Clause 3.6 of the EFACP details that development of the Eaton Commercial Centre is to be in accordance with the Mixed

Business zone of LPS3 and that the EFACP does not impose any additional development requirements for the Mixed Business Precinct.

Clause 3.6.2 of the EFACP describes that bulky goods showrooms, health services, offices, eating and drinking establishments and light and service industries are preferred land uses in the Mixed Business Precinct. It is considered that the proposed Service Station, being a “P” Permitted use under LPS3 is an appropriate use in the precinct and is a use that is in keeping with these listed Preferred Uses in the EFACP.

As the provisions of the EFACP are not ultimately binding on a decision-maker, it is considered that the assessment of land use is best considered under the provisions of LPS3.

4.2.5 Matters to be Considered

Clause 67 - Part 2 - Schedule 2 (**Deemed Provisions**) of the *Planning and Development (Local Planning Schemes) Regulations 2015* (Regulations) outlines matters to be given due regard by local government when assessing development applications.

Table 3 below provides an assessment against matters relevant to this proposal.

Table 3: Matters to be Considered

Relevant Matters to be Considered	Comment
(a) <i>The aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area;</i>	<p>The propose use and development is consistent with the aims and provisions of the Shire’s LPS3 for the following reasons:</p> <ul style="list-style-type: none"> • The proposal seeks approval for a land use which is a “P” (Permitted) use in the zone and thus capable of approval. • The proposed development is in general compliance with the provisions of LPS3. • The proposed development accords with the objectives of the Mixed Business zone applicable to the site. • The proposed development will improve the amenity of the site.
(b) <i>The requirements of orderly and proper planning including any proposed local planning scheme or amendment to this Scheme that has been advertised under the Planning and Development (Local Planning Schemes) Regulations 2015 or any other proposed planning instrument that the local government is seriously considering adopting or approving;</i>	<p>This report demonstrates the proposed development is in general compliance with the local planning framework applicable to the subject site.</p> <p>The draft local planning scheme has not been advertised, so it is not a relevant matter, at this time, to be considered.</p>
(c) <i>Any approved State Planning Policy</i>	This report demonstrates the proposed development is in compliance with the relevant State Planning Policies.
(d) <i>Any environmental protection policy</i>	Section 4.6 of this report provides an

[Appendix ORD: 12.2.5B]

Relevant Matters to be Considered	Comment
<i>approved under the Environmental Protection Act 1986 section 31 (d);</i>	assessment against the EPA's <i>Guidance for the Assessment of Environmental Factors – Separation Distances between Industrial and Sensitive Land Uses</i> . The assessment demonstrates all potential impacts are capable of being managed.
<i>(e) Any policy of the Commission</i>	This matter is not relevant to this proposal.
<i>(f) Any policy of the State</i>	This matter is not relevant to this proposal.
<i>(fa) Any local planning strategy for this Scheme endorsed by the Commission</i>	<p>The Shire of Dardanup Local Planning Strategy was endorsed by the WAPC in 2015. The Shire's LPS describes the subject land as being within a District Centre as part of the Eaton Town Centre.</p> <p>The relevant objective of the LPS is to “<i>Consolidate the Eaton Town Centre as the commercial and cultural centre of the Shire</i>”. The LPS discusses urban expansion of Eaton and to cater for residential and commercial growth of the locality. The LPS states that, “<i>As a result of the population increase in Eaton and Millbridge, there will need to be consideration given to the expansion of the existing commercial area and community facilities.</i>”</p> <p>Accordingly, it is considered that the proposed development aligns with the objectives of the Shire's LPS.</p>
<i>(g) Any local planning policy for the Scheme area;</i>	This report demonstrates the proposed development is in general compliance with the local planning policies applicable to the subject site.
<i>(h) Any structure plan or local development plan that relates to the development.</i>	The Eaton Fair Activity Centre Plan applies to the subject land. This report addresses the provisions of the EFACP.
<i>(i) Any report of the review of the local planning scheme that has been published under the Planning and Development (Local Planning Schemes) Regulations 2015.</i>	A report on the review of the Shire's local planning scheme is not publicly available and has not been published.
<i>(j) In the case of land reserved under this Scheme, the objectives for the reserve and the additional and permitted uses identified in this Scheme for the reserve.</i>	This matter is not relevant to this proposal as the land is not reserved under the Scheme.
<i>(k) The built heritage conservation of any place that is of cultural significance.</i>	This matter is not relevant to this proposal.
<i>(l) The effect of the proposal on the cultural heritage significance of the area in which the development is located.</i>	This matter is not relevant to this proposal.

[Appendix ORD: 12.2.5B]

Relevant Matters to be Considered	Comment
<p>(m) <i>The compatibility of the development with its setting, including -</i></p> <p>(i) <i>the compatibility of the development with the desired future character of its setting; and</i></p> <p>(ii) <i>the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development;</i></p>	<p>The proposed development is entirely compatible with its setting for the following reasons:</p> <ul style="list-style-type: none"> • The proposed development is suitably located within an established commercial area and consistent with other commercial uses (such as the Dapco Tyre & Auto Service facility occurring on the immediately opposite corner). • The proposal presents an attractive, high quality built form which enhances the appearance of the subject site and its impact on adjoining properties and the streetscape; • The amenity of the subject site and surrounds will be improved through the redevelopment of the subject site; <p>Having regard to the above, the nature of the proposed development is entirely compatible with its surroundings, and improves amenity of the locality.</p>
<p>(n) <i>The amenity of the locality including the following –</i></p> <p>(i) <i>Environmental impacts of the development;</i></p> <p>(ii) <i>The character of the locality;</i></p> <p>(iii) <i>Social impact of the development;</i></p>	<p><u>Environmental Impacts:</u></p> <p>The proposed development is not anticipated to result in any adverse environmental impacts.</p> <p><u>Character of the Locality:</u></p> <p>The proposed development will not alter the established character of the locality which is described as being a mixed business, commercial locality that accommodates a range of businesses and uses.</p> <p><u>Social Impacts:</u></p> <p>The proposed development will not have any adverse social impacts on the surrounding locality, but will provide a positive social impact through the creation of jobs through construction and operation of the facility.</p>
<p>(o) <i>The likely effect of the development on the natural environment or water resources and any means that are proposed to protect or to mitigate impacts on the natural environment or the water resource.</i></p>	<p>This matter is not relevant to this proposal.</p>
<p>(p) <i>whether adequate provision has been made for the landscaping of the land to which the application relates and whether any trees or other vegetation on the land should be preserved.</i></p>	<p>A large area of the land has been proposed for landscaping (13.85% of the site area), and a landscape plan has been prepared demonstrating the planting of trees and plants around the site.</p>

[Appendix ORD: 12.2.5B]

Relevant Matters to be Considered	Comment
(q) <i>the suitability of the land for the development taking into account the possible risk of flooding, tidal inundation, subsidence landslip, bush fire, soil erosion, land degradation or any other risk.</i>	This matter is not relevant to this proposal.
(r) <i>The suitability of the land for the development taking into account the possible risk to human health or safety.</i>	This matter is not relevant to this proposal.
(s) <i>The adequacy of –</i> <i>(i) the proposed means of access to and egress from the site; and</i> <i>(ii) arrangements for the loading, unloading, manoeuvring and parking of vehicles;</i>	As outlined in this Report and the supporting Transport Impact Statement (TIS) prepared by Transcore (Annexure 5), the proposed access arrangements to and from the site are satisfactory, occurring from a new 10m wide crossover to Blue Wren Drive and an existing crossover to Albatross Crescent (which only requires minor modifications). An existing secondary crossover to Albatross Crescent will be removed. The development has been appropriately designed, including appropriate arrangements for loading, unloading, manoeuvring and parking of vehicles.
(t) <i>The amount of traffic likely to be generated by the development, particularly in relation to the capacity of the road system in the locality and the probable effect on traffic flow and safety;</i>	The Transport Impact Statement prepared by Transcore (refer Annexure 5) indicates that the proposed development will not have an unacceptable level of impact on the surrounding road network.
(u) <i>the availability and adequacy for the development of the following –</i> <i>(i) public transport services;</i> <i>(ii) public utility services;</i> <i>(iii) storage, management and collection of waste;</i> <i>(iv) access for pedestrians and cyclists (including end of trip storage, toilet and shower facilities);</i> <i>(v) access by older people and people with disability;</i>	The Transport Impact Statement prepared by Transcore (refer Annexure 5) addresses these matters.
(v) <i>The potential loss of any community service or benefit resulting from the development other than potential loss that may result from economic competition between new and existing businesses.</i>	No loss of any community service or benefit is expected to occur as a result of the proposed development.
(w) <i>The history of the site where the development is to be located.</i>	No significant historical matter is relevant to this proposal.
(x) <i>The impact of the development on the community as a whole notwithstanding the impact of the development on</i>	The proposed development will have a positive benefit on the community as a whole, as it provides for the retail sale of fuel and convenience goods to the immediate locality,

Relevant Matters to be Considered	Comment
<i>particular individuals.</i>	including those travelling along Eaton Drive.
<i>(y) Any submissions received on the application.</i>	The Shire is able to advertise the application if it considers that to be a necessary requirement, and to assess any relevant submission made.
<i>(za) The comments or submissions received from any authority consulted under clause 66.</i>	The Shire can refer the application to any authority it considers appropriate, and to assess any relevant submissions/comments made.
<i>(zb) Any other planning consideration the local government considers appropriate.</i>	The Shire can determine whether there are any other planning considerations that are relevant.

Having regard to **Table 3** above, the proposal appropriately addresses the relevant matters to be given due regard as set out in the Deemed Provisions.

4.3 SHIRE OF DARDANUP DRAFT NEW LOCAL PLANNING SCHEME

PWA is aware that the Shire of Dardanup has prepared a draft new local planning scheme which was initiated by the Council in July 2021 and is (at the time of this report) currently with the EPA for consent to advertise. In speaking with the Shire's officers, the Shire does not expect to commence advertising of the draft local planning scheme for public comment until the end of January 2022.

Accordingly, at this time, the draft local planning scheme is not a seriously entertained planning proposal as the scheme has not yet been publicly advertised. Therefore, any resultant scheme changes, including change of zoning or land use permissibility should not affect the progress and determination of this application.

However, PWA has consulted with the Shire to ensure that any likely zoning/use change under the draft scheme will not materially affect the proposed development being sought for approval.

PWA has been advised that the zoning of the subject land will likely change to a "Service Commercial" zone. Under the "Service Commercial" zone, we are advised that a "Service Station" land use will be still be a use that is capable of approval, under a "D" (Discretionary) land use permissibility classification. Accordingly, it follows that there is little risk to this proposal if the application is delayed and the Shire's draft local planning scheme needs to be formally considered at the time of the DAP's determination of this application.

4.4 LOCAL PLANNING POLICIES

4.4.1 Local Planning Policy CP084 – Advertising Signage

The Shire's Local Planning Policy CP084 – 'Advertising Signage' (**Policy**) provides guidance on the development of signage or advertisements.

Table 1 of the Policy lists a range of signs that do not require approval as long as the Development Standards associated with those signs are met.

Table 2 of the Policy lists a range of signs that require development approval.

Table 2 of the Policy confirms that Monolith Signs shall be no more than 6.00m in height, have a maximum area of 15m² and restricted to one monolith sign per road frontage. The proposed development involves a 6.00m high monolith sign adjacent to Albatross Crescent displaying fuel pricing and the 7-Eleven logo, and is less than 15m² in area, and therefore complies with the Policy.

The proposed development also includes a range of '7-Eleven' Wall Signs on the building and fuel canopy, together with directional signage, and it is considered that these signs generally comply with the Policy.

The proposed development also includes the main Building Price Board sign which is affixed to the building and projects above the building line. This sign is not defined as a Monolith Sign as it is not ground-mounted. Accordingly, there are no specific signage standards for this type of proposed price board sign. The policy provides for "Any Other Sign" which is not listed or defined within the general terms of the definitions or otherwise mentioned in the Policy, and Table 2 describes that these signs, "are be considered against the provisions of a sign definition of 'nearest fit' and the performance criteria". Accordingly, the Shire has the discretion to consider this type of sign against the Performance Criteria listed under Part 7.5 of the Policy.

4.5 STATE PLANNING POLICIES

4.5.1 WAPC SPP 7.0 – Design of the Built Environment

Table 5 below provides a response to the Design Principles of State Planning Policy 7.0 Design of the Built Environment (**SPP7.0**).

Table 5: Assessment of Design Principles of SPP7.0

SPP 7.0 Design Principles	Comment
<p>1. Context and character <i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i></p>	<p>The proposed development/use is consistent with the existing commercial context of the site, and no specific or defined character exists in this area.</p> <p>The development of the site will improve the amenity of the locality, by incorporating well-designed built form (together with landscaped setting).</p>
<p>2. Landscape quality <i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i></p>	<p>The proposed development provides 13.85% of the site area as landscaping which is considered to be an excellent outcome for a service station development.</p> <p>The landscaping areas provides opportunity for planting of trees and shrubs which is considered to be a positive aspect of the proposed development, as there is currently very little in the way of landscaping on the site.</p>

SPP 7.0 Design Principles	Comment
	<p>It is considered that the proposed landscaping regime provides both a good quantity and high quality.</p> <p>Landscape treatment to the streets is more visible and thus an important aspect to consider – and in response, the planting and landscaping areas have been focussed in these areas, so that the development presents with a foreground of landscaped areas. This is particularly so for the verge area of Eaton Drive which is proposed to be landscaped.</p> <p>The width of landscaped bed sizes are good, enabling consolidated plantings to occur as well as the establishment of trees.</p> <p>A Landscaping Plan has been prepared in support of the application– refer Annexure 4.</p>
<p>3. Built form and scale <i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i></p>	<p>The built form is a function of the use (service station).</p> <p>The built form, scale and height of the proposed development is appropriate to this setting and appropriate for the development type. Adjacent residential uses are single storey in height, and the adjacent and nearby commercial uses presents as more of a two-storey development form. The proposed development has a mix of single level and elevated structures including 8m high feature blade wall and 7m high pylon sign structure attached to the building (total 10m height).</p>
<p>4. Functionality and build quality <i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life-cycle.</i></p>	<p>From a functionality perspective, the proposed development is fit for purpose, and the proposed building materials also align with the function of a service station</p> <p>The design of the development is aimed at providing user efficiency and convenience.</p>
<p>5. Sustainability <i>Good design optimises the sustainability of the built environment, delivering positive environmental, social and economic outcomes.</i></p>	<p>The proposed building and fuel canopy is designed to support PV (Solar) Panels, and there is a possibility to include future EV charging stations as the need arises.</p> <p>The operator, 7-Eleven, uses recyclable bins and have a coffee cup recycle program.</p>
<p>6. Amenity <i>Good design provides successful places that offer a variety of uses and activities while optimising internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable, productive and healthy.</i></p>	<p>The development improves the amenity of the locality and provides an environment that is universally accessible. Good quality landscaping areas (including planting of trees) is also proposed to provide a healthy environment and improved amenity.</p> <p>The convenience function of the service station itself is considered to provide a new amenity to the locality.</p>
<p>7. Legibility <i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help</i></p>	<p>A footpath is proposed along the frontage of the convenience store and will connect with a future footpath in the verge of Blue Wren Drive. This will provide a clear connection and identifiable link to assist pedestrians to find their way around, when road upgrades occur in future.</p>

SPP 7.0 Design Principles	Comment
<i>people find their way around.</i>	A new footpath is also proposed to be constructed in the verge of Albatross Crescent as shown on the Site Plan (to replace a removed crossover) to enhance pedestrian connectivity.
<p>8. Safety <i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i></p>	The building design maximises opportunities for casual surveillance over the forecourt area through large windows and the main entrance to the building. There is also passive surveillance towards Eaton Drive as the building has windows facing towards Eaton Drive and the intersection with Blue Wren Drive.
<p>9. Community <i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i></p>	<p>The development is specifically designed for convenience of the local community, enabling a venue which provides goods 24/7.</p> <p>It is a well-placed addition which will add to the diversity and choice of convenience stores / service stations in the locality, and puts less pressure on other facilities in the area.</p>
<p>10. Aesthetics <i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i></p>	<p>The proposed building includes a blade wall element and interesting screen feature at the corner of the building, providing articulation.</p> <p>The quality landscaping design of the site also results in an attractive setting for the development.</p>

As a result, it is considered that the proposed development complies with the Design Principles of SPP7.0.

4.6 ENVIRONMENTAL PROTECTION AUTHORITY GUIDANCE STATEMENT NO. 3 – SEPARATION DISTANCES BETWEEN INDUSTRIAL & SENSITIVE LAND USES

The Environmental Protection Authority (EPA) Guidance Statement No. 3 – Separation Distances between Industrial and Sensitive Land Uses (**EPA Guidance Statement**) provides generic buffer distances intended to mitigate impacts of industrial developments on sensitive land uses.

With respect to fuel facilities that are intended for 24-hour operation, the EPA Guidance Statement identifies potential impacts as gaseous, noise, odour and risk, and recommends a generic buffer distance of 200m. These recommended buffer distances are not absolute separation distances, but are instead default provisions in the absence of site specific technical assessment.

The closest fuel bowser to a residential dwelling is approximately 36 metres (to the dwellings to the south of the subject land located along Blue Wren Drive). However there is a 2.50m high barrier wall proposed along the Blue Wren Drive boundary, eliminating the 'line of sight' between the fuel bowsers and the residential receivers.

[Appendix ORD: 12.2.5B]

Table 6 below provides an assessment of the potential impacts from the operation of the proposed Service Station, and provides justification to demonstrate the appropriateness of the development on the subject site.

Table 6: Mitigation of Potential Amenity or Environmental Impacts

Issue	Comment
Noise	<p>It is common for Service Stations to be located either opposite or directly adjoining residential properties.</p> <p>Transmission of noise in these circumstances is not an issue, and noise generated from a fuel facility is generally consistent with other similar commercial uses.</p> <p>Of particular note, a 2.50m high masonry wall is being proposed along the Blue Wren Drive boundary as previously described, separating the proposed development from the existing residential development to the south.</p> <p>A comprehensive Acoustic Assessment has been prepared to address noise issues, and it is has been concluded that the site will comply with the Environmental Protection (Noise) Regulations (refer Annexure 7).</p>
Risk	<p>As the proposed Service Station provides for the retail sale of fuel, the operator must obtain a Dangerous Goods Storage and Handling Licence to store and sell fuel on the site. The Licence application is made post planning approval.</p> <p>For this application, the operator has commissioned a Dangerous Goods Technical Note (refer Annexure 6) to address the issue of Risk. The report concludes that the proposed service station development will comply with all the applicable dangerous goods regulatory requirements.</p>
Odour / Gaseous	<p>The underground fuel storage tanks will be equipped with a vapour recovery system which ensures that all petrol vapours from the underground tanks are drawn back into the fuel tanker at the time of fuel delivery. This enables the returned vapours to be recondensed into liquid.</p> <p>The Dangerous Goods Licencing process also assesses the likely impact of vapours/odours. Accordingly, the assessment of odour/gaseous issues is appropriately addressed and managed through the Dangerous Goods Licencing process.</p> <p>For this application, the Dangerous Goods Technical Note at Annexure 7 also addresses the issues associated with odours/gases.</p>
Lighting	<p>Potential sources of light spill from the proposed development are primarily from the lighting of the fuel canopy, the retail building, and any external lights in the forecourt or perimeter of the development site.</p> <p>Lighting proposed within the fuel canopy are inset and orientated internally to ensure light spill is contained within the site. The 2.50m high masonry wall proposed along the Blue Wren Drive property boundary will also assist in shielding some light spill (including light associated with car headlights) to residential development to the south.</p> <p>Further, it is a requirement to comply generally with Australian Standard AS4282 – Control of Obtrusive Effects of Outdoor Lighting, and the final design of lighting (and ultimate operation) will be regulated by AS4282. A condition of approval can also require the development to comply with AS4282, and this kind of condition has been included on many other service station approvals that PWA has secured for other proponents.</p>

[Appendix ORD: 12.2.5B]

As demonstrated in the **Table 6** above, the proposed Service Station development has been appropriately designed, sited and assessed (by qualified consultants) to mitigate any potential amenity and environmental impacts on nearby sensitive (residential) land uses. Accordingly, a lesser separation distance has been demonstrated to be acceptable.

5.0 CONCLUSION

This application seeks approval for a proposed new 7-Eleven “Service Station” at Lot 303 (#15) Albatross Crescent, Eaton.

In summary, the proposed development warrants approval for the following reasons:

- The proposed “Service Station” is generally compliant with the development standards and requirements of the Shire of Dardanup LPS3;
- The proposed “Service Station” use is a “P” use which means that the use is Permitted by LPS3;
- The proposed “Service Station” use is a use which is capable of approval in the Shire’s draft new local planning scheme and accordingly, presents no risk from a land use perspective;
- The proposed development provides for the redevelopment of the site, which increases amenity;
- The proposal is supported by a comprehensive Traffic Impact Statement, demonstrating acceptable traffic movements;
- The proposal is supported by a Dangerous Goods Technical Note, demonstrating that the proposal is operationally acceptable under relevant legislation; and
- The proposal is supported by a comprehensive Acoustic Assessment, demonstrating that the operation of the proposed development conforms to the required standards under the Environmental Protection Noise Regulations (subject to a noise management plan).

Having regard to the above, the proposed “Service Station” development should be supported.

For these reasons, and in light of the assessment contained within this report, we respectfully request that the Shire of Dardanup have regard to the merits and broader benefits of the proposal when undertaking its assessment of the application, and to recommend approval to the Regional JDAP, subject to reasonable conditions.

Peter Webb & Associates

ANNEXURES

ANNEXURE 1

Certificate of Title

WESTERN



AUSTRALIA

REGISTER NUMBER 303/DP38275	
DUPLICATE EDITION 2	DATE DUPLICATE ISSUED 24/8/2020

RECORD OF CERTIFICATE OF TITLE
UNDER THE TRANSFER OF LAND ACT 1893

VOLUME 2597 FOLIO 585

The person described in the first schedule is the registered proprietor of an estate in fee simple in the land described below subject to the reservations, conditions and depth limit contained in the original grant (if a grant issued) and to the limitations, interests, encumbrances and notifications shown in the second schedule.

BGRoberts
REGISTRAR OF TITLES



LAND DESCRIPTION:

LOT 303 ON DEPOSITED PLAN 38275

REGISTERED PROPRIETOR:
(FIRST SCHEDULE)

WEST COAST PROPERTY DEVELOPMENTS PTY LTD OF LOT 104 KEIRNAN STREET, WHITBY
(T J289276) REGISTERED 18/5/2005

LIMITATIONS, INTERESTS, ENCUMBRANCES AND NOTIFICATIONS:
(SECOND SCHEDULE)

1. J717820 LEASE TO PEPPERCORN HOLDINGS NO. 5 PTY LTD OF LEVEL 5, 232 ADELAIDE STREET, BRISBANE, QUEENSLAND EXPIRES: SEE LEASE. REGISTERED 27/4/2006.

Warning: A current search of the sketch of the land should be obtained where detail of position, dimensions or area of the lot is required.
* Any entries preceded by an asterisk may not appear on the current edition of the duplicate certificate of title.
Lot as described in the land description may be a lot or location.

-----END OF CERTIFICATE OF TITLE-----

STATEMENTS:

The statements set out below are not intended to be nor should they be relied on as substitutes for inspection of the land and the relevant documents or for local government, legal, surveying or other professional advice.

SKETCH OF LAND: DP38275
PREVIOUS TITLE: 2535-584, 2535-585
PROPERTY STREET ADDRESS: 15 ALBATROSS CR, EATON.
LOCAL GOVERNMENT AUTHORITY: SHIRE OF DARDANUP

ANNEXURE 2
Development Plans
(Project Consultants)

GENERAL

- A001 DRAWING LIST
- A002 SCHEDULE FINISHES

ARCHITECTURAL

- A102 PLAN SITE PROPOSED
- A103 PLAN BUILDING
- A104 PLAN CANOPY CAR
- A201 ELEVATION BUILDING SHEET 1
- A202 ELEVATION BUILDING SHEET 2
- A203 ELEVATION CANOPY CAR SHEET 1
- A204 ELEVATION CANOPY CAR SHEET 2
- A205 ELEVATION BUILDING SHEET 3

SIGNAGE

- S101 PLAN SITE SIGNAGE
- S201 ELEVATION SIGNAGE

DEVELOPMENT
NOT FOR CONSTRUCTION



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PROJECT

PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET

DRAWING LIST

REV. DESCRIPTION

REV.	DESCRIPTION
A	INITIAL DA PLAN
B	REVISED TO 7 ELEVEN COMMENTS
C	TOWER SIGN ADDED, OTHER AMENDMENTS
D	NOISE WALLS ADDED

16/11/21
18/11/21
23/11/21
06/12/21

A001
NTS | D

PAINT FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
P1	DULUX WASH & WEAR 101 LOW SHEEN ACRYLIC COLOUR: VIVID WHITE PW1 H9 (IF SPECIFIED TO BE PAINTED)	INTERIOR CEILING
P2	DULUX WASH & WEAR 101 SEMI GLOSS ACRYLIC COLOUR: WHITE WATSONIA PW2D4	INTERIOR WALLS
P3	DULUX SUPER ENAMEL HIGH GLOSS COLOUR: COLORBOND SHALE GREY CB16	INTERIOR DOORS & ARCHITRAVES
P4	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: VIVID WHITE PW1H9	7-ELEVEN VENT PIPES, PETROL CANOPY COLUMNS & DOWNPIPES
P5	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: 7-ELEVEN GONDOLA GREY (DULUX REF A2225) TINT FORMULA: EE=11, M=86.	BUILDING - EXTERNALLY REFER A015 FOR LOCATION. EXTERIOR DOOR, DOWNPIPES GUTTER, SELECTED EXTERNAL CONCRETE PANEL WALLS.
P6	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: BLACK	PETROL CANOPY COLUMNS AND BOLLARDS 300mm ABOVE GROUND LEVEL
P7	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: 7-ELEVEN TOWER SILVER (DULUX REF A0290)	EXTERNAL - BUILDING TOWER (IF APPLICABLE)
P8	DULUX ROADMASTER A1 - NON SLIP COLOUR: LF YELLOW ADDITIVE: GLASS BEAD	CAR PARK LINES -FACE AND TOP OF KERB IMMEDIATELY IN FRONT OF STORE & AS NOTED ON SITE PLAN
P9	DULUX WASH & WEAR 101 BARRIER. LOW SHEEN-BLACK-BASE-EXTRA BRIGHT 52L87663	FOTG ZONE INTERIOR WALL & INTERNAL WALL TO SLURPEE ZONE
P10	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: GOLDEN BANNER P15.H9	BOLLARDS
P11	DULUX WASH & WEAR LOW SHEEN ACRYLIC COLOUR: HOT LIPS S05H9	FRONT AND UNDERSIDE OF FOTG BULKHEAD

CONCRETE FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
S1	AVISTA CONC. SEALER PRIMER AVISTA CONC. SEALER EXTENDED WEAR - CLEAR AVISTA CONC. SEALER SLIP REDUCING ADDITIVE	BUILDING APRON - RAISED CONC. FOOTPATH INCLUDING RAMPS AROUND BUILDING

TILE FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
T1	JOHNSON 150x150mm JOHNSON ULTRA WHITE GLOSS CERAMIC WALL TILES WITH WHITE GROUT PRODUCT CODE: 614580	BEHIND HAND BASINS, CLEANER'S SINK, DBL BOWL SINK, PREP AREA

METAL FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
C1	FOLDED COLORBOND METAL FLASHING COLOR: COLORBOND 'SURFMIST'	ROOF FLASHING/ PARPAPET CAPPING
C2	FOLDED COLORBOND METAL FLASHING / METAL SHEET WALL CLADDING COLOR: COLORBOND 'IRONSTONE'	ROOF FLASHING/ PARPAPET CAPPING/ BIN ROOM

LAMINATE FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
LA1	GLOSS WHITE ALASKA GLIS LAMINATE SUPPLIER: HALIFAX VOGEL GROUP CODE: 018876	SERVICE COUNTER JOINERY
LA2	BLACK COMPACT LAMINATE VELVET FINISH SUPPLIER: MAICA	SERVICE COUNTER JOINERY
LA3	PREMIUM BLACK 60 LAMINATE SUPPLIER: HALIFAX VOGEL GROUP	COFFEE ISLAND JOINERY

GLASS FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
SP	DULUX BRIGHT DELIGHT TOUGHENED GLASS SPLASHBACK CODE: S08G9	SERVICE COUNTER

STAINLESS STEEL FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
M01	1mm THICK STAINLESS STEEL	SERVICE COUNTER JOINERY

STONE FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
ST1	PRODUCT: CAESAR STONE 20mm THICK SUPPLIER: NATURAL STONE QUARTZ SURFACES COLOUR: NIGHT SKY 6100	COFFEE ISLAND

VINYL FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
V1	ARMSTRONG EXCELON 3.2 x 305 x 305mm VINYL FLOOR TILES COLOUR: IMPERIAL TEXTURE BLUE GREY 7A519033	SALES AREA
V2	POLYFLOR XL PUR 2mm VINYL SHEET COLOUR: FLINT 3720	SLURPEE ZONE
V3	ARMSTRONG - SAFEGUARD COLOUR: LIGHT GREY SLIP RESISTANCE: R10 / P3	COOLROOM / OFFICE / STORE ROOM
V4	ARMSTRONG VINYL SHEET COLOUR: BLACK	SALES AREA COVING
V5	POLYFLOR POLYCLAD PLUS PU. 2mm THK WALL VINYL SHEET. COLOUR: NIMBUS GREY 2710	BOH WALL CLADDING

GENERAL NOTE:

- ALL ALTERNATIVE PRODUCTS TO BE APPROVED BY 7-ELEVEN

DEVELOPMENT
NOT FOR CONSTRUCTION



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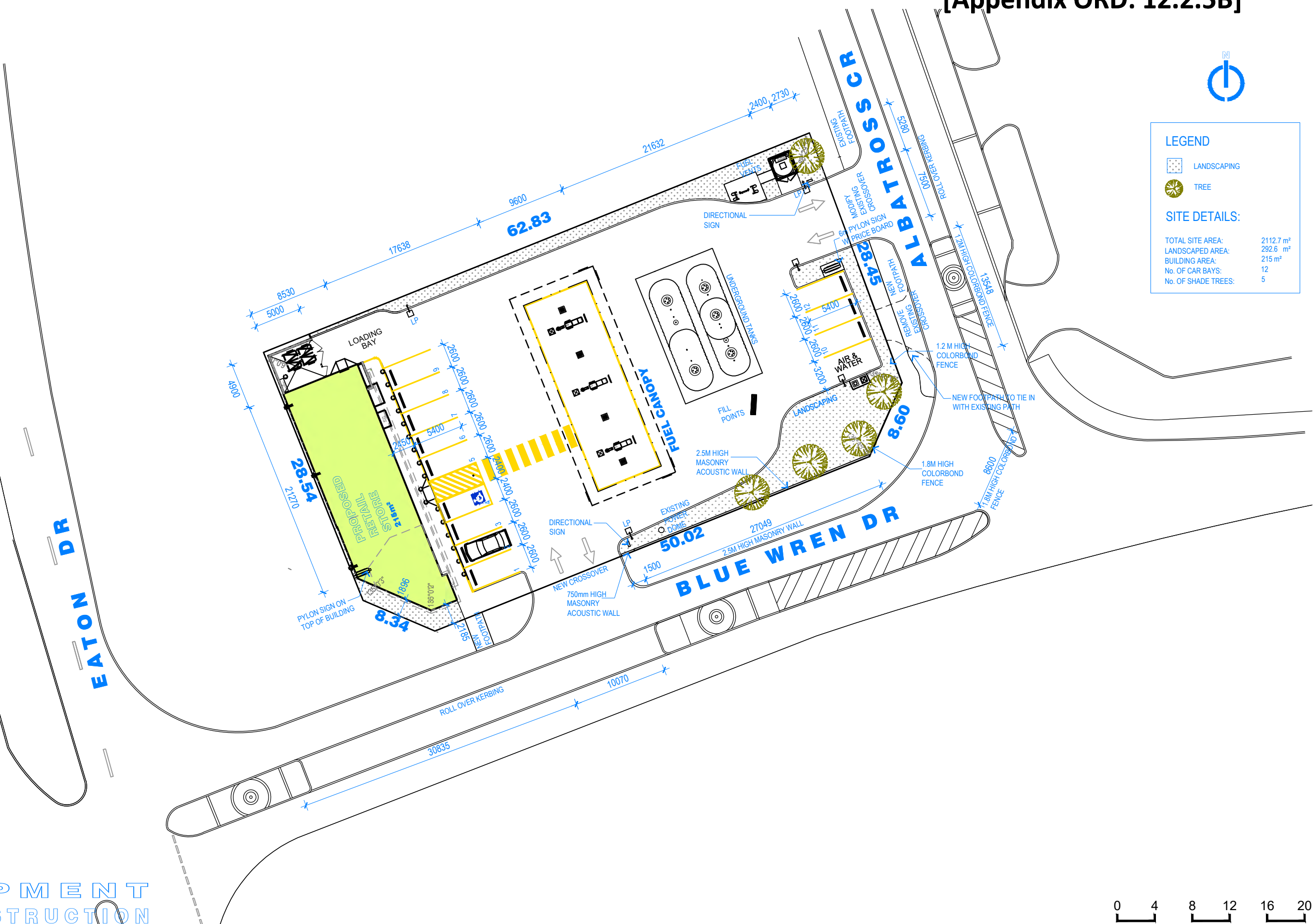
PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
SCHEDULE
FINISHES (SERVICE STATION)

REV. DESCRIPTION
A INITIAL DA PLAN
B REVISED TO 7 ELEVEN COMMENTS
C TOWER SIGN ADDED, OTHER AMENDMENTS

16/11/21
18/11/21
23/11/21

A002
NTS | C



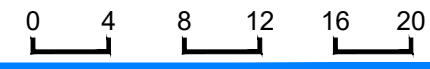
LEGEND

- LANDSCAPING
- TREE

SITE DETAILS:

TOTAL SITE AREA:	2112.7 m ²
LANDSCAPED AREA:	292.6 m ²
BUILDING AREA:	215 m ²
No. OF CAR BAYS:	12
No. OF SHADE TREES:	5

DEVELOPMENT
NOT FOR CONSTRUCTION



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DESIGN | DOCUMENT | DELIVER

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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
SITE PLAN
PROPOSED

REV.	DESCRIPTION	DATE
A	INITIAL DA PLAN	16/11/21
B	REVISED TO 7 ELEVEN COMMENTS	18/11/21
C	TOWER SIGN ADDED, OTHER AMENDMENTS	23/11/21
D	NOISE WALLS ADDED	06/12/21
E	SITE DETAILS ADDED	07/12/21
F	FOOTPATH ADDED TO ALBATROSS CRES	13/12/21
G	NOISE WALLS ADJUSTED FOR SIGHT LINES	16/12/21

A102

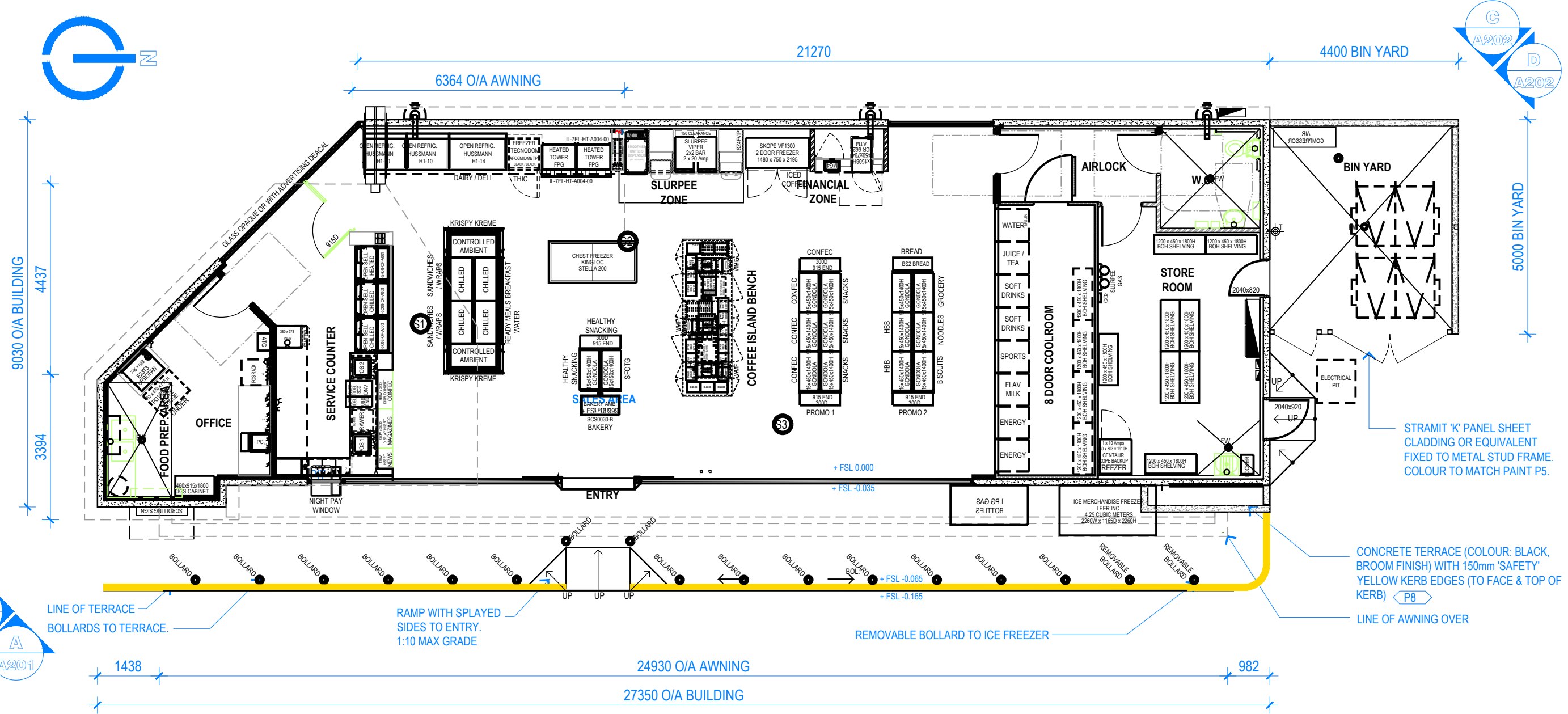
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G

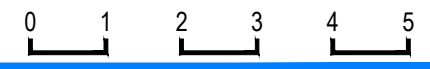
NOTE
ALL DIMENSIONS GIVEN ARE TO STRUCTURAL WALLS & FRAMING, UNLESS NOTED OTHERWISE

WALL TYPE LEGEND

- 150mm THICK PRECAST CONCRETE WALL PANELS). PLASTERBOARD LINING INTERNALLY WALL INSULATION TO MEET PART 'J' OF BCA REQUIREMENTS.
- 90mm STUD WALL WITH 13mm PLASTERBOARD LINING INTERNALLY AND 9mm CFC SHEET LINING EXTERNALLY, WALL CONSTRUCTION TO MEET PART 'J' OF BCA REQUIREMENTS, REFER NOTES ON PLAN.
- WALL CONSTRUCTION TO RELEVANT AUTHORITY'S REQUIREMENTS TO ACHIEVE 'CLASS 2' FOOD REGISTRATION COMPLIANCE.
- 75mm THICK INSULATED COOLROOM PANELS



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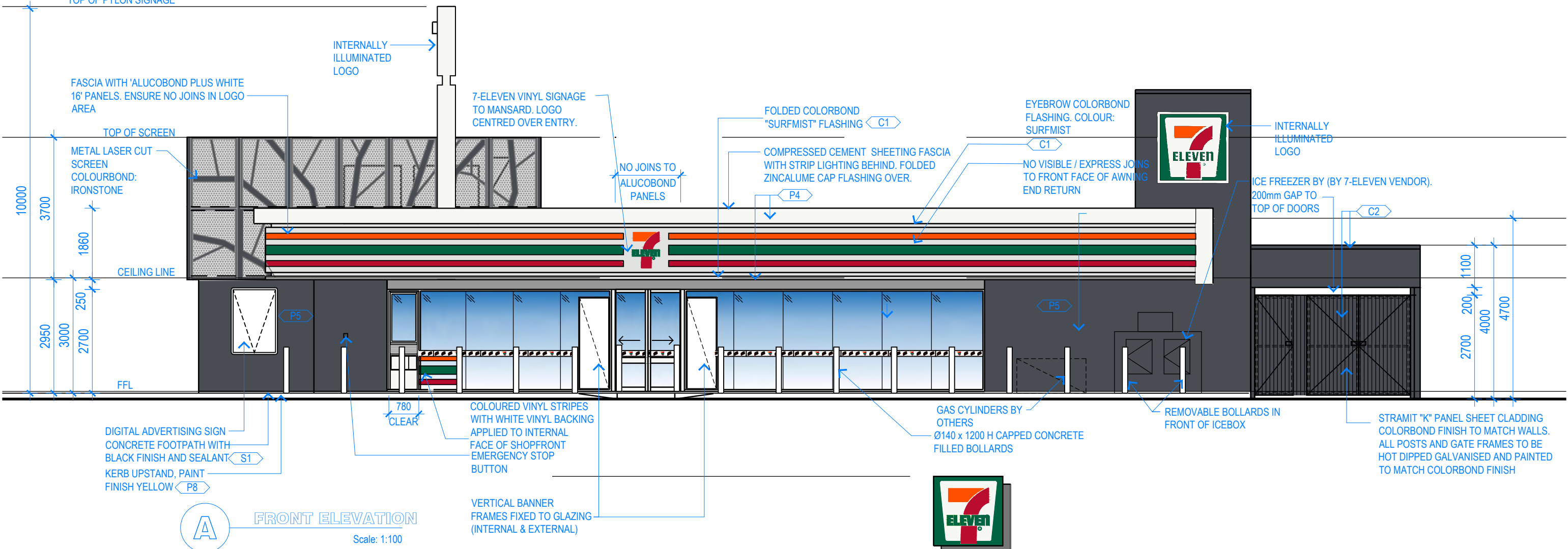
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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

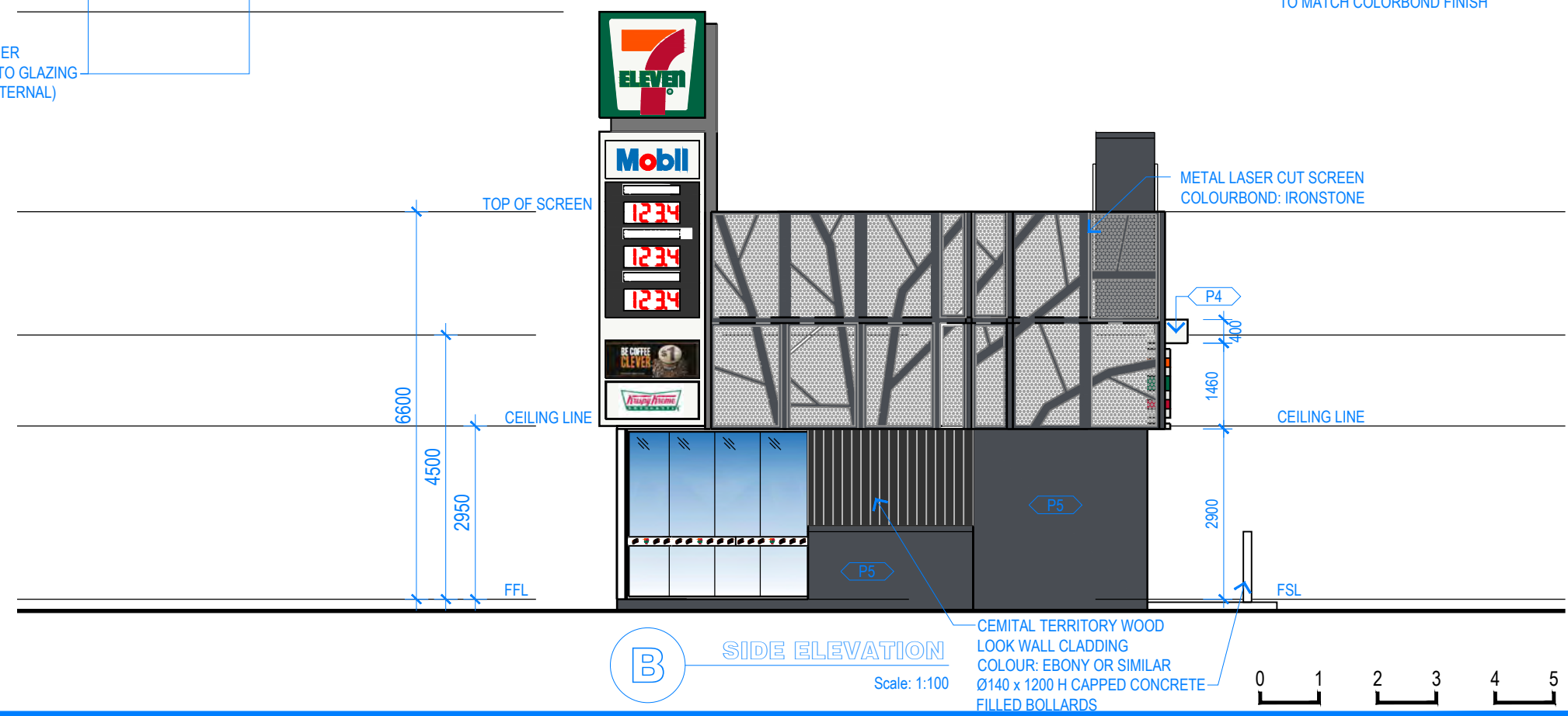
SHEET
PLAN
BUILDING

REV.	DESCRIPTION	DATE
A	INITIAL DA PLAN	16/11/21
B	REVISED TO 7 ELEVEN COMMENTS	18/11/21
C	TOWER SIGN ADDED, OTHER AMENDMENTS	23/11/21

A103
1:100 | C



A FRONT ELEVATION
Scale: 1:100



B SIDE ELEVATION
Scale: 1:100

- GENERIC NOTES**
1. ALL WORKS TO BE COMPLETED BY LESSOR UNLESS NOTED
 2. REFER TO DRAWING G002 FOR FINISHES SCHEDULE

DEVELOPMENT
NOT FOR CONSTRUCTION

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DESIGN | DOCUMENT | DELIVER

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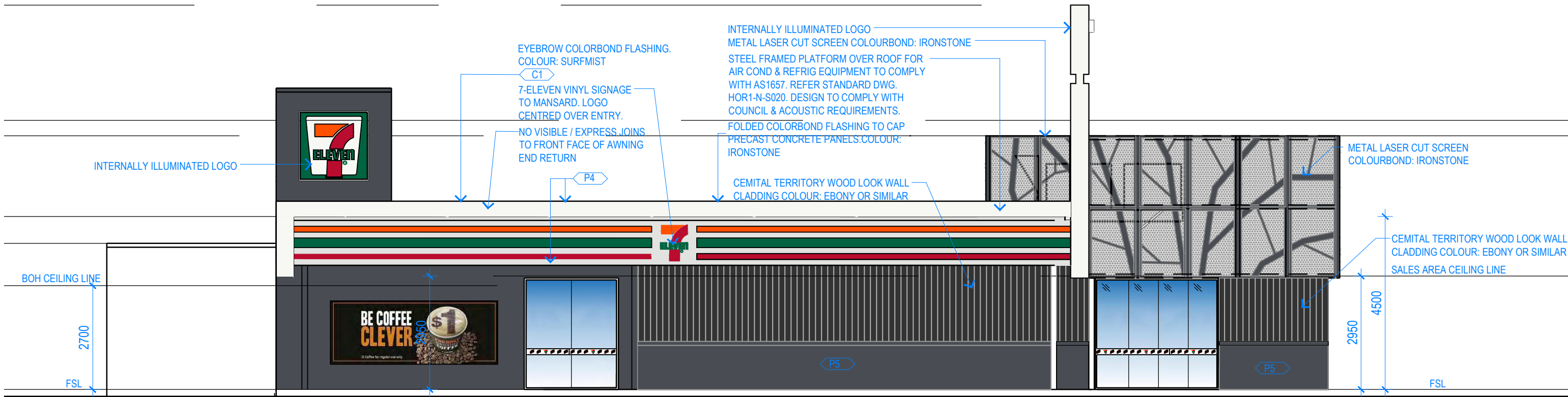
PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
ELEVATION
BUILDING
SHEET 1

REV.	DESCRIPTION
A	INITIAL DA PLAN
B	REVISED TO 7 ELEVEN COMMENTS
C	TOWER SIGN ADDED, OTHER AMENDMENTS

16/11/21
18/11/21
23/11/21

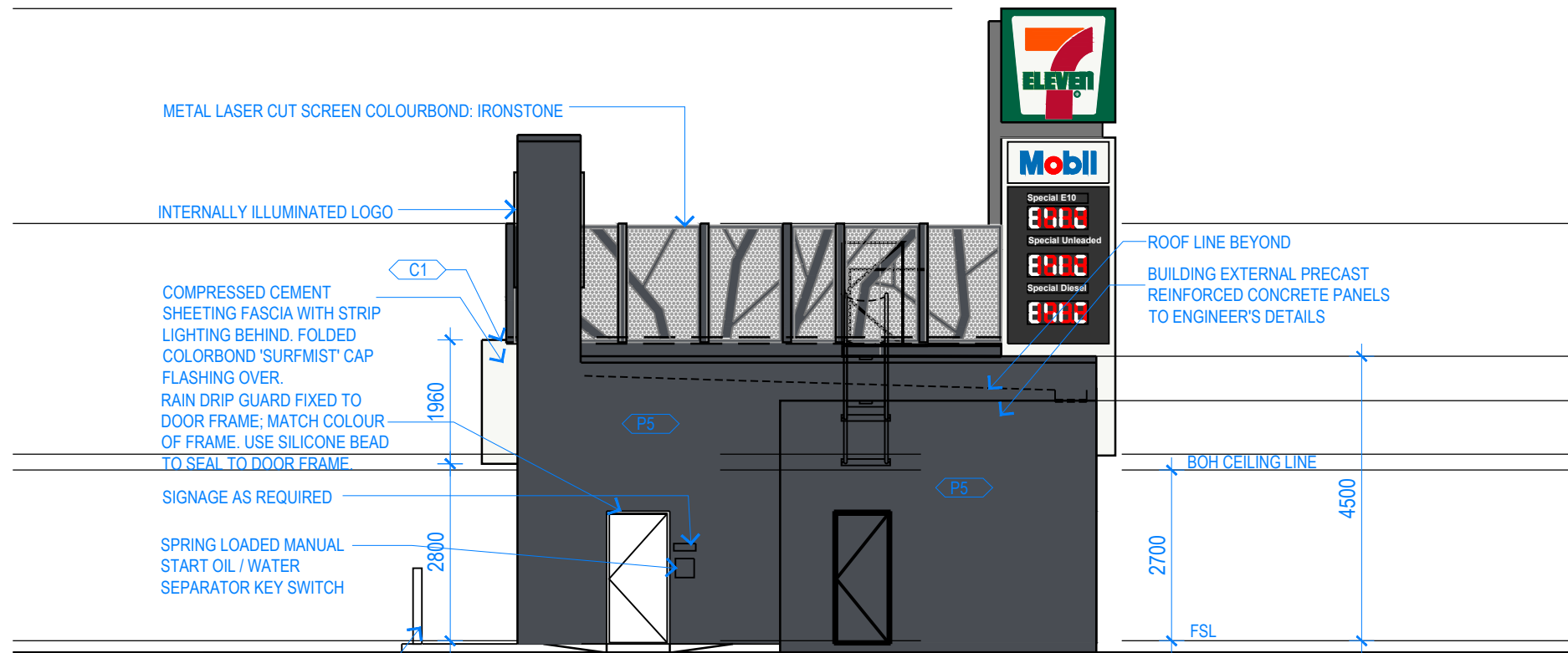
A201
1:100 | **C**



C REAR ELEVATION
Scale: 1:100

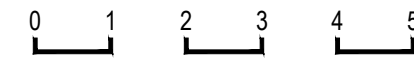
GENERAL NOTES

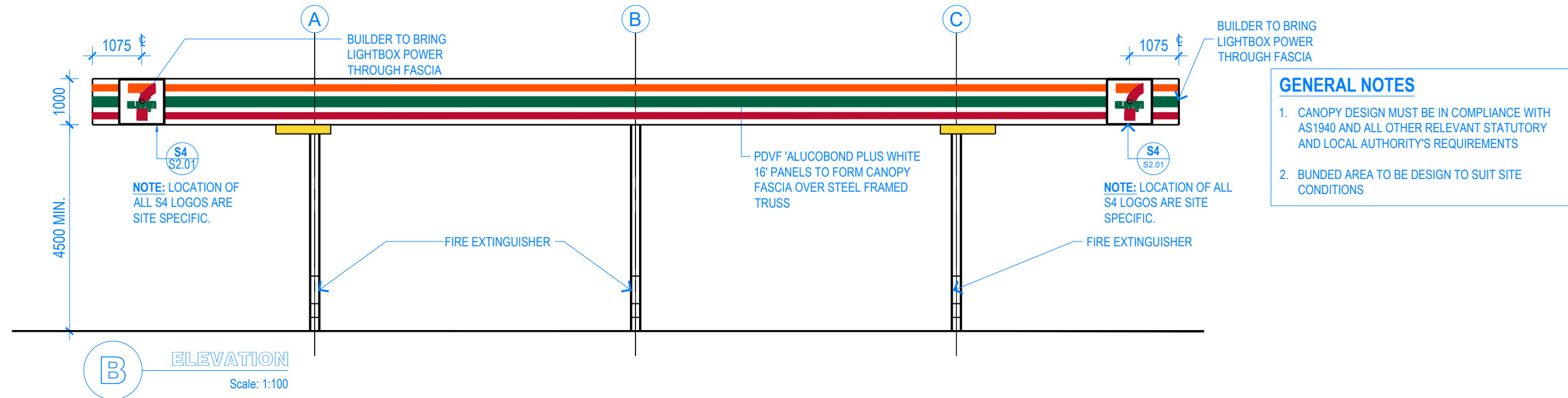
1. REFER TO DRAWING GEN17-D-A033 FOR SIGNAGE DETAILS
2. ALL 7-ELEVEN BRANDING TO BE SUPPLIED AND INSTALLED BY 7-ELEVEN
3. REFER STANDARD DRAWING GEN17-D-S010 & S030 FOR PANELS AND POSTS DETAILS



D SIDE ELEVATION
Scale: 1:100

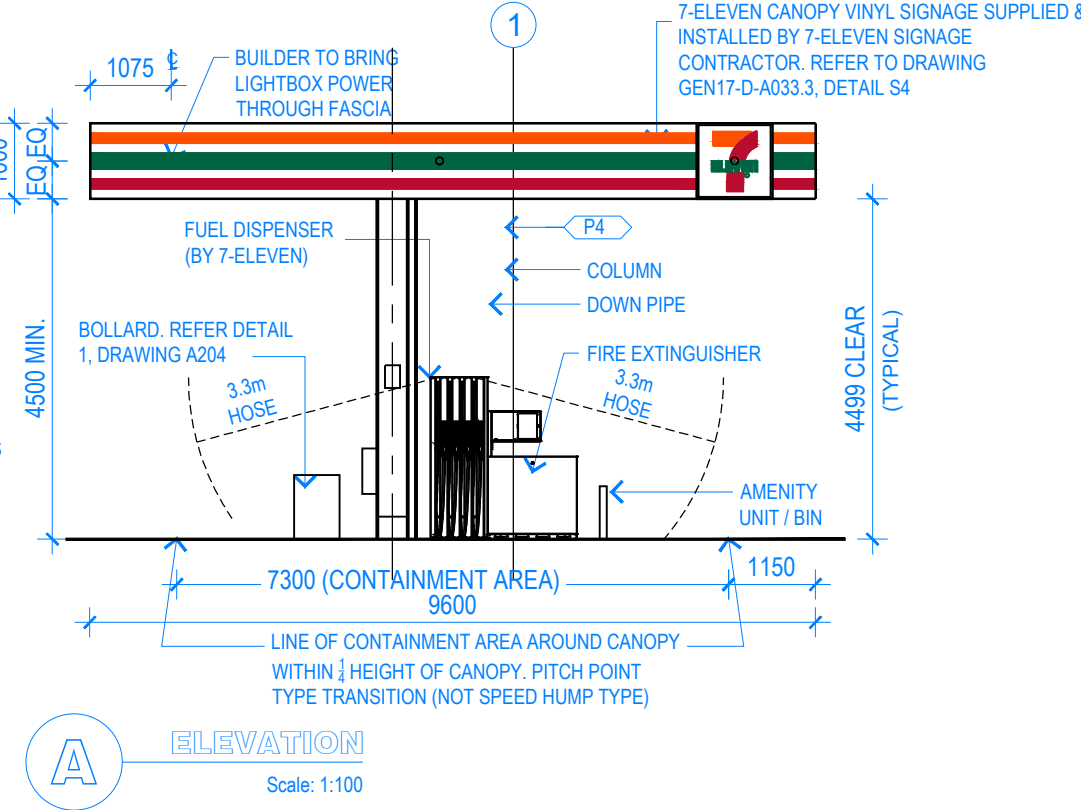
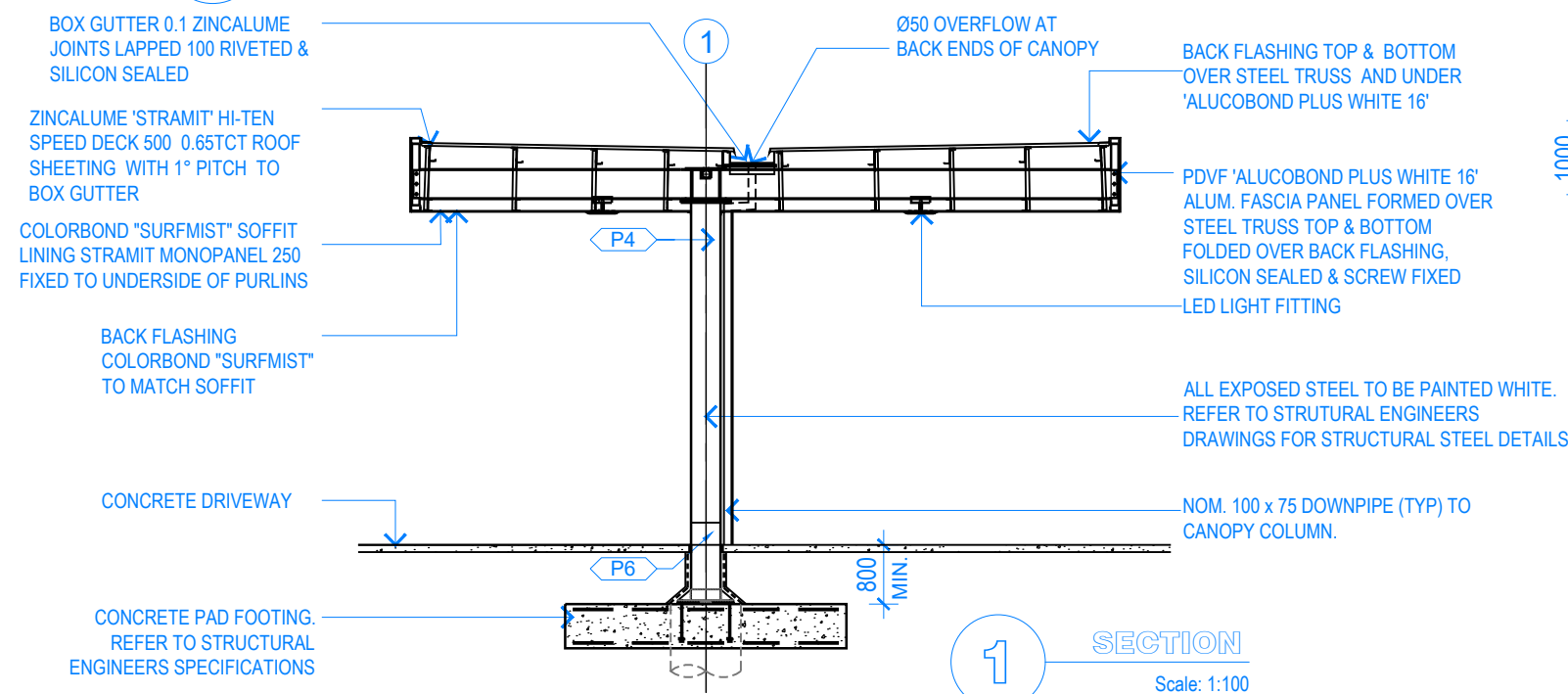
DEVELOPMENT
NOT FOR CONSTRUCTION



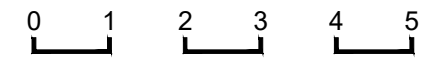


GENERAL NOTES

- CANOPY DESIGN MUST BE IN COMPLIANCE WITH AS1940 AND ALL OTHER RELEVANT STATUTORY AND LOCAL AUTHORITY'S REQUIREMENTS
- BUNDED AREA TO BE DESIGN TO SUIT SITE CONDITIONS



DEVELOPMENT
 NOT FOR CONSTRUCTION



project consultants
 DESIGN | DOCUMENT | DELIVER

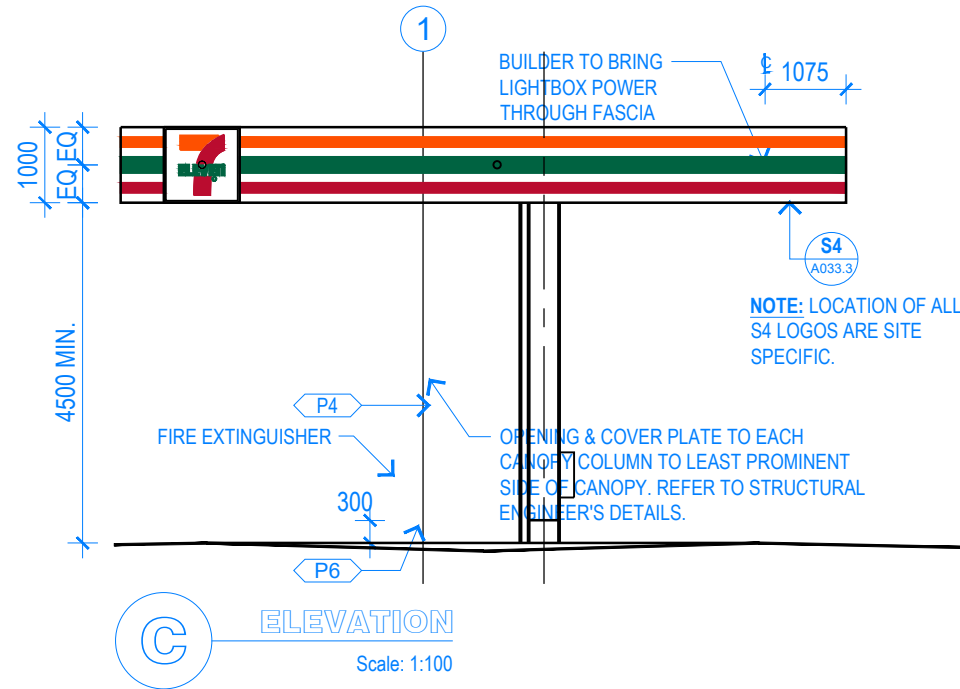
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PROJECT
 PROPOSED SERVICE STATION
 15 ALBATROSS CRESCENT
 EATON, WA, 6232

SHEET
 ELEVATION
 CANOPY
 CAR
 SHEET 1

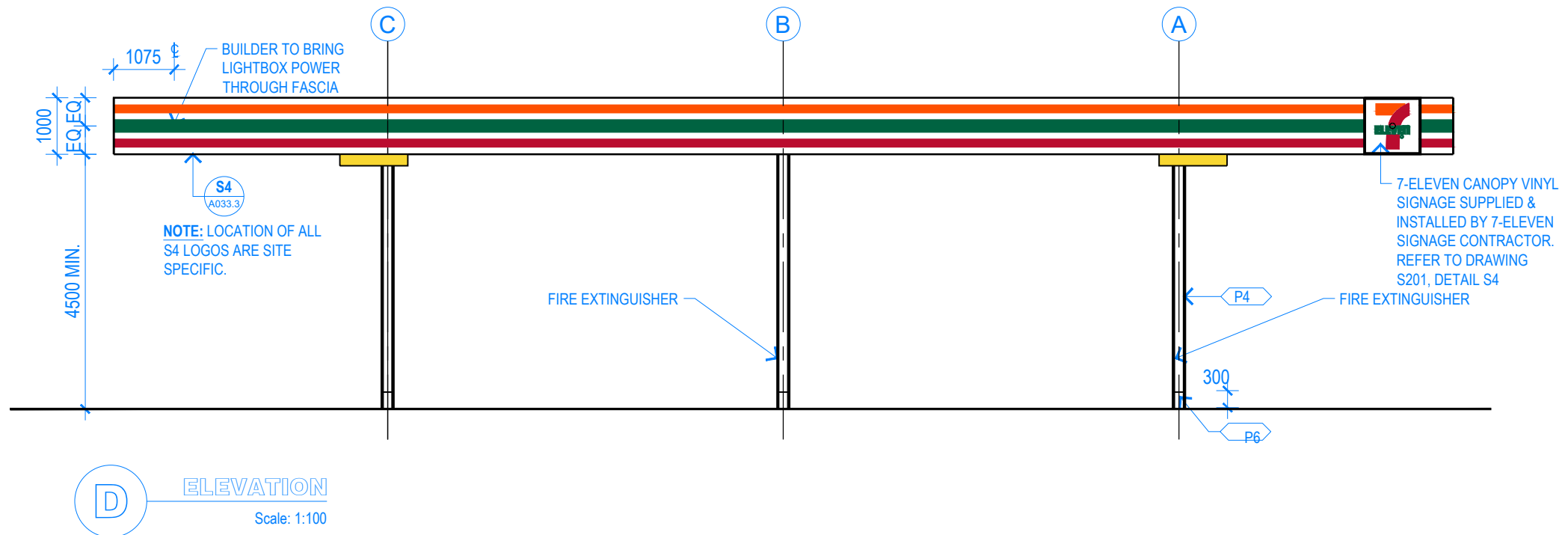
REV.	DESCRIPTION	DATE
A	INITIAL DA PLAN	16/11/21
B	REVISED TO 7 ELEVEN COMMENTS	18/11/21
C	TOWER SIGN ADDED, OTHER AMENDMENTS	23/11/21

A203
 1:100 | C



GENERAL NOTES

1. THIS GENERIC DRAWING IS FOR INFORMATION ONLY AND NOT TO BE USED FOR CONSTRUCTION
2. CANOPY DESIGN MUST BE IN COMPLIANCE WITH AS1940 AND ALL OTHER RELEVANT STATUTORY AND LOCAL AUTHORITY'S REQUIREMENTS
3. BUNDED AREA TO BE DESIGN TO SUIT SITE CONDITIONS



DEVELOPMENT
NOT FOR CONSTRUCTION



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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
ELEVATION
CANOPY
CAR
SHEET 2

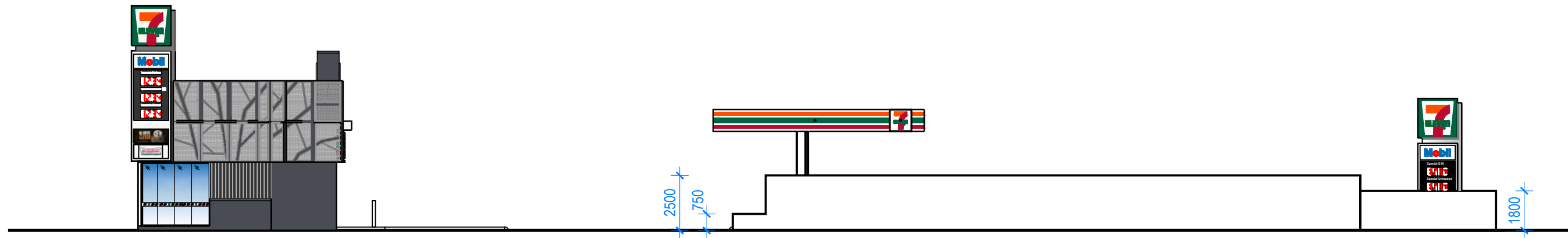
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A	INITIAL DA PLAN
B	REVISED TO 7 ELEVEN COMMENTS
C	TOWER LOGO ADDED, OTHER AMENDMENTS

0 1 2 3 4 5

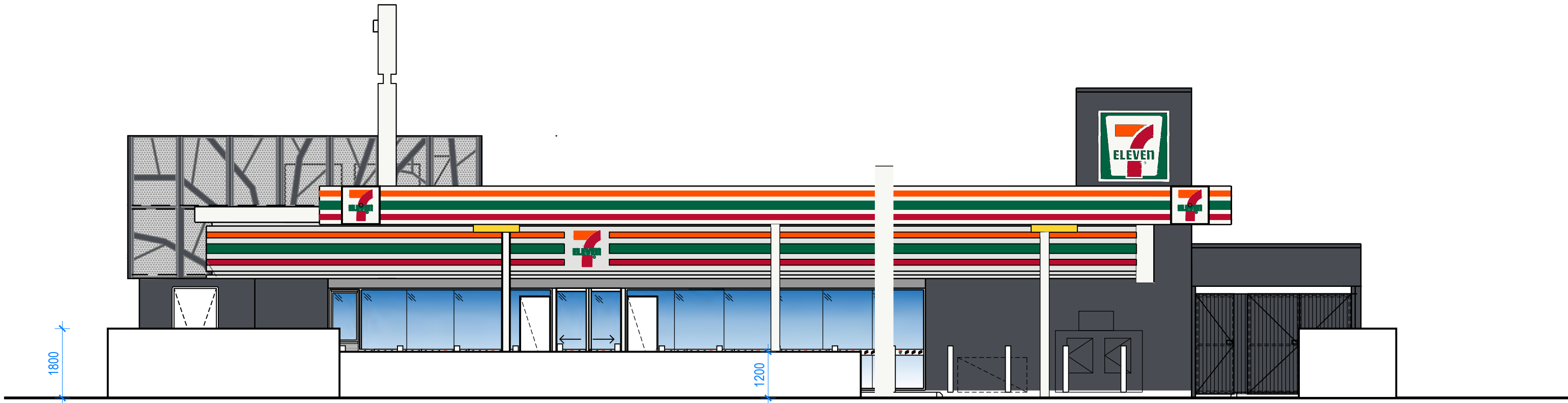
16/11/21
18/11/21
23/11/21

A204

1:100 | **C**

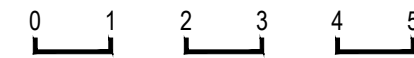


E BLUE WREN DRIVE ELEVATION
 Scale: 1:200



F STREET ELEVATION
 Scale: 1:100

DEVELOPMENT
 NOT FOR CONSTRUCTION



project consultants
 DESIGN | DOCUMENT | DELIVER

PROJECT
 PROPOSED SERVICE STATION
 15 ALBATROSS CRESCENT
 EATON, WA, 6232

SHEET
 ELEVATION
 BUILDING
 SHEET 3

REV. DESCRIPTION
 A NOISE WALLS ADDED
 B NOISE WALLS AMENDED
 C NOISE WALLS AMENDED

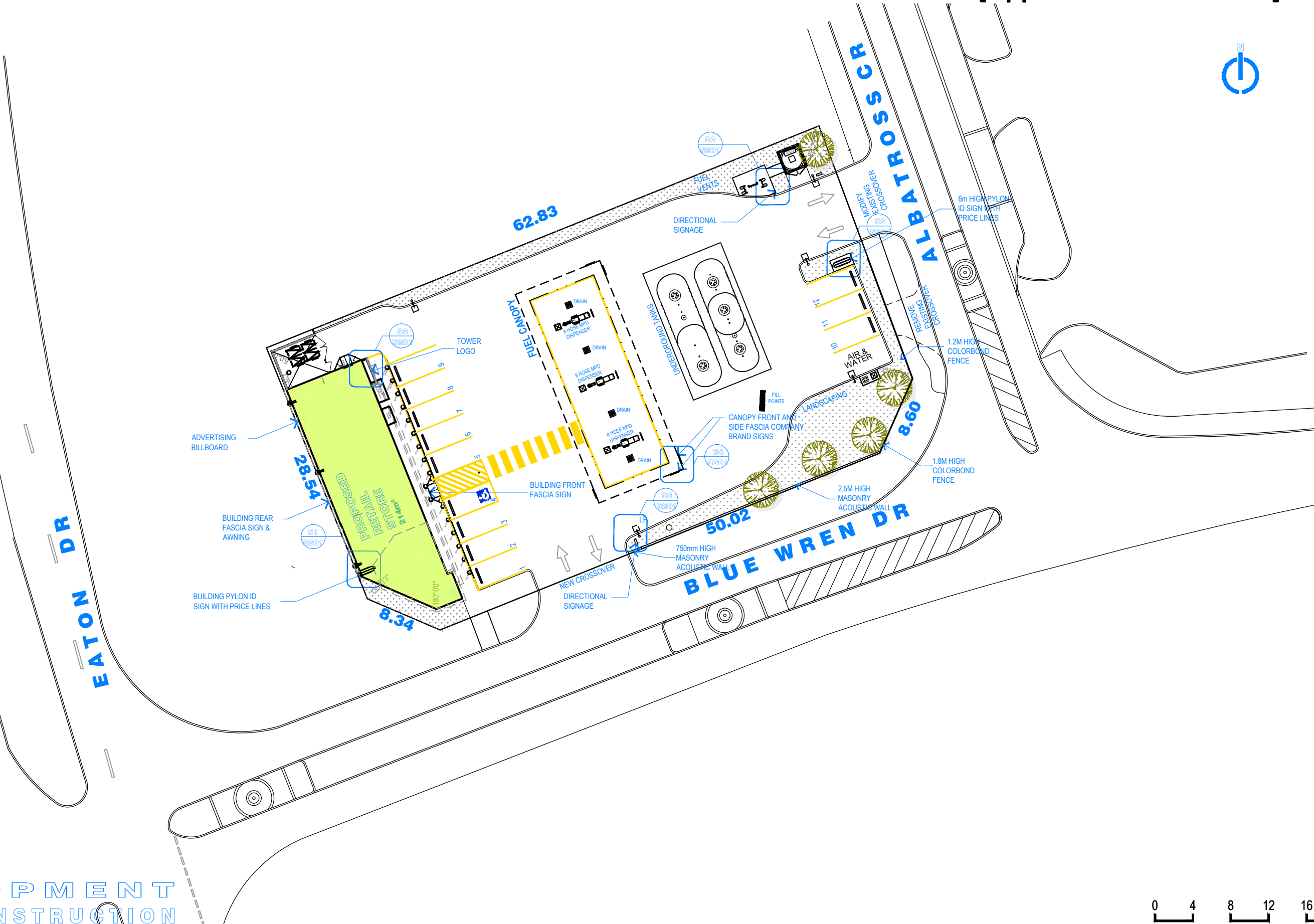
06/12/21
 13/12/21
 16/12/21

A205

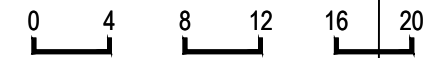
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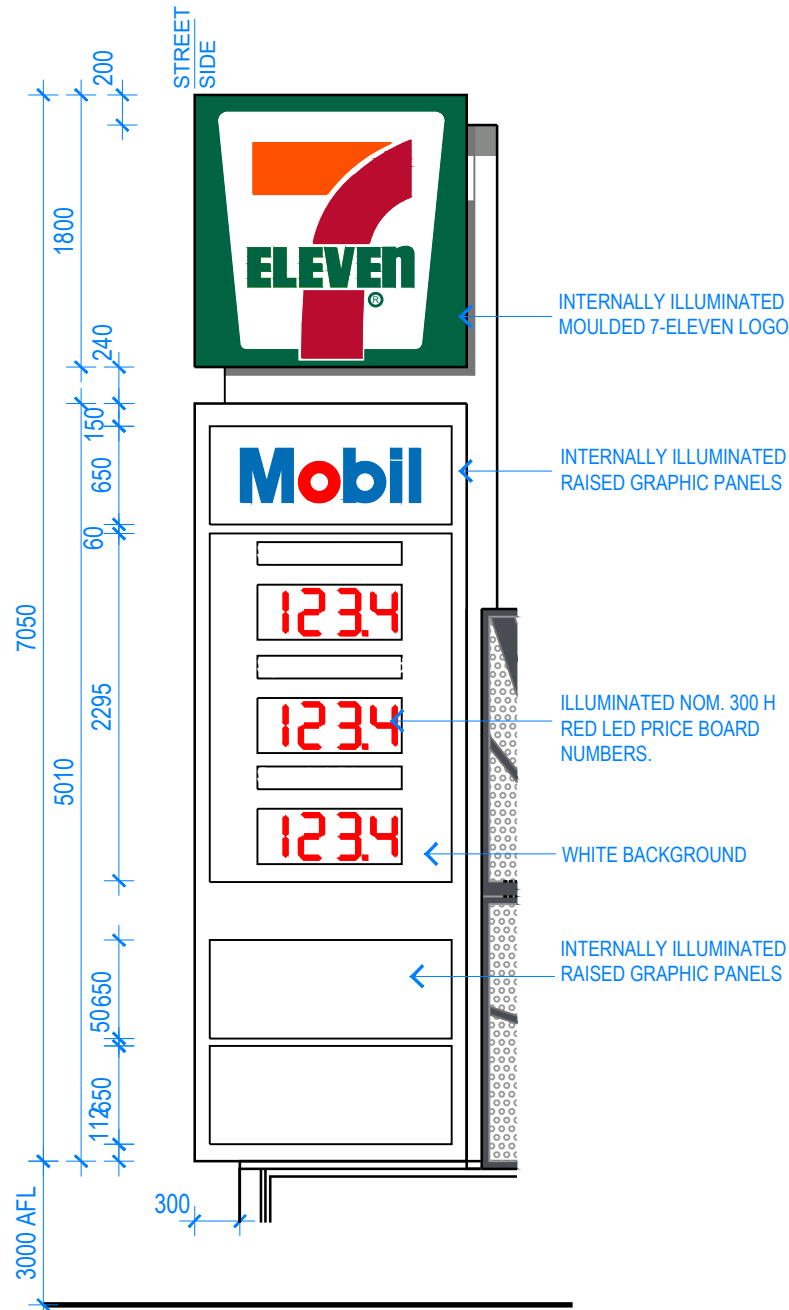
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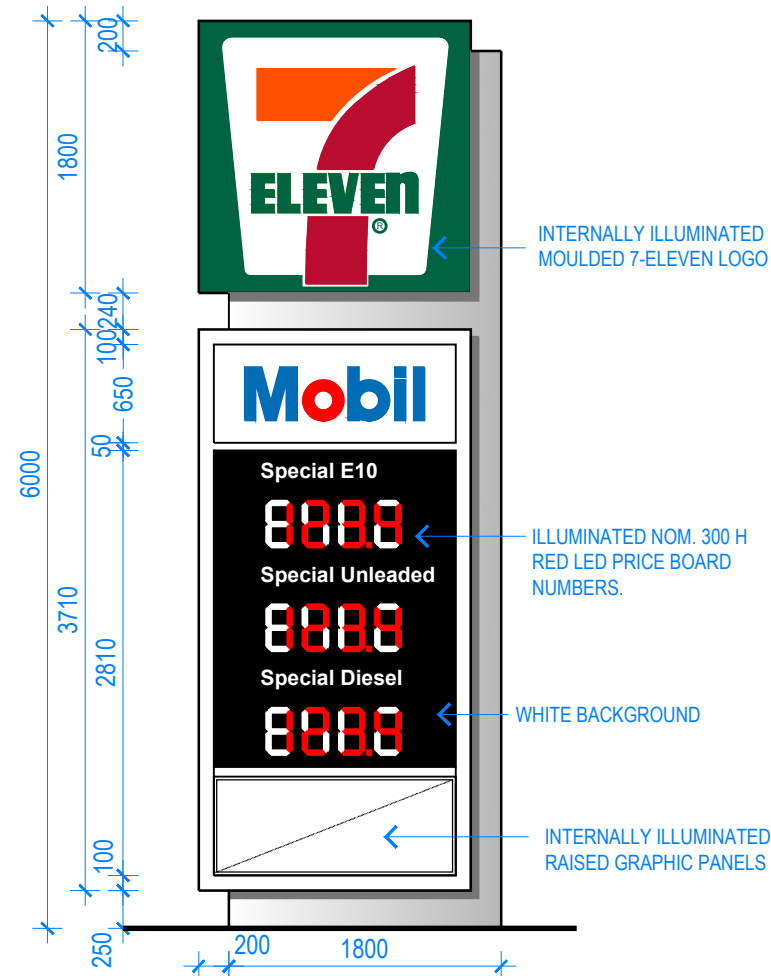


DEVELOPMENT
NOT FOR CONSTRUCTION

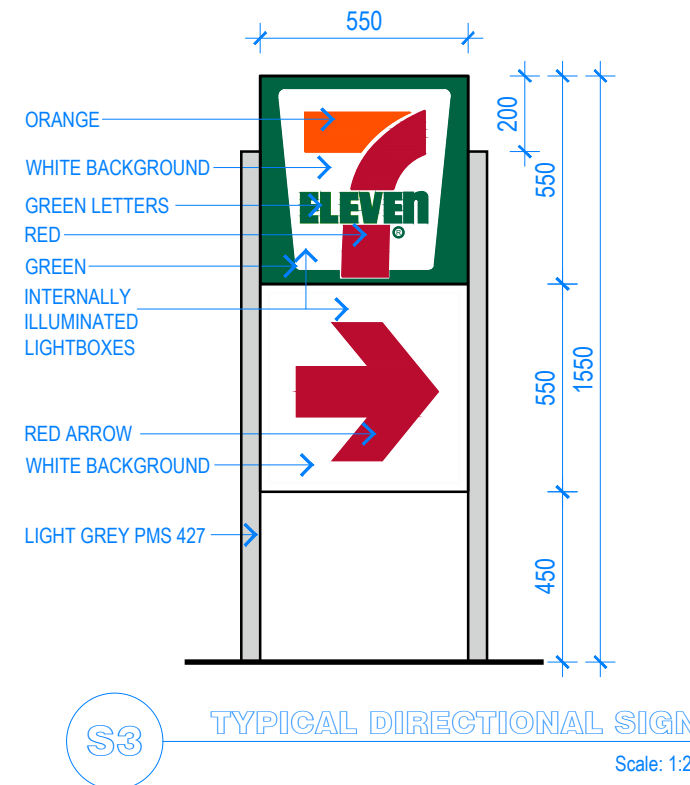




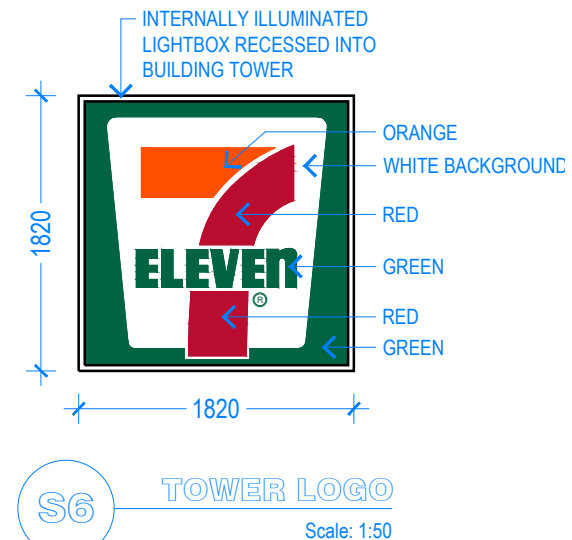
S1 BUILDING SIGN ELEVATION
Scale: 1:50



S2 6M PYLON SIGN ELEVATION
Scale: 1:50



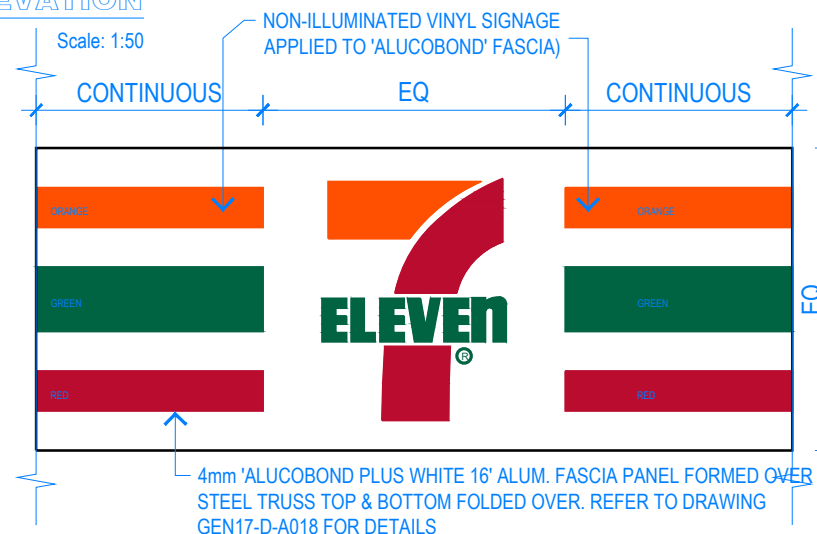
S3 TYPICAL DIRECTIONAL SIGN
Scale: 1:20



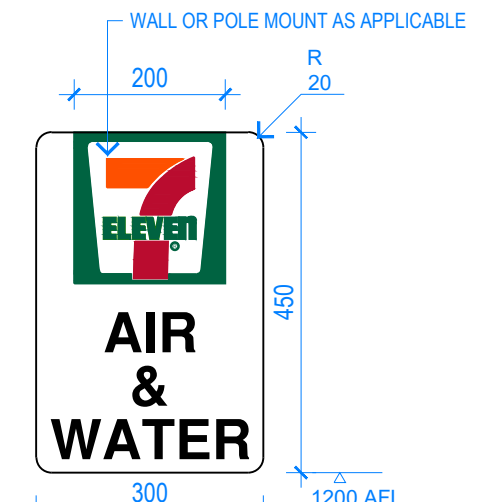
S6 TOWER LOGO
Scale: 1:50

PRICEBOARD NOTES

1. PRODUCTS DISPLAYED ON PRICE BOARDS ARE INDICATIVE ONLY
2. STEEL FRAME TO BE GALVANISED. REFER TO ENGINEER'S DETAIL FOR STRUCTURAL DETAIL



S4 TYPICAL BUILDING / AWNING FASCIA DETAIL
Scale: 1:20



S5 AIR & WATER SIGN
Scale: 1:10

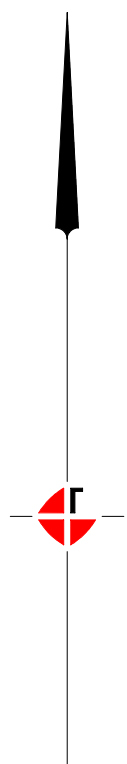
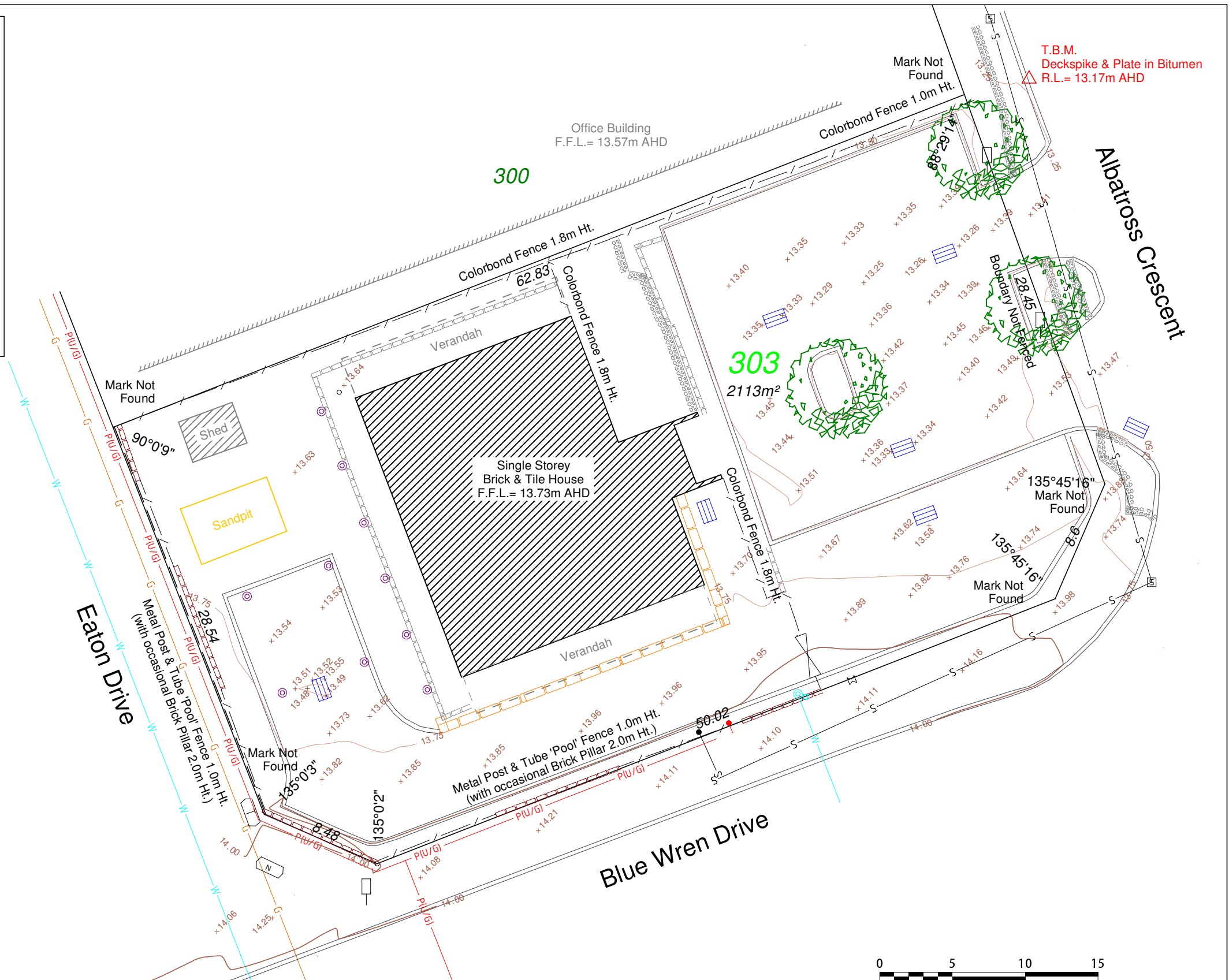
NOTE: REFER TO PROPOSED BUILDING ELEVATIONS FOR DIMENSIONS

DEVELOPMENT
NOT FOR CONSTRUCTION

LEGEND

Tree	Mailbox	Gate	Sign
Power M/H	ROAD EDGE	ROAD CENTRELINE	TOP
LTP	TOE	FENCE	DRAINAGE
PP	POWER (OVERHEAD)	POWER (UNDERGROUND)	SEWER
Power Dome	WATER	BRICK WALL	PAVING
Power Meter	GAS	CONCRETE	KERB
Stay	WATER VALVE	LIMESTONE RETAINING WALL	BUILDING
Hydrant	WATER SV	BOUNDARY	
Tap	Sewer M/H		
Water Conn.	H.C.		
Water Meter	NBN		
Water Valve	Telstra Pit		
Water SV	Comm. Pit		
Sewer M/H	Drainage M/H		
H.C.	Grated Pit		
NBN	Pole		
Telstra Pit	Gas Meter		
Comm. Pit	Survey Peg		
Drainage M/H	Temporary Bench Mark		
Grated Pit	Spot Height (R.L.)		
Pole			
Gas Meter			
Survey Peg			
Temporary Bench Mark			
Spot Height (R.L.)			

NOTE: A.H.D Levels derived from Sewer M/H 0948



Thompson
surveying consultants

A.C.N. 008 938 903 ABN 78 008 938 903
6/18 Casuarina Drive, Bunbury
PO Box 1719 BUNBURY WA 6231
Ph (08) 9721 4000
eMail info@thompsonsurveying.com.au

CLIENT: TOMAHAWK PROPERTY				
PROJECT: LOT 303 ON D.P. 38275 (C/T 2597/585), (# 15) ALBATROSS CRESCENT, EATON				
TITLE: FEATURE SURVEY				
DATUM	HOR: N/A	VERT: A.H.D.	CHKd: SH	
File: DWG20912FS JD		Scale (@A3)	Vert: N/A	
Date: 24/Nov./2021	Rev.n 0		Hor: 1 : 300	

SERVICES:			
ELEC.	U/ground	ROAD TYPE	Bitumen
GAS	Yes	KERB	Mountable
PHONE	Yes	FOOTPATH	Concrete
SEWER	Yes	SOIL	Sand
WATER	Yes	VEGETATION	Bush & Grass
COASTAL CATEGORY: NO			

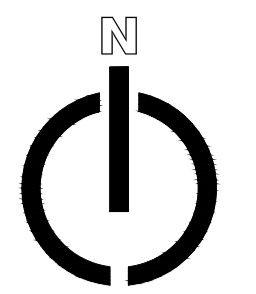
TOMAHAWK PROPERTY

NOTE: IF THE PROPOSED BUILDING IS LESS THAN 1 METRE (or otherwise specified in Information Sheet No. 60) OF THE SEWER MAIN THE WATER CORPORATION WILL REQUIRE A SURVEYED LOCATION OF THE SEWER MAIN PRIOR TO APPROVAL OF THE BUILDING APPLICATION. See Water Corporation - Development Services Branch - Information Sheet No. 60 'Survey Requirements for Building Developments'

NOTE: All services plotted from information supplied by others, site verification required prior to construction. Boundary position is not deemed to be correct until a resurvey has been carried out. Check Certificate of Title for any easements etc. No responsibility accepted for changes occurring after date of survey

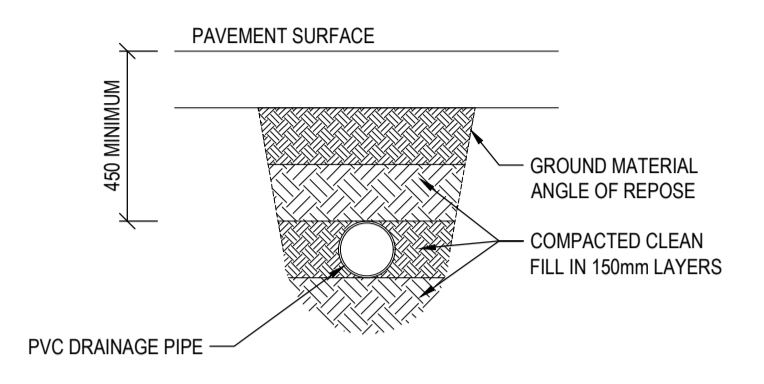
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ANNEXURE 3
Stormwater Design
(JCCE)



SITE DATA:
 SITE CLASSIFICATION = ASSUMED CLASS A SAND
 SOIL PERMEABILITY = ASSUMED 2.5 m/day
 GROUND WATER TABLE = ASSUMED OVER 2.0m BELOW EXISTING GROUND LEVEL

- GENERAL NOTES:**
- DATUM IS LOCAL AND TO BE VERIFIED ON SITE.
 - CHECK ALL DIMENSIONS ON SITE. READ ALL ENGINEERING DRAWINGS IN CONJUNCTION WITH ARCHITECTURAL & SURVEY DRAWINGS. ANY DISCREPANCIES BETWEEN ENGINEERING DRAWINGS AND ARCHITECTURAL DRAWINGS SHALL BE CONFIRMED PRIOR TO COMMENCING CONSTRUCTION. DO NOT SCALE FROM THESE DRAWINGS.
 - ALL WORK TO BE IN ACCORDANCE WITH 'AS3500-2003 PLUMBING & DRAINAGE'. THE 'BUILDING CODE OF AUSTRALIA' AND THE LOCAL AUTHORITY'S STANDARD SPECIFICATIONS.
 - LOT CONNECTION PIT (WHEN APPLICABLE) TO LOCAL AUTHORITY SPECIFICATIONS.
 - WHERE MANHOLES ARE LOCATED IN THE AREAS SUBJECT TO VEHICULAR LOADING, STANDARD TRAFFICABLE LIDS ARE TO BE INSTALLED & BASED TO BUILDERS DETAIL.
 - ALL DRAINAGE PIPEWORK SHALL BE PVC CLASS HD STORMWATER, UNLESS WHERE LOCATED UNDERNEATH ANY STRUCTURES PIPEWORK SHALL BE PVC SEWER CLASS S88.
 - ALIGNMENT OF PIPES SHALL BE AS SHOWN ON THE PLAN & SHALL BE TO THE PIPE OR MANHOLE CENTRELINE.
 - BEFORE CONSTRUCTION COMMENCES, THE CONTRACTOR SHALL:
 - CHECK ON SITE THE LOCATION OF THE EXISTING SERVICES WITH THE APPROPRIATE AUTHORITY. ENSURE PROPOSED STORMWATER PIPE DOES NOT CLASH WITH ANY EXISTING SERVICES.
 - ARRANGE FOR THE LOCATION AND THE LEVEL OF THE CONNECTION POINT TO EXISTING STORMWATER MANHOLE TO BE VERIFIED BY A SURVEYOR.
 - CONFIRM THAT BOUNDARY PEGS OR OTHER SURVEY REFERENCE POINTS TO BE USED IN SETTING OUT OF THE PROJECT ARE LOCATED IN THE CORRECT POSITIONS.
 - ENSURE A PERMIT & REINSTATEMENT SPECIFICATIONS ARE OBTAINED FROM THE LOCAL AUTHORITY IF EXCAVATION WILL BE IN A ROAD RESERVE OR RIGHT OF WAY.
 - ENSURE ALL DETAILS HAVE BEEN CHECKED AND THAT NO DISCREPANCIES EXIST. ALL QUERIES AND DISCREPANCIES ARE TO BE RESOLVED PRIOR TO COMMENCING WORKS.
 - ALL EXCAVATIONS SHALL BE SECURED & MADE SAFE IN ACCORDANCE WITH REQUIREMENTS OF THE OCCUPATIONAL SAFETY & HEALTH ACT OF 1984, THE OCCUPATIONAL SAFETY & HEALTH REGULATION 1996 & OF ANY RELEVANT REGULATORY BODY.
 - PROPERTIES WHICH HAVE BEEN EXCAVATED SHALL BE RETURNED TO AT LEAST A SIMILAR CONDITION TO THAT WHICH EXISTED PRIOR TO CONSTRUCTION.
 - TRENCH BACKFILL SHALL BE CLEAN GRANULAR MATERIAL, COMPACTED TO A LEVEL NOT LESS THAN THAT OF THE SURROUNDING UNDISTURBED GROUND, FOR THE FULL DEPTH OF EXCAVATION. BACKFILL UNDER ROADS SHALL BE COMPACTED TO THE REQUIREMENTS OF THE LOCAL AUTHORITY.
 - ALL CONNECTION INTO EXISTING LOCAL AUTHORITY STORMWATER ARE TO BE CARRIED OUT BY THE CONTRACTOR TO LOCAL AUTHORITY SPECIFICATIONS. THIS STORM WATER DESIGN IS TO BE READ IN CONJUNCTION WITH THE BUILDERS ARCHITECTURAL DRAWINGS (PARTIALLY REPRODUCED HERE). CLIENT IS TO ENSURE LOCAL AUTHORITY HAVE APPROVED THESE DRAWINGS BEFORE BEING ISSUED FOR PRICING, TENDER & CONSTRUCTION.

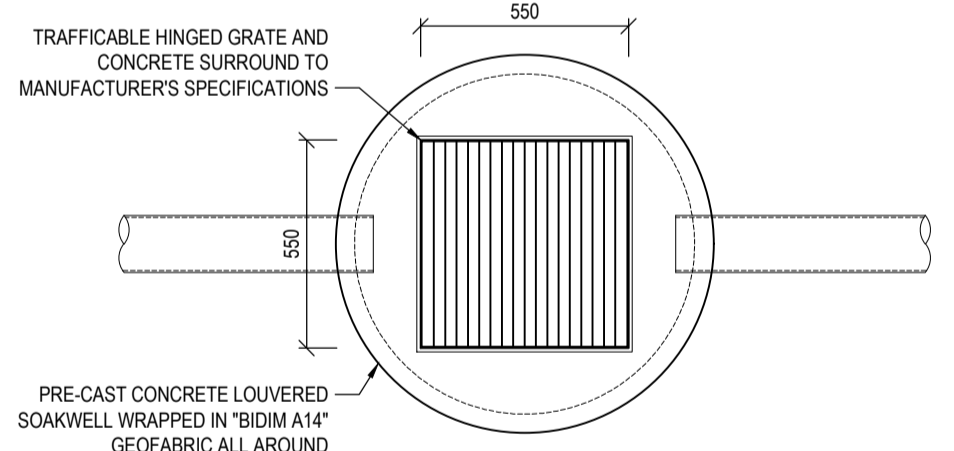


TYPICAL PVC DRAINAGE PIPE BEDDING DETAILS
SCALE 1:20

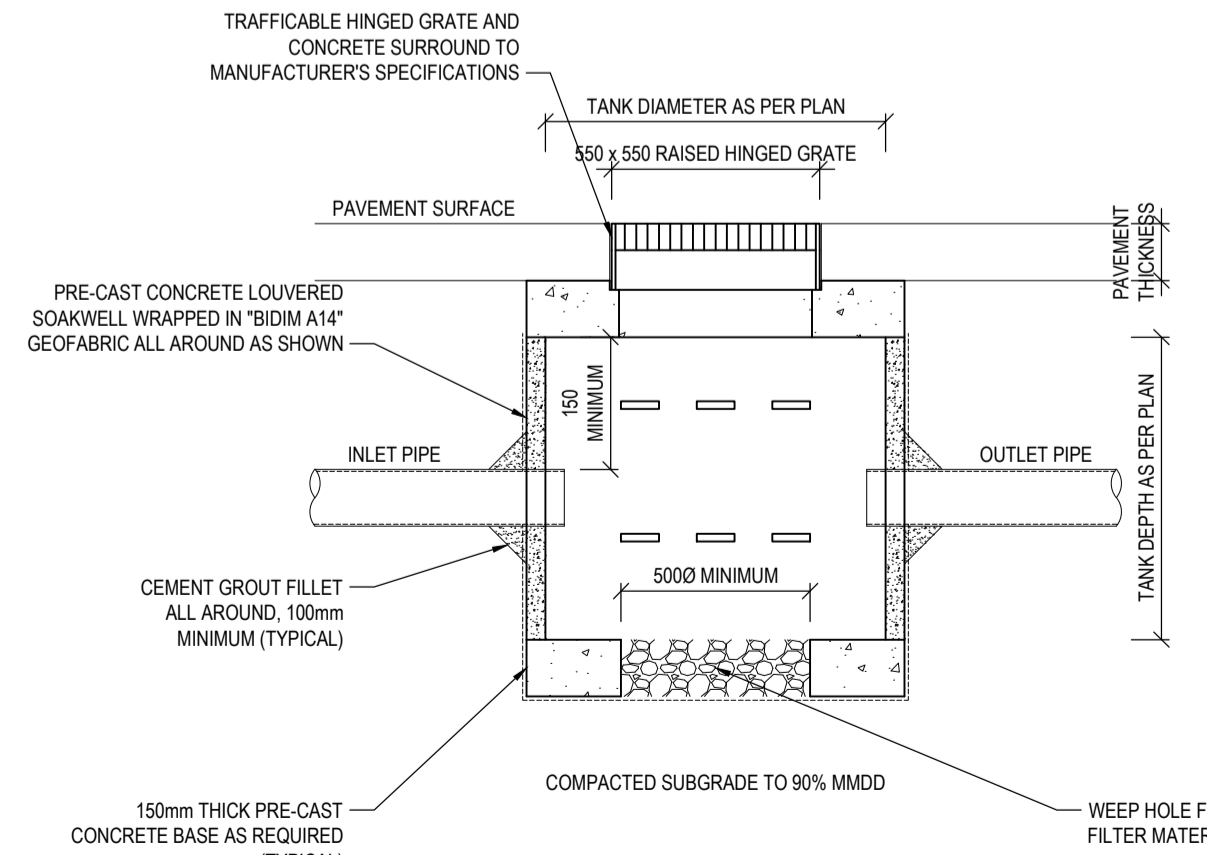
- LEGEND:**
- 1500 uPVC CLASS S88 PIPE
 - 1500 uPVC CLASS S88 PIPE FROM BUILDING DOWNPIPE UNO
 - DN160 UPP FUEL PIPE OR EQUIVALENT
 - ⊠ TRAFFICABLE STORMWATER TANK WITH GRATE COVER
 - ⊠ TRAFFICABLE STORMWATER TANK WITH CONCRETE COVER
 - ⊠ TRAFFICABLE PRECAST SUMP PIT WITH GRATE COVER
 - FALL DIRECTION
 - IL+10.00 PIPE INVERT LEVEL
 - TT+10.00 TOP OF TANK LEVEL
 - RL+10.00 PAVEMENT LEVEL
 - HP+10.00 PAVEMENT HIGH POINT
 - LP+10.00 PAVEMENT LOW POINT

DRAINAGE CALCULATIONS:

IMPERVIOUS AREA	1777 sqm
REQUIRED STORAGE	1777 / 65
	27.34 cum
SOAKWELL SIZE	1800mm Ø x 1200mm DEEP
STORAGE CAPACITY PER SOAKWELL	3.05 cum
NUMBER OF SOAKWELLS	9
TOTAL STORAGE CAPACITY	27.45 cum

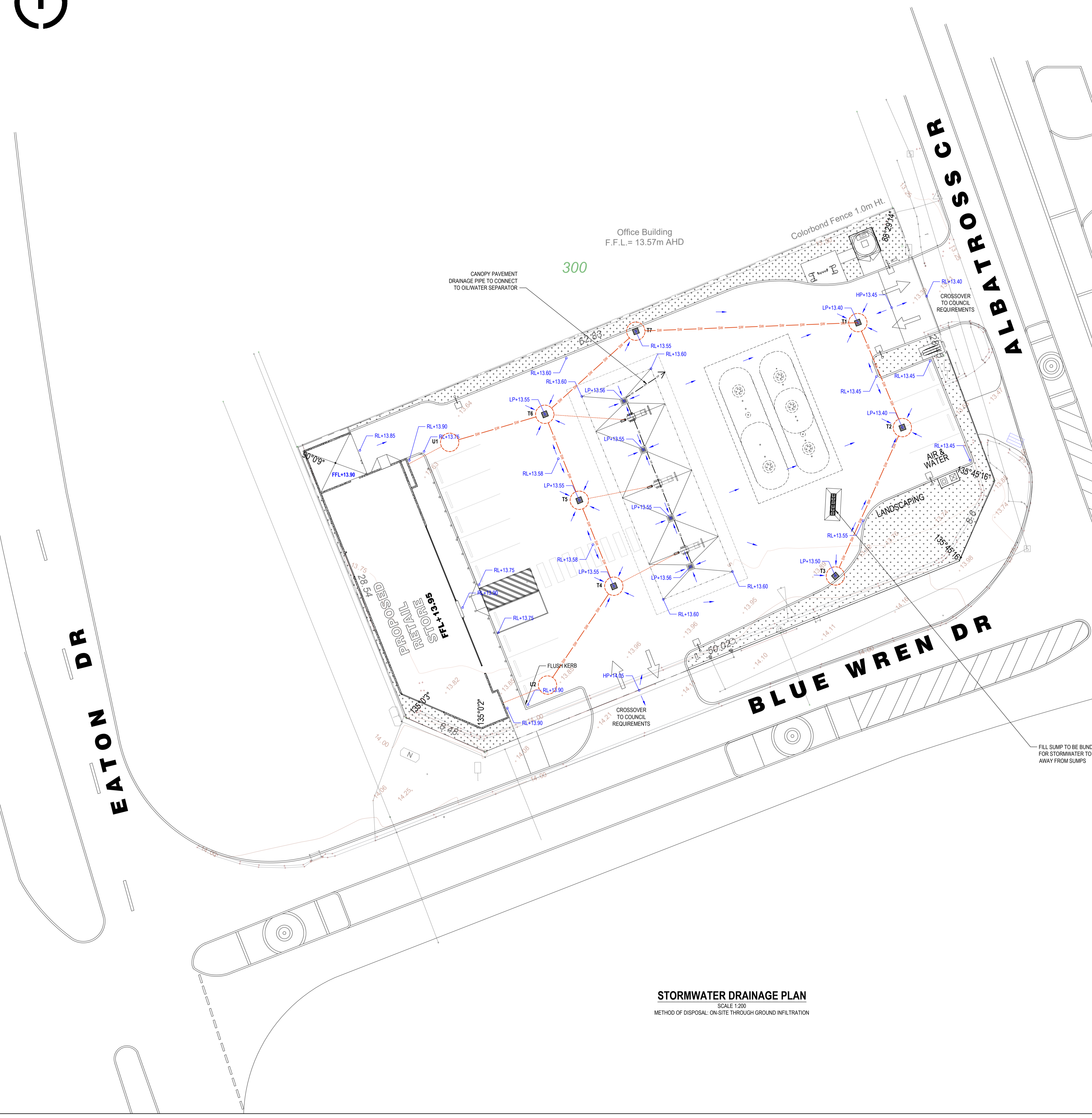


TYPICAL SOAKWELL TANK PLAN
SCALE 1:20



TYPICAL SOAKWELL TANK DETAILS
SCALE 1:20

NOTE: ALL UNITS ARE IN MILLIMETERS UNO



STORMWATER DRAINAGE PLAN
SCALE 1:200
METHOD OF DISPOSAL: ON-SITE THROUGH GROUND INFILTRATION

TANK SCHEDULE

TANK NUMBER	TANK SIZE	TOP OF TANK LEVEL	INLET/OUTLET PIPE INVERT LEVEL
T1 - T2	18000 x 1200DEEP	TT + 13.40	IL + 12.60
T3	18000 x 1200DEEP	TT + 13.50	IL + 12.70
T4 - T7	18000 x 1200DEEP	TT + 13.55	IL + 12.75
U1	18000 x 1200DEEP	TT + 13.45	IL + 12.95
U2	18000 x 1200DEEP	TT + 13.60	IL + 13.10

A	ISSUE FOR DEVELOPMENT APPROVAL	JC	12-DEC-2021
REV	DESCRIPTION	BY	DATE
ISSUE FOR DEVELOPMENT APPROVAL			
PROJECT: PROPOSED SERVICE STATION 15 ALBATROSS CRESCENT, EATON, WESTERN AUSTRALIA			
TITLE: STORMWATER DRAINAGE PLAN AND DETAILS			
DRAWING: J. GABRIEL	SCALE: AS NOTED	PROJECT #: 20673	REVISION: A
DESIGN: J. CUBONG	PAPER SIZE: A1	DRAWING #: C1	



ANNEXURE 4
Landscaping Plan
(Urban Retreat Garden Design)



LANDSCAPE PLAN DRAWING INDEX












DRAWING NO.	TITLE
21181_01	LANDSCAPE PLAN
21181_02	PLANT SCHEDULE
21181_03	PLANTING SPECIFICATIONS






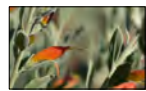















TOTAL SITE LANDSCAPE AREA = 274m² (13% OF SITE AREA)



Scale 1:200

revision/issue	description	drawn	checked	date
B	DEVELOPMENT APPLICATION	AC	AC	13.12.2021
A	DEVELOPMENT APPLICATION	AC	AC	10.12.2021
project		drawn	description	
PROPOSED SERVICE STATION		AC	LANDSCAPE PLAN	
location		checked	AC	
15 ALBATROSS CRESCENT EATON WA		checked	AC	
scale		date 10.12.2021		
1:300 @ A3 unless indicated		project no	dwg no	rev
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PLANT SCHEDULE					
SYMBOL	CODE	BOTANIC NAME	MATURE HEIGHT X WIDTH	MINIMUM INSTALLATION SIZE	NUMBER
TREES					
	Ef	Eucalyptus forrestiana 'Fuchsia Gum'	6m x 4m	75 Litre	5
SHRUBS					
	Ac	Adenanthos cunninghamii 'Lighthouse'	1m x 1m	5 Litre	13
	Bp	Banksia petiolaris	60cm x 3m	13cm	6
	Cv	Callistemon viminalis 'Slim'	3m x 1.2m	5 Litre	29
	Dd	Damperia diversifolia	50cm x 1m	13cm	19
	Eg	Eremophila glabra 'Dawes'	80cm x 1m	13cm	14
	Hs	Hibbertia spicata 'Ocean Reef'	50cm x 25cm	17cm	19
	Pf	Pimelea ferruginea 'White Solitaire'	50cm x 60cm	14cm	10
	Rb	Rhagodia baccata	1m x 2m	Tube	17
	Sc	Scaevola crassifolia prostrate	50cm x 2m	13cm	20
	Ts	Thryptomene saxicola 'Pink Lace'	50cm x 1m	14cm	7
	Vc	Verticordia chrysanthella	50cm x 1m	13cm	22
GRASSES					
	Ah	Anigozanthos hybrid 'Everlasting Gold'	55cm x 35cm	14cm	31
	Cc	Conostylis candicans	30cm x 50cm	14cm	30
GROUND COVER					
	As	Acacia saligna prostrate	30cm x 3m	13cm	7
	Eg	Eremophila glabra 'Kalbarri Carpet'	20cm x 2m	13cm	18
	Ga	Grevillea alpina x rosmarinifolia 'Firecracker'	40cm x 80cm	13cm	11
	Gc	Grevillea crithmifolia 'Green Carpet'	20cm x 2m	13cm	15
	Gl	Grevillea laurifolia x willsii 'Royal Mantle'	50cm x 3m	14cm	14
	Hp	Hemiandra pungens alba	30cm x 1.5m	13cm	24
TOTAL PLANTS					331

TREE VARIETIES					
	Eucalyptus forrestiana				
SHRUB VARIETIES					
					
Adenanthos cunninghamii 'Lighthouse'	Banksia petiolaris	Callistemon viminalis 'Slim'	Damperia diversifolia	Eremophila glabra 'Dawes'	Hibbertia spicata
					
Pimelea ferruginea 'White Solitaire'	Rhagodia baccata	Scaevola crassifolia prostrata	Thryptomene saxicola 'Pink Lace'	Verticordia chrysanthella	
GRASS VARIETIES					
	Anigozanthos hybrid 'Everlasting Gold'		Conostylis candicans		
GROUND COVER VARIETIES					
					
Acacia saligna prostrate	Eremophila glabra 'Kalbarri Carpet'	Grevillea alpina x rosmarinifolia 'Firecracker'	Grevillea crithmifolia 'Green Carpet'	Grevillea laurifolia x willsii 'Royal Mantle'	Hemiandra pungens alba
EXISTING VERGE TREE LEGEND					
	Verge trees to be retained (4)				

B	DEVELOPMENT APPLICATION	AC	AC	13.12.2021
A	DEVELOPMENT APPLICATION	AC	AC	10.12.2021
revision/issue	description	drawn	checked	date
project	PROPOSED SERVICE STATION	drawn	AC	description
location	15 ALBATROSS CRESCENT EATON WA	checked	AC	PLANT SCHEDULE
		scale	date	10.12.2021
		project no	21181	dwg no
				02
				rev
				2

PLANTING SPECIFICATIONS

1. SPECIFIED PLANT SPECIES HAVE BEEN SOURCED FROM BENARA NURSERY, DOMUS NURSERY, TRILLION TREES AND APACE WA STOCK LISTS. SHOULD PLANT SPECIES BE UNAVAILABLE AT TIME OF PLANTING, CONTACT DESIGNER FOR SUBSTITUTIONS.

2. ALL PLANTING AREAS ARE TO BE PREPARED AND PLANTED IN ACCORDANCE WITH INDUSTRY BEST PRACTICE, TYPICAL DRAWINGS INCLUDED IN THIS PLAN SET AND THE INSTRUCTIONS BELOW.

A. PREPARATION OF SOIL IN GARDEN BED AREAS:

- I. REMOVE ALL TRACES OF BUILDERS' MATERIAL FROM PLANTING AREAS INCLUDING RUBBLE, SAND, MORTAR AND ALL OTHER EXTRANEIOUS MATERIAL.
- II. REMOVE ALL WEEDS IN GARDEN BED AREAS BY SPRAYING WEEDS WITH AN STANDARD INDUSTRY HERBICIDE FOLLOWING MANUFACTURER'S SPECIFICATIONS AND LEAVE FOR RECOMMENDED TIME.
- III. REMOVE DEAD PLANT MATTER AFTER TIME SPECIFIED ON HERBICIDE PRODUCT.
- IV. UNDERTAKE SOIL IMPROVEMENT SUITABLE FOR NATIVE PLANTS.

B. PLANTING

- I. PLANT TREES AS SPECIFIED IN DRAWING 'TYPICAL TREE PLANTING'.
- II. PLANT SHRUBS (INCLUDING GRASSES AND GROUND COVERS) AS SPECIFIED IN DRAWING 'TYPICAL SHRUB PLANTING' (REFER THIS PLAN SET FOR DETAIL) AND WATER IN THOROUGHLY.

C. MULCH

- i. AFTER PLANTING, APPLY PINE BARK WOOD CHIPS TO A MINIMUM DEPTH OF 75MM (MAXIMUM 100MM) TO PLANTING BEDS, KEEPING MULCH CLEAR OF PLANT STEMS.
- II. CHECK MULCH IS FREE OF WOOD SLIVERS AND EXTRANEIOUS MATERIAL
- III. TIDY AND GRADE MULCH AFTER APPLICATION.

IRRIGATION SPECIFICATIONS

- 1. ALL GARDEN BEDS TO BE IRRIGATED. WATER SUPPLY IS SCHEME WATER.
- 2. IRRIGATION LAYOUT BY OTHERS.

EXISTING TREES

- 1. EXISTING VERGE TREES TO BE RETAINED TO BE PROTECTED DURING LANDSCAPE INSTALLATION.

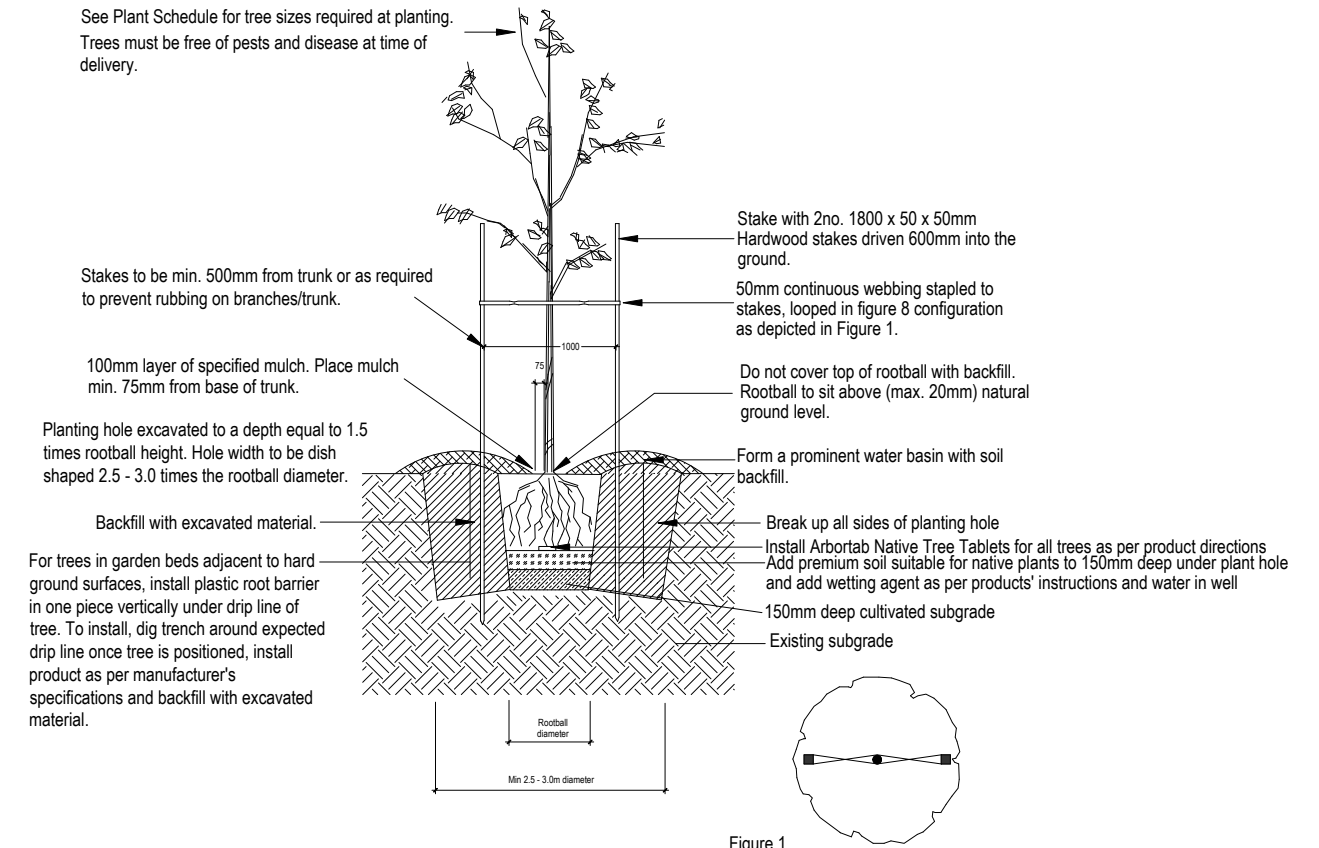
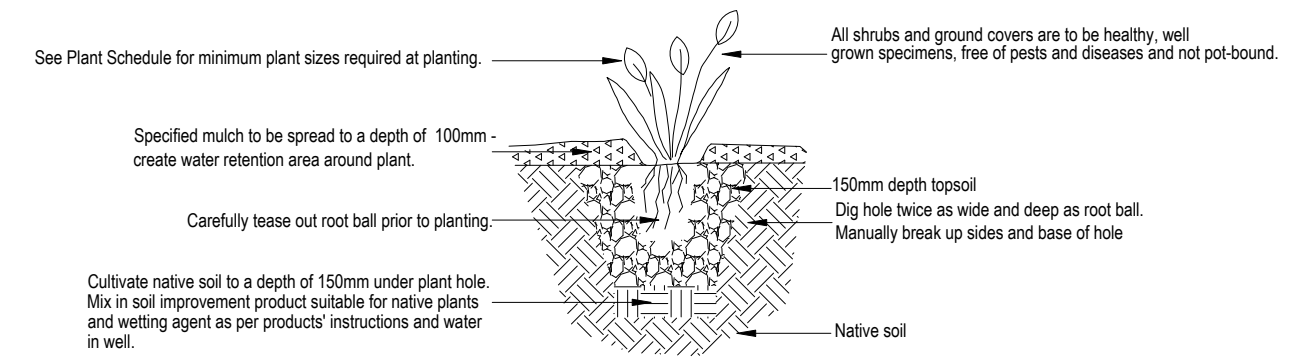


Figure 1.
Tree stakes and tie 'double figure of eight' configuration.

2 TREE PLANTING DETAIL
Typical Section

Not to scale



3 SHRUB PLANTING DETAIL
Typical Section

Not to scale

A DEVELOPMENT APPLICATION		AC	AC	10.12.2021
revision/issue	description	drawn	checked	date
project	PROPOSED SERVICE STATION	drawn	checked	description
location	15 ALBATROSS CRESCENT EATON WA	AC	AC	PLANTING SPECIFICATIONS
scale		project no	date	dwg no
		21181	10.12.2021	03
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ANNEXURE 5
Traffic Impact Statement
(Transcore)



transport planning
traffic engineering
modelling

Proposed Service Station

15 Albatross Crescent, Eaton

Transport Impact Statement

PREPARED FOR:
Tomahawk Property

December 2021

Document history and status

Author	Revision	Approved by	Date approved	Revision type
Shaju Maharjan	r01	R White	9/12/2021	
Shaju Maharjan	r01a	R White	20/12/2021	Final
Shaju Maharjan	r01b	R White	20/12/2021	Minor Amendment

File name: t21.315.sm.r01b

Author: Shaju Maharjan

Project manager: Behnam Bordbar

Client: Tomahawk Property

Project: 15 Albatross Crescent, Eaton

Document revision: r01b

Project number: t21.315

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APPENDIX A: PROPOSED DEVELOPMENT PLAN

APPENDIX B: TURN PATH ANALYSIS



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1 Introduction

This Transport Impact Statement (TIS) has been prepared by Transcore on behalf of Tomahawk Property with regard to the proposed service station to be located at 15 Albatross Crescent, Eaton in the Shire of Dardanup.

The subject site is located on the northern side of Blue Wren Drive between Eaton Drive and Albatross Crescent as shown in **Figure 1**.

The subject site is currently occupied by South West Community Care (Eaton) with two driveway crossovers on Albatross Crescent. The subject site is bounded by commercial property to the north, Eaton Drive to the west, Blue Wren Drive to the south and Albatross Crescent to the east.

Pedestrian access to the subject site is available via an existing footpath network along Albatross Crescent fronting the subject site.

The Transport Impact Assessment Guidelines (WAPC, Vol 4 – Individual Developments, August 2016) states: *“A Transport Impact Statement is required for those developments that would be likely to generate moderate volumes of traffic¹ and therefore would have a moderate overall impact on the surrounding land uses and transport networks”*.

Section 6 of Transcore’s report provides details of the estimated trip generation for the proposed development. Accordingly, as the total peak hour vehicular trips are estimated to be less than 100 trips, a Transport Impact Statement is deemed appropriate for this development.

Key issues that will be addressed in this report include the traffic generation and distribution of the proposed development, access and egress movement patterns and assessment of fuel tanker and service vehicle operations.

¹ *Between 10 and 100 vehicular trips per hour*



Figure 1: Location of the subject site

2 Proposed Development

The Development Application (DA) for the subject site proposes a service station comprising:

- ✚ A fuel canopy with a total of six fuel-filling positions (3 bowsers) for light vehicles;
- ✚ A convenience store having 215m² GFA;
- ✚ A total of 12 parking bays inclusive of one ACROD bay;
- ✚ One air & water bay; and,
- ✚ A designated loading bay.

It is Transcore's understanding that adequate parking supply is provided on site to address the parking requirements for the proposed development.

Bin storage area is provided at the north western corner of the convenience store building. The waste collection/deliveries will be accommodated within the site.

The proposed development plan is included in **Appendix A**.

3 Vehicle Access and Parking

3.1 Access

Figure 2 shows the two existing crossovers on Albatross Crescent currently providing access to the subject site.

As part of the proposed development the existing northern crossover on Albatross Crescent will be retained/modified to provide better access facility to the site whereas the southern crossover on Albatross Crescent will be removed. The proposal also entails one new full movement crossover on Blue Wren Drive to the south of the subject site. The location of crossovers of the proposed development is shown in Figure 3.



Figure 2. Existing crossovers

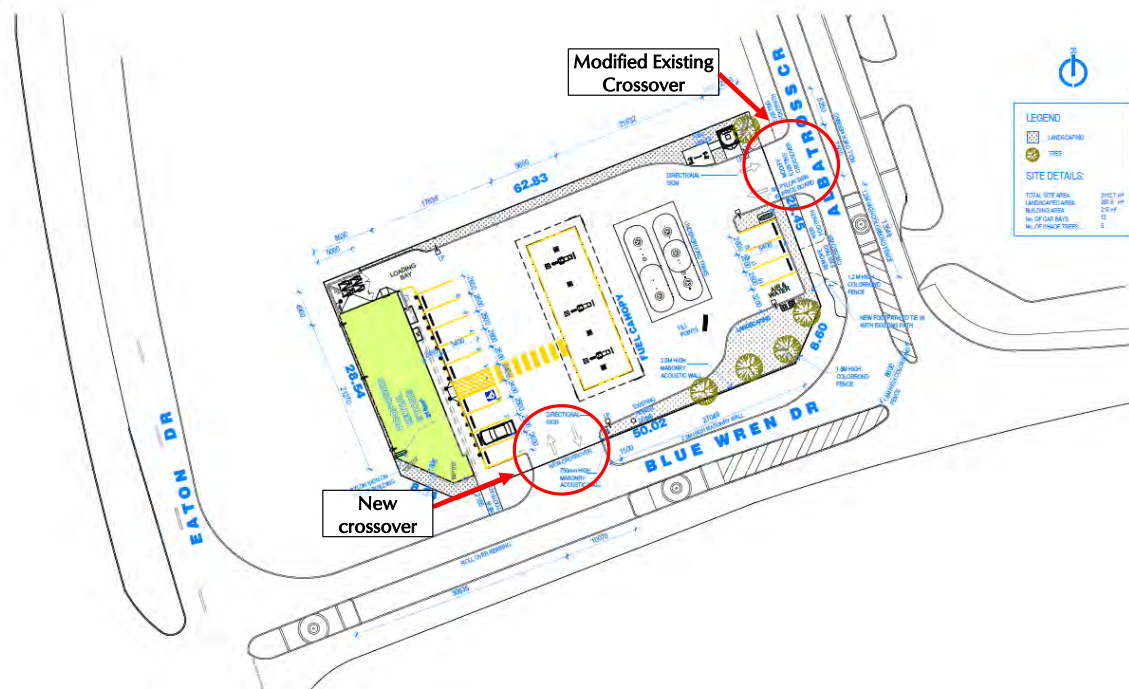


Figure 3. Proposed development crossovers

3.2 Parking

According to the development plan prepared by Project Consultants in **Appendix A**, the development provides the parking provision of 12 car parking spaces (inclusive of one ACROD bay) plus one air and water bay.

It is considered that the proposed parking provision is sufficient to accommodate the demands of the proposed development.

4 Provision for Service Vehicles

The largest truck size that is expected to use the site would be a 17.25m long fuel tanker. Fuel tankers are proposed to enter the site via the proposed full movement crossover on Blue Wren Drive, access the fill point and then exit the site via the same crossover onto Blue Wren Drive in forward gear.

A designated loading bay is proposed at the northeast corner of the convenience store building. Service vehicles are also expected to enter the site via the proposed crossover on Blue Wren Drive, reverse into the loading bay and then exit the site via the same crossover onto Blue Wren Drive in forward gear.

According to the information provided to Transcore, it is understood that deliveries will occur during the night time.

The swept path analysis undertaken for 17.25m fuel tanker and 8.8m service vehicle confirm satisfactory access, egress and circulation within the site and are included in **Appendix B**.

5 Hours of Operation

The proposed development is expected to operate 24 hours 7 days a week.

6 Daily Traffic Volumes and Vehicle Types

6.1 Trip Generation and Distribution

6.1.1 Existing Traffic Generation

The subject site is presently occupied by South West Community Care (Eaton) with a gross building floor area of approximately 465m².

The traffic generation for the existing land use was estimated based on relevant trip rate from *the Institute of Transportation Engineers – Trip Generation Manual 10th Edition (ITE)* document. The trip rates used to estimate the existing traffic generation are as follows:

South West Community Care – Small Office Building (712)

- ✚ Weekday daily: 16.19vpd per 1000 sqft GFA/ 0.929 = 17.42/100m² GFA;
- ✚ Weekday AM peak hour: 1.92vph per 1000sqft GFA/ 0.929 = 2.06vph/100m² GFA; and,
- ✚ Weekday PM peak hour: 2.45vph per 1000sqft GFA/ 0.929 = 2.63vph/100m² GFA.

Accordingly, the estimated traffic generation for the existing development were:

- ✚ Weekday daily: 17.42x4.65 (building GFA) = 81vpd;
- ✚ Weekday AM peak hour: 2.06x4.65 (building GFA) = 10vph; and,
- ✚ Weekday PM peak hour: 2.63x4.65 (building GFA) = 13vph.

Therefore, it is estimated that the existing development generates about **81** vehicular trips per day on a typical weekday with approximately **10** and **13** trips during a typical weekday AM and PM peak hours respectively. These figures include both inbound and outbound vehicle movements.

6.1.2 Proposed Development Traffic Generation

The traffic generation rates for the proposed service station were also sourced from the *Institute of Transportation Engineers – Trip Generation Manual 10th Edition (ITE)* document using “Gasoline/ Service Station with Convenience Store (945)” land use as reference.

The trip rates which were used to estimate traffic generation for the proposed development are as follows:

Service Station – Gasoline/ Service Station with Convenience Store (945)

- ✚ Weekday daily: 205.36vpd per fuelling point;
- ✚ Weekday AM peak hour: 12.47vph per fuelling point; and,
- ✚ Weekday PM peak hour: 13.99vph per fuelling point.

Accordingly, it is estimated that the traffic generation for the proposed service station are:

- ✚ Weekday daily: $205.36 \times 6 = 1,232$ vpd;
- ✚ Weekday AM peak hour: $12.47 \times 6 = 75$ vph; and,
- ✚ Weekday PM peak hour: $13.99 \times 6 = 84$ vph.

Therefore, it is estimated that the proposed service station development would generate **1,232** vehicular trips per day on a typical weekday with approximately **75** and **84** trips during the typical weekday AM and PM peak hours respectively. These figures include both inbound and outbound vehicle movements.

The directional split of inbound and outbound trips for the proposed development is assumed to be about 50/50 for inbound and outbound during the peak hours.

6.2 Traffic Impact Comparison

Allowing for the existing land use, it is estimated that the net additional traffic as a result of the proposed service station development is approximately **1,151** vehicle trips per day on a typical weekday and **65** and **71** trips for typical weekday morning and afternoon peak hours respectively. These figures include both inbound and outbound vehicle movements as shown in **Table 1**.

Table 1. Trip generation comparison table

	Existing Development	Proposed Development	Net Increase
Weekday daily	81	1,232	+1,151
Weekday AM peak hour	10	75	+65
Weekday PM peak hour	13	84	+71

Therefore, as a result of the proposed development the maximum net increase of about 71 peak hour trips for typical weekday PM peak hour is estimated on the local road network. This level of traffic impact is not likely to have any significant impact on the traffic operations of the surrounding road networks.

6.3 Traffic Flow

With respect to the location of the development, permeability and layout of the surrounding road network, the assumed distribution for additional traffic as a result of the proposed service station is assumed as follows:

- 40% to/ from Eaton Drive north;
- 40% to/ from Eaton Drive south;
- 10% to/from Albatross Crescent north;
- 10% to/from Blue Wren Drive east.

The directional morning, afternoon and total daily trip distribution of the development-generated traffic for the proposed service station development is illustrated in Figure 4.

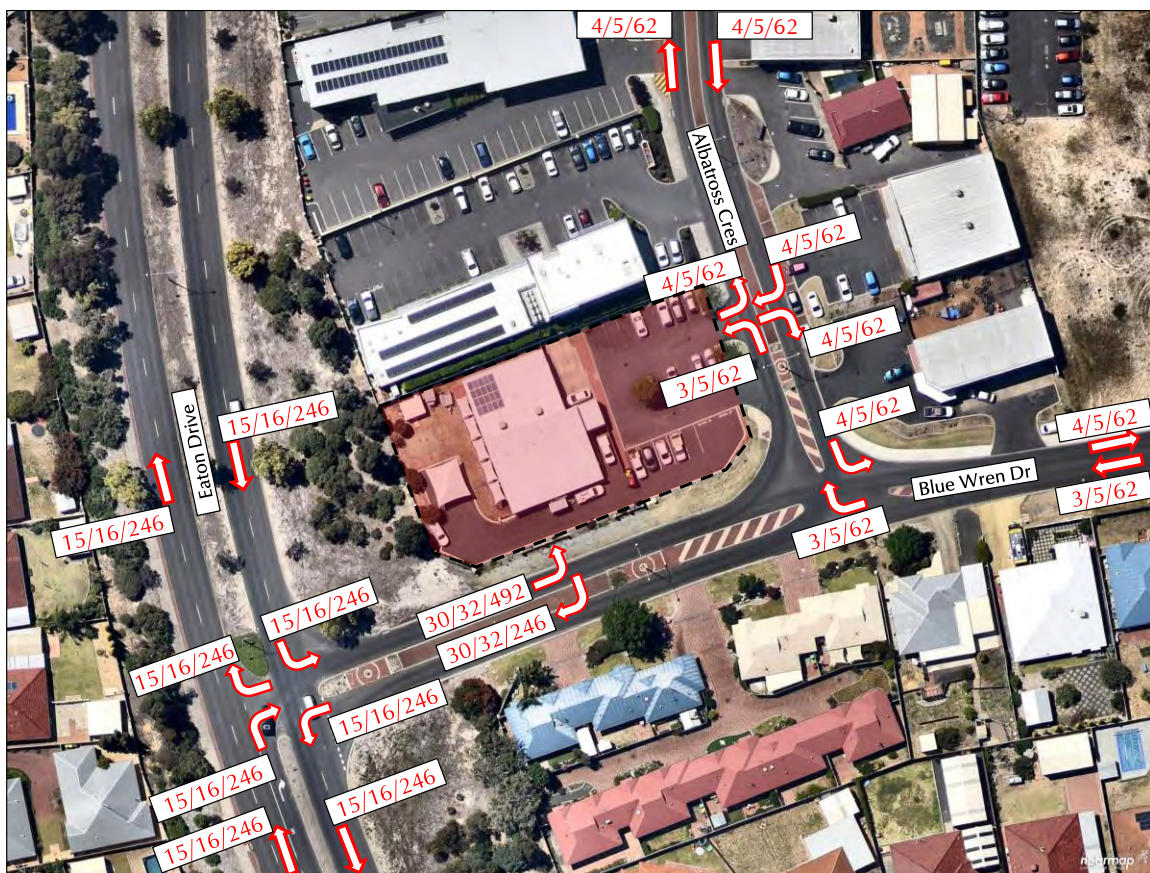


Figure 4: Estimated traffic movements for the proposed development – morning, afternoon peak hours and total daily trips

6.4 Impact on Surrounding Roads

The WAPC Transport Impact Assessment Guidelines (2016) provides guidance on the assessment of traffic impacts:

“As a general guide, an increase in traffic of less than 10 per cent of capacity would not normally be likely to have a material impact on any particular section of road but increases over 10 per cent may. All sections of road with an increase greater than 10 per cent of capacity should therefore be included in the analysis. For ease of assessment, an increase of 100 vehicles per hour for any lane can be considered as equating to around 10 per cent of capacity. Therefore, any section of road where development traffic would increase flows by more than 100 vehicles per hour for any lane should be included in the analysis.”

It is clear that the traffic increase from the proposed development would be significantly less than the critical threshold (100vph per lane). The proposed development will not increase traffic on any lanes on the surrounding road network by more than 100vph, therefore the impact of the development traffic on the surrounding road network will not be significant.

7 Traffic Management on the Frontage Streets

Eaton Drive, west of the subject site, is constructed as a four-lane divided carriageway with landscaped median as illustrated in **Figure 5**. It features pedestrian paths on the western side of the road.

Eaton Drive is classified as a *Distributor A* in the Main Roads WA *Functional Road Hierarchy* and operates under the speed limit of 60km/h in this area.



Figure 5: Northbound view along Eaton Drive

Blue Wren Drive is constructed as a two-lane divided road with 3.5m wide median (flush kerbed median with raised traffic islands where street lights are located) in the vicinity of the subject site as shown in **Figure 6**.

Blue Wren Drive is classified as an *Access Road* in the Main Roads WA *Functional Road Hierarchy* and operates under the default built up area speed limit of 50km/h.

It forms a priority-controlled T-intersection with Eaton Drive to the west.



Figure 6: Eastbound view along Blue Wren Drive

Albatross Crescent, east of the subject site, is constructed as a two-lane divided road with 2.5m wide median (flush kerbed median with raised traffic islands where street lights are located) as shown in **Figure 7**. Pedestrian paths are provided on the western side of the road. Three on street parking bays are provided on the eastern side of the road right opposite to the subject site.

It is classified as an *Access Road* in the Main Roads WA *Functional Road Hierarchy* and operates under the default up area speed limit of 50km/h.

Albatross Crescent form T-intersections with Recreation Drive to the north and Blue Wren Drive to the south.



Figure 7: Northbound view along Albatross Crescent

8 Public Transport Access

Existing public transport services in the vicinity of the site are presented in the current TransBunbury network map as illustrated in **Figure 8**.

TransBunbury route 845 runs along Eaton Drive adjacent to the subject site. This bus route operates between Bunbury and Australind via Bunbury Forum and Eaton Fair Shopping Centre.

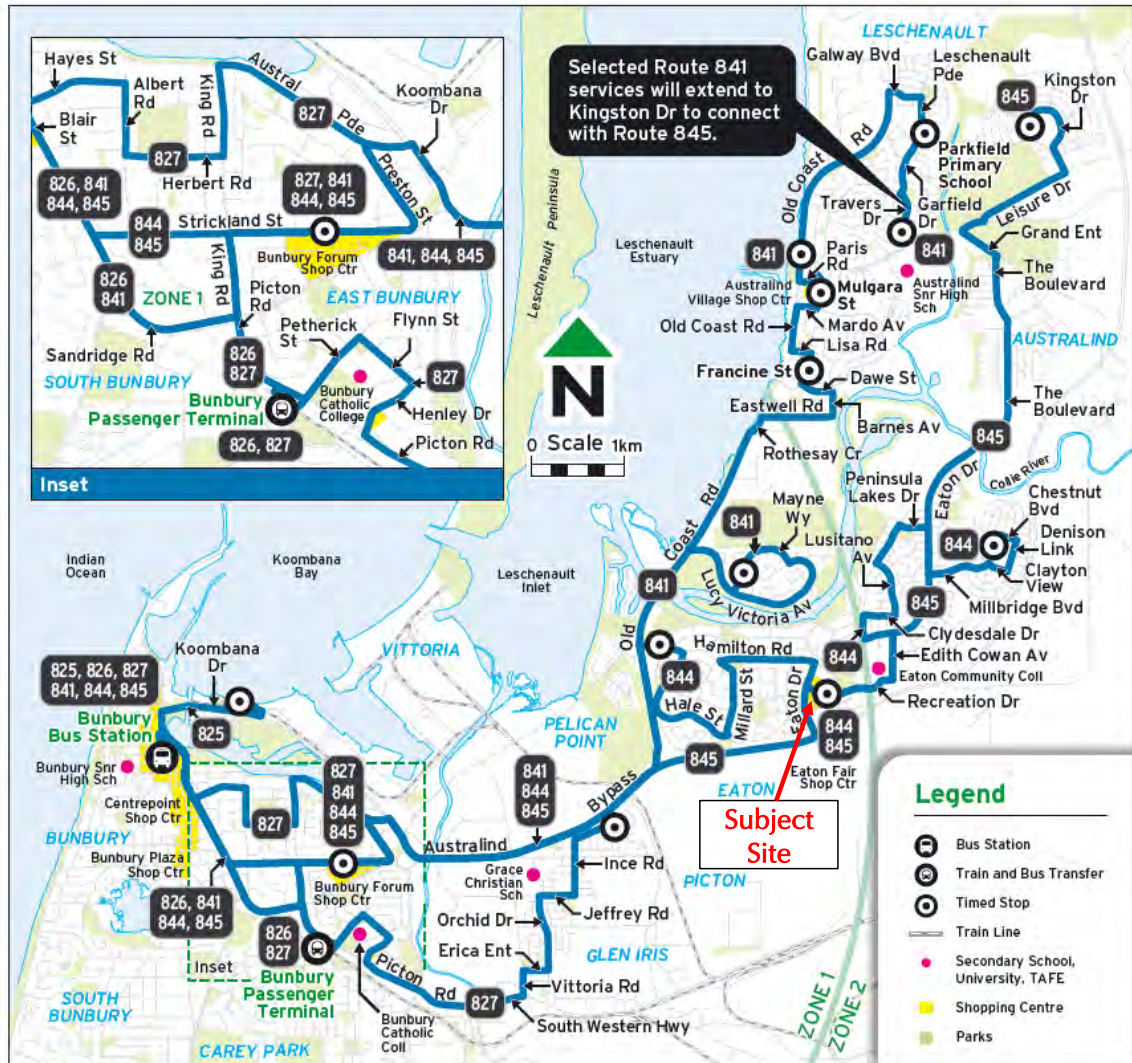


Figure 8: Public transport services (TransBunbury Maps)

9 Pedestrian and Cycle Access

Pedestrian access to the proposed development is directly available via the external footpath network on Albatross Crescent adjacent to the subject site.

A 2.0m wide foot path network is provided on the western side of Eaton Drive. Due to the nature of the proposed development, cyclist access to the site will be minimal.

10 Site Specific Issues

No particular transport site-specific issues have been identified for the proposed service station.



11 Safety Issues

No particular transport safety issues have been identified for the proposed service station.



12 Conclusions

This Transport Impact Statement (TIS) has been prepared by Transcore on behalf of Tomahawk Property with regard to the proposed service station to be located at 15 Albatross Crescent, Eaton in the Shire of Dardanup.

The subject site is presently occupied by South West Community Care (Eaton) and is proposed to be developed as a service station comprising six fuel filling positions (three bowsers) and a convenience store.

Vehicle access/ egress to the subject site is currently available via two existing crossovers on Albatross Crescent. As part of the proposed development the existing northern crossover on Albatross crescent will be modified to provide better access facility to the site whereas the southern crossover on Albatross Crescent will be removed/closed. It is also proposed to provide one full movement crossover on Blue Wren Drive on the southern side of the site.

A total of 12 car parking bays (including one ACROD bay) plus one 'air & water' bay are provided on site. A designated loading bay is also provided at the north-eastern side of the convenience store building.

Turn path analysis undertaken for a 17.25m fuel tanker and 8.8m service vehicle confirms satisfactory entry, egress and circulation within the subject site.

The traffic analysis undertaken in this report shows that the net traffic increase as a result of the proposed development is not significant and as such would not have a significant impact on the surrounding road network.

The subject site has good accessibility by the existing road network, pedestrian paths and enjoys good public transport coverage through existing bus service operating in close proximity of the subject site.

Accordingly, it is concluded that the traffic related issues should not form an impediment to the approval of the proposed service station.

Appendix A

PROPOSED DEVELOPMENT PLAN





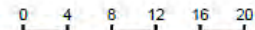
LEGEND

- LANDSCAPING
- TREE

SITE DETAILS:

TOTAL SITE AREA	2112.7 m ²
LANDSCAPED AREA	202.8 m ²
BUILDING AREA	215 m ²
NO. OF CAR BAYS	2
NO. OF SHADE TREES	0

DEVELOPMENT
 NOT FOR CONSTRUCTION

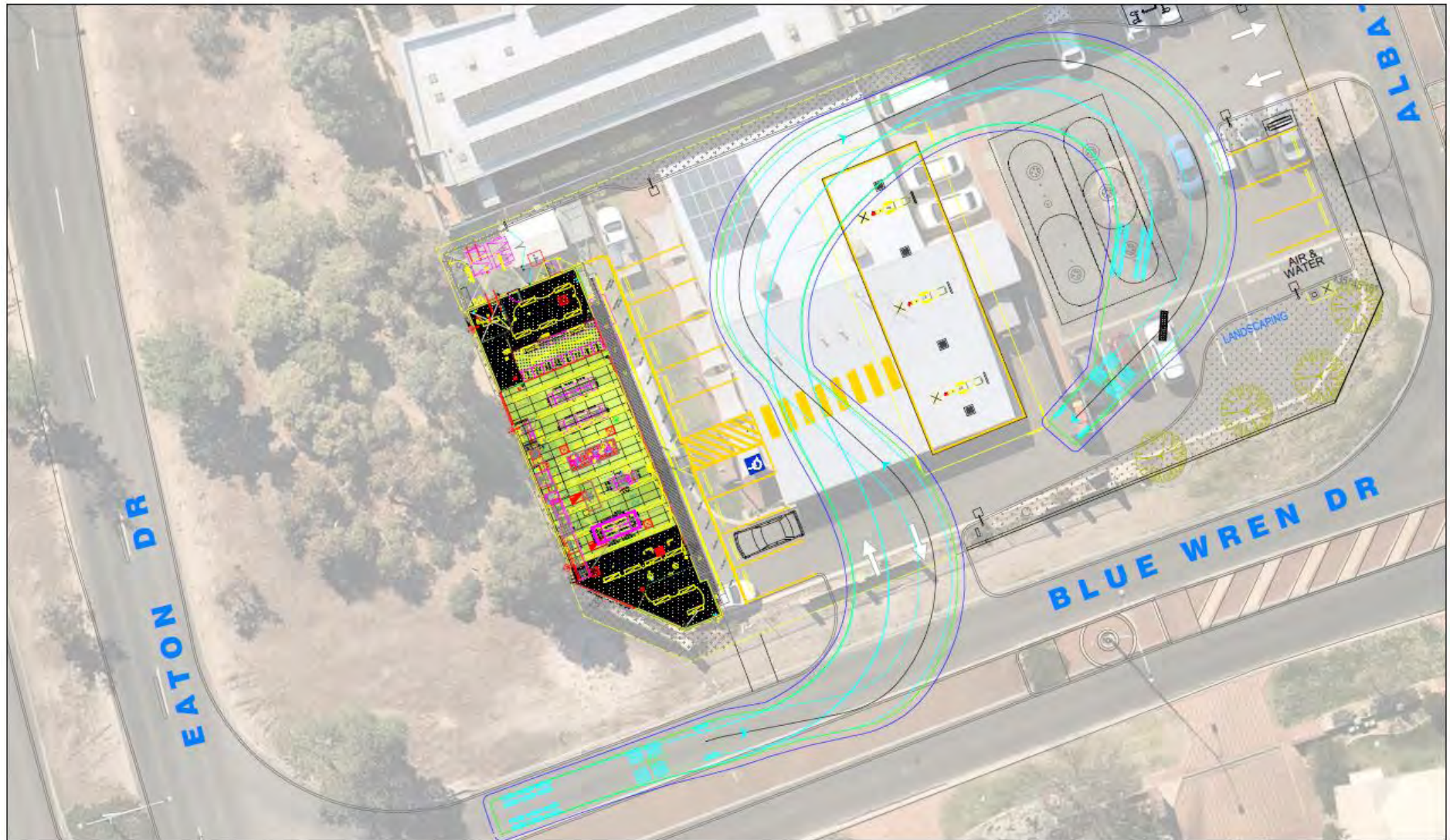


<p>project consultants DESIGN DOCUMENT DELIVER</p> <p><small>COPYRIGHT - THIS DRAWING & DESIGN MUST NOT BE COPIED IN WHOLE OR PART WITHOUT WRITTEN CONSENT FROM PROJECT CONSULTANTS WA PTY LTD</small></p>	<p>PROJECT PROPOSED SERVICE STATION OF ALBATROSS CRESSCENT EATON, WA, 6225</p>	<p>SHEET SITE PLAN PROPOSED</p>	<table border="1"> <thead> <tr> <th>REV.</th> <th>DESCRIPTION</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td>A</td> <td>INITIAL DA PLAN</td> <td>16/11/21</td> </tr> <tr> <td>B</td> <td>REVISED TO 7 ELEVEN COMMENTS</td> <td>18/11/21</td> </tr> <tr> <td>C</td> <td>TOWER SIGN ADDED, OTHER AMENDMENTS</td> <td>23/11/21</td> </tr> <tr> <td>D</td> <td>NOISE WALLS ADDED</td> <td>06/12/21</td> </tr> <tr> <td>E</td> <td>SITE DETAILS ADDED</td> <td>07/12/21</td> </tr> <tr> <td>F</td> <td>FOOTPATH ADDED TO ALBATROSS CRES</td> <td>13/12/21</td> </tr> <tr> <td>G</td> <td>NOISE WALLS ADJUSTED FOR SIGHT LINES</td> <td>16/12/21</td> </tr> </tbody> </table>	REV.	DESCRIPTION	DATE	A	INITIAL DA PLAN	16/11/21	B	REVISED TO 7 ELEVEN COMMENTS	18/11/21	C	TOWER SIGN ADDED, OTHER AMENDMENTS	23/11/21	D	NOISE WALLS ADDED	06/12/21	E	SITE DETAILS ADDED	07/12/21	F	FOOTPATH ADDED TO ALBATROSS CRES	13/12/21	G	NOISE WALLS ADJUSTED FOR SIGHT LINES	16/12/21	<p>A102</p> <p>1:400 G</p>
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F	FOOTPATH ADDED TO ALBATROSS CRES	13/12/21																										
G	NOISE WALLS ADJUSTED FOR SIGHT LINES	16/12/21																										

Appendix B

TURN PATH ANALYSIS





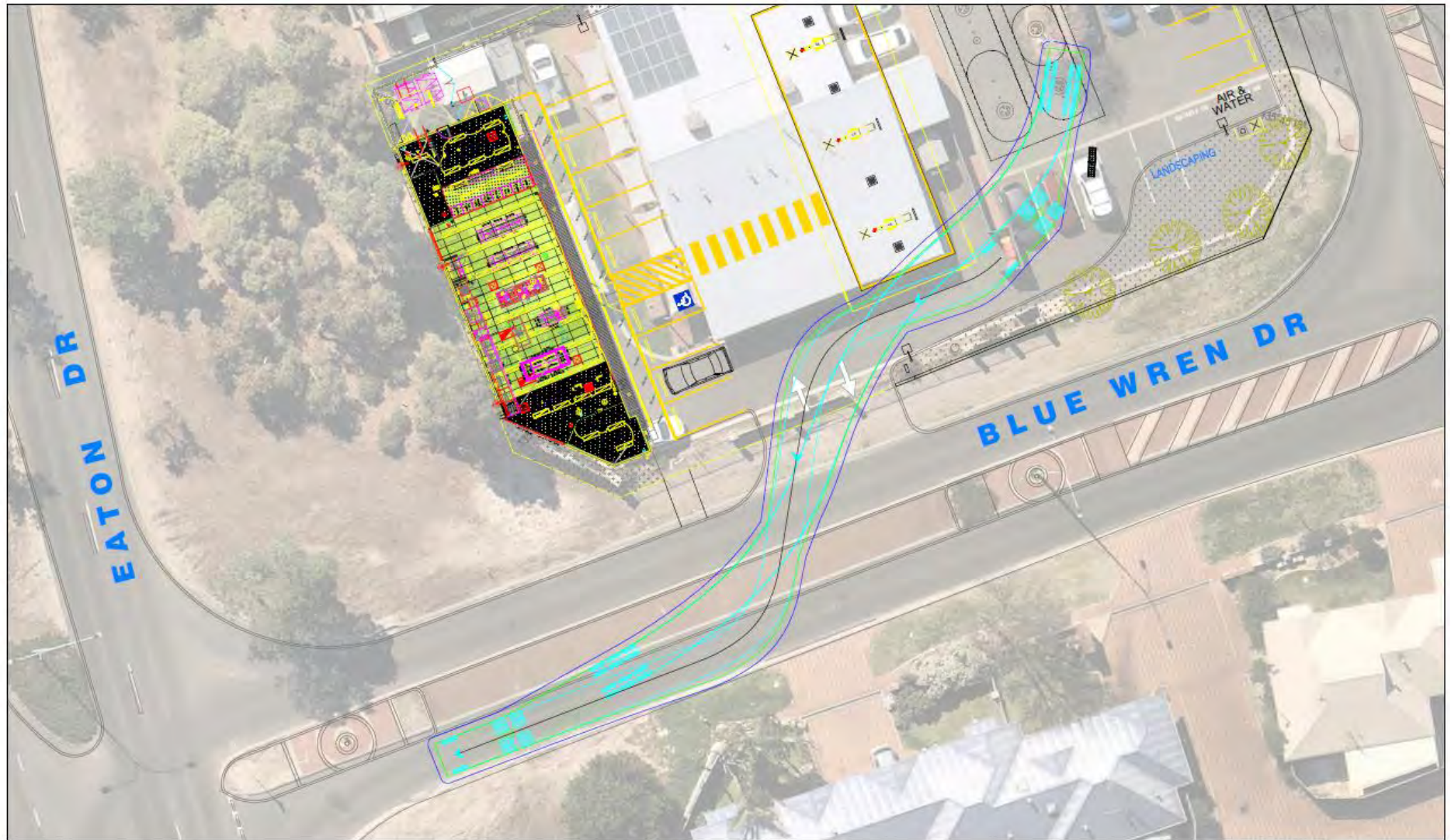
15 Albatross Crescent, Eaton
17.25m Semi-Trailer
Fuel Tanker Entry

LEGEND
Vehicle Body
Wheel Path
500mm Clearance






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20/12/2021
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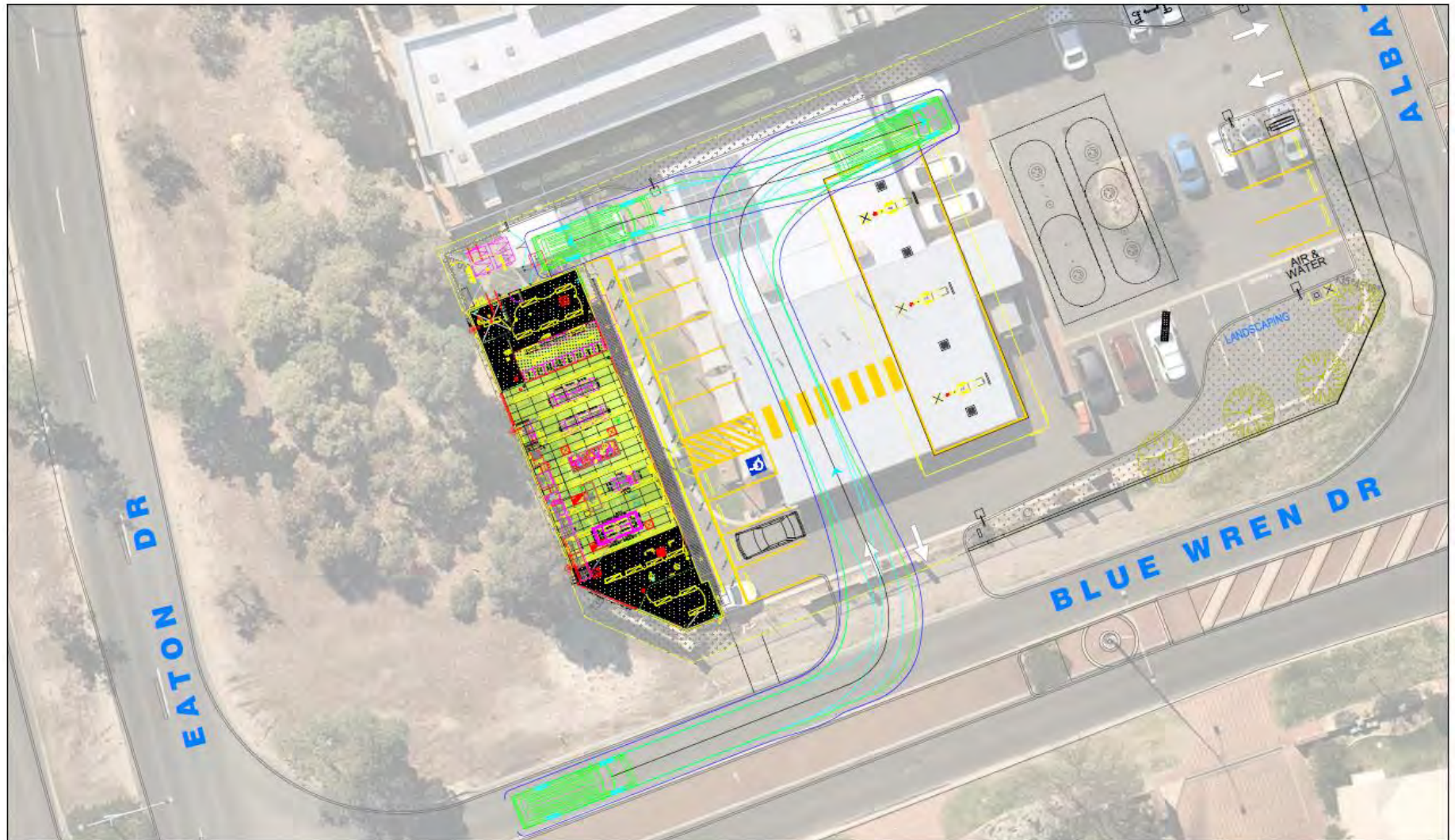


15 Albatross Crescent, Eaton
17.25m Semi-Trailer
Fuel Tanker Exit

LEGEND
Vehicle Body 
Wheel Path 
500mm Clearance 

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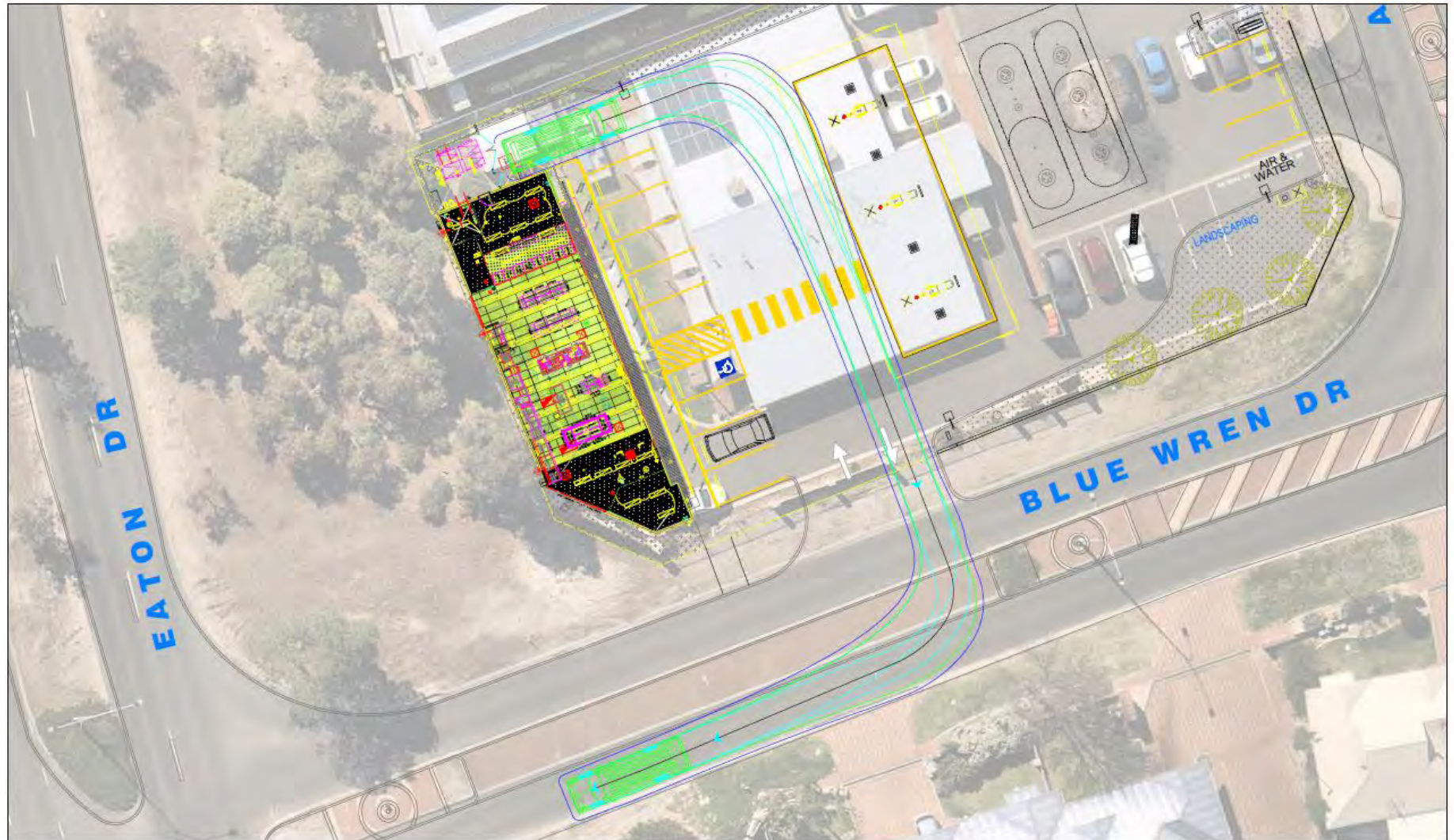
15 Albatross Crescent, Eaton
Austroads 2013: 8.8m Service Vehicle
Service Vehicle Entry

LEGEND
Vehicle Body
Wheel Path
500mm Clearance



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20/12/2021
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15 Albatross Crescent, Eaton
Austroads 2013: 8.8m Service Vehicle
Service Vehicle Exit

LEGEND
Vehicle Body
Wheel Path
500mm Clearance



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ANNEXURE 6
Dangerous Goods Technical Note
(Cadre Engineering)

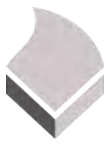


CADRE ENGINEERING

Dangerous Goods Technical Note

Proposed Service Station 7-Eleven Eaton WA

Client:	Tomahawk Property
Site Address:	15 Albatross Crescent, Eaton WA 6232
Document No:	CE21-TP01-01



Revision Control

The following table records the revision history of this document. Contact admin@cadreng.com.au for the latest revision.

Revision No.	Issued By	Issued To	Issue Date
01	Liam Richer	James Wortley	24/11/2021
02	Liam Richer	James Wortley	09/12/2021
03	Liam Richer	James Wortley	09/12/2021

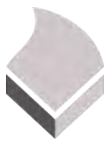
Distribution

Upon completion, this document will be distributed to the persons identified in the table below. These persons will be notified of future revisions of this document.

Representative Name	Company / Organisation	Contact Email
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Executive Summary

This technical note has been prepared to support the Development Application for the proposed Service Station at 15 Albatross Crescent, Eaton WA. This technical note addresses the following:

- The proposed service station development will comply with all the applicable dangerous goods regulatory requirements - refer to Section 1;
- The risk associated with hazardous atmospheres at the site is controlled in accordance with industry best practice and available exposure guidelines - refer to Section 2;
- A background on Stage 1 vapour recovery systems and their use - refer to Section 3.

This technical note was prepared by Liam Richer, a dangerous goods engineer with 10 year's experience in dangerous goods safety and compliance. Liam is accredited by the Department of Mines, Industrial Regulation and Safety (DMIRS) for the endorsement of dangerous goods site licence applications and is a consulting member of the Australian Institute of Dangerous Goods Consultants (AIDGC) and Petroleum Industry Contractors Association (PICA). Liam has extensive experience in the design and assessment of service stations and other fuel storage and handling systems in the Petroleum Industry.

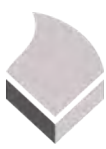
Further enquiries can be directed to:

Liam Richer

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045 777 2738



1. Compliance with Legislation and Best Practice

1.1 Licencing

The proposed 7-Eleven service station in Eaton will comply with the current WA Dangerous Goods Legislation and industry best practice. This will be evidenced by the requirement to hold a Dangerous Goods Site Licence issued by the Department of Mines, Industrial Regulation and Safety (DMIRS). As part of the application process for this licence, the operator will need to demonstrate that the facility can be operated in accordance with the following West Australian legislation:

- Dangerous Goods Safety Act 2004
- Dangerous Goods Safety (Storage and Handling of Non-explosives) Regulations 2007

1.2 Australian Standards

Demonstrating this compliance will require a detailed assessment against the recommendations outlined in the following standards:

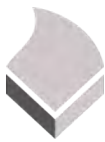
- AS 1940-2017 The storage and handling of flammable and combustible liquids
- AS 4897-2008 The design, installation and operation of underground petroleum storage systems
- AS/NZS 60079.10.1 The classification of hazardous gas atmospheres

At the time of review and endorsement of this application, the final design will be required to meet or exceed all of the applicable requirements of those standards or will not be allowed to store and handle dangerous goods.

1.3 Preliminary Design Review

Although the design is only at the development application stage, Cadre has completed a preliminary review of the design for the Eaton site. From that review, it appears that the facility will be able to comply with the relevant legislative requirements and best practice recommendations. Cadre make the following comments regarding the design:

1. All dispensers are located at least 8.9m from the site boundary ($\leq 4m$ required)
2. No hazardous areas extend over the site boundary as per concept design
3. No fixed ignition sources within the hazardous areas around the dispensers, tank turrets and dip points.
4. Underground bulk storage of flammable liquids only therefore all separation distances required by AS 1940 do not extend over the site boundary.
5. Line of sight appears to be available for all fuel dispensers from the site console.
6. All fuel dispensers are within 30m of the console ($\leq 40m$ required)
7. Spill catchment is shown for the dispensing area with an oily water treatment system specified
8. The location of the fill point and tanker standing area allows for the tanker to be off of public roads and can be driven out of the site without needing to reverse.
9. The tanker standing area also limits the potential for vehicles colliding with the fuel transfer hoses.
10. Sufficient free area is available such that the underground fuel storage tanks could be placed without being subjected to loading from site structures.
11. Tank vents are located $>4m$ from the opening to any building to avoid accumulation of nuisance vapour.

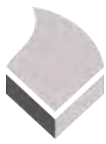


2. Risks Associated with Hazardous Atmospheres

Hazardous Atmospheres are defined by the WA Dangerous Goods Safety (Storage and Handling of Non-Explosives) Regulations 2007. As per the below table the proposed Service Station in Eaton **will adequately control the risk associated with hazardous atmospheres** in accordance with industry best practice.

Hazardous atmosphere is an atmosphere which:	Control Measures
<i>“(a) does not contain a safe concentration of oxygen for breathing; or</i>	All sources of vapour are located outside in adequately ventilated locations (as per AS 1940), which means sufficient air changes are available to ensure sufficient oxygen for breathing.
<i>(b) contains any gas, vapour, mist, fume or dust at a concentration that is at or above the maximum concentration specified for that substance in an exposure standard; or</i>	The limits from the Australian WES will not be exceeded at the site. Refer to Section 2.2 below.
<i>(c) contains dangerous goods of Division 2.1 or vapour from dangerous goods of Class 3 or subsidiary hazard 3 so that the concentration of the dangerous goods is more than 5% of the lower explosive limit for the goods; or</i>	Hazardous Area classification and compliance with the AS/NZS 60079 series will ensure that risks associated with explosive atmospheres are addressed. Refer to Section 2.1 below.
<i>(d) contains any combustible dust at a concentration sufficient to create a hazardous area.”</i>	No credible source of combustible dusts identified.

Further discussion is provided below with regards to hazardous area classification and its relevance to exposure standards.



2.1 Hazardous Area Classification

Hazardous Area Classification is a process undertaken by a suitably qualified person by which areas on the site are designated as potentially containing explosive atmospheres. This process is conducted in Australia in accordance with AS/NZS 60079.10 and has a nationally recognised unit of competency (UEENEEM052A).

A full hazardous area classification will be conducted for the final design of the proposed Service Station in Eaton. Based on the generic hazardous area extents in AS/NZS 60079.10.1 these largest of these hazardous areas are:

- Within 4m of each petrol bowser;
- 1.5m of the outlet of the tank vent; and
- Within 6m of the fuel tanker's release points

The above are conservative distances established by industry best practice meaning that outside the hazardous areas, **the concentration of flammable vapour will not exceed the lower explosive limit (LEL).**

As per the concept drawings for the proposed Eaton service station, **no hazardous areas will extend across the site boundary.**

The **risk associated with explosive atmospheres will be adequately controlled** at the Eaton Service Station through stringent design requirements which remove potential ignition sources from within the identified hazardous areas in accordance with the recommendations of AS 1940 and AS/NZS 60079 series.

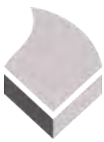
2.2 Hazardous Atmospheres

The risk associated with hazardous atmospheres at the proposed Eaton Service Station is reduced through a combination of **good design practices, site layout and operational measures.**

Compliance with AS 4897 and AS 1940 will ensure that the only likely sources of fuel vapour are from:

- Release from the underground tank vents and filling connections during tanker delivery;
- Release from vehicle dispensing at the forecourt when car fuel tanks are opened and during dispensing; and
- Release from the underground tank vents due to changes in temperature throughout the day.

The specific risk reduction measures for each vapour source are identified in the table below:



Source	Vapour Risk Reduction Measures
Tanker Delivery	<ul style="list-style-type: none">● Fuel deliveries will only be undertaken 2-3 times per week in which the fuel is transferred for 30 - 60 minutes. Shorter release times from this source mean a much lower average concentration of fuel vapour in the site's vicinity● Deliveries occur outside of peak customer periods. This avoids the cumulative effect of large numbers of cars being refuelled while fuel is also transferred to the tanks.● A VR1 Vapour Recovery system will be installed. This system will ensure that 95% of the vapours liberated during the fuel delivery will be returned to the tanker and not released to the environment. Refer to Section 4 for information regarding vapour recovery systems.● Fuel Tankers complying with AS 2809 with trained operators will be used for fuel deliveries. This greatly reduces the likelihood of accidental product and vapour release.
Vehicle Dispensing	<ul style="list-style-type: none">● Dispensing into vehicles is at a much lower flow rate (20-30L/min) by comparison with fuel tanker deliveries (~2000L/min). The lower the flow rate, the less vapour liberated from the liquid.● Car fuel tanks are designed to limit product and vapour spillage during refuelling, including being a relatively tight fit between the nozzle and the fill pipe.● Multiple safety features built into the dispenser nozzle limit the possibility of accidental spillage, including automatic shut off if dropped, used at an elevated angle, or where the container is being overfilled.● The proposed Eaton Service Station will have an attendant monitoring dispensing activities in accordance with AS 1940. The attendant is trained to activate the remote shut down of the dispensers if the dispensing equipment is misused or a spill occurs.● Children under 16 years of age are prevented from operating the fuel dispensing equipment. This reduces their exposure to the products and reduces the likelihood of unsafe use of the dispensing equipment.
Tank Vents	<p>Outside of fuel delivery, release from the tank vents only occurs at low pressures due to changes in temperature. These very low flow rates mean a reduced release of vapour from the tank vent.</p> <p>The vent outlet is 4m above ground level and clear of obstructions allowing greater dispersion of any vapour released.</p> <p>Vehicle dispensing draws liquid out of the tanks, meaning no vapour is likely to be released from the tank vents while a dispenser is operating.</p>



3. Vapour Recovery Systems

Many solutions have been proposed for the control and recovery of fuel vapour from filling and dispensing operations. The most effective of these solutions is Stage 1 Vapour Recovery (VR1). When a fuel tanker delivers product to the underground storage tank, vapour sitting in the tanks is normally forced out of the tank breather vents by the gravity flow of liquid. A VR1 system has a separate vapour return line which is connected back to the fuel tanker. The vacuum created by liquid product leaving the fuel tanker by the fill line then draws the vapour back into the tanker. This captured vapour is then transferred back to the bulk terminal for processing.

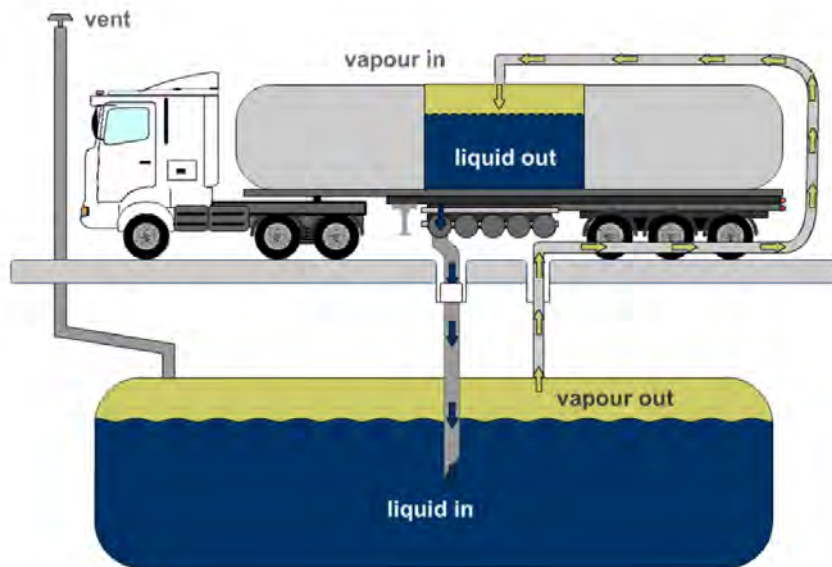


Figure 1 - VR1 System (NSW EPA Standards and Best Practice Guidelines for Vapour Recovery at Petrol Service Stations)

In Western Australia, it is a requirement of the Environmental Protection (Recovery of Vapours from the Transfer of Organic Liquids) Regulations 1995 that a Stage 1 vapour recovery system is installed on all metropolitan underground petrol tanks. VR1 systems have a significant track record of effectiveness and safety. A compliant VR1 system will be installed at the proposed Eaton Service Station.

ANNEXURE 7
Acoustic Assessment
(Reverberate Consulting)


[Appendix ORD: 12.2.5B]



7-Eleven Service Station Albatross Cres, Eaton Environmental Noise Impact

Service Station – Albatross Cres, Eaton
Environmental Noise Impact Report
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Document Information

Project	7-Eleven Service Station – Albatross Cres, Eaton	
Client	Tomahawk Property Pty Ltd	
Report title	Environmental Noise Impact Assessment	
Project Number	P191125	
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Revision Table

Report revision	Date	Comments
0	9 December 2021	Draft for client review
1	10 December 2021	Updated with feedback

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Glossary

A-weighting	A spectrum adaption that is applied to measured noise levels to represent human hearing. A-weighted levels are used as human hearing does not respond equally at all frequencies.
dB	Decibel—a unit of measurement used to express sound level. It is based on a logarithmic scale which means a sound that is 3 dB higher has twice as much energy. We typically perceive a 10 dB increase in sound as a doubling of the loudness of that sound.
Frequency (Hz)	The number of times a vibrating object oscillates (moves back and forth) in one second. Fast movements produce high frequency sound (high pitch/tone), but slow movements mean the frequency (pitch/tone) is low. 1 Hz is equal to 1 cycle per second.
L ₁₀	Noise level exceeded for 10 % of the measurement time. The L ₁₀ level represents the typical upper noise level and is often used to represent traffic or industrial noise emission.
L _{A10}	A-weighted L ₁₀
L _{A10,adj}	Adjusted L _{A10} . Adjustment based on obvious tonality, impulsive or Modulation characteristics in the audible noise at a receiver point. Based on the adjustment methodology in Environmental Protection (Noise) Regulations 1997 Regulation 9
L _{A1,adj}	Adjusted, A-weighted noise level exceeded for 1 % of the measurement time. The L _{A1, adj} level represents mostly short duration, high level sound events.
L _{Amax,adj}	Adjusted, A-weighted maximum instantaneous noise level.

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1.Introduction

Reverberate Consulting has been engaged by Tomahawk Property Pty Ltd to prepare a planning stage Environmental Noise Impact Assessment for the proposed 7-Eleven Service Station development at 15 Albatross Crescent, Eaton.

This report covers the main types of environmental noise emission from the site as part of the study:

- car park and customer vehicle activity
- delivery and supply trucks
- mechanical plant and equipment at the site

The purpose of this report is to present the findings of potential noise emissions from the site.

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2. Site and Surrounds

The proposed Service Station is located on a parcel of land on the corner of Albatross Crescent and Blue Wren Drive, refer to the Site Plan Figure 1 and the Site Layout in Appendix C. It consists of a convenience store building located on the Western end of the site with fuel/bowser operations under a fuel canopy in the middle of the site. The proposed Service Station will operate 24/7.

The site is immediately adjoining commercially zoned land to the North. Residential receivers are located across Blue Wren Drive to the South, and on the far side of Eaton Drive to the West. Commercial land is located on the far side of Albatross Crescent to the East

The dominant noisy activity for the Service Station is caused by truck and car movements. The principal sources generally are patron vehicles as well as the following truck movements:

- Fuel tanker deliveries. Maximum of 1 semitrailer tanker per 24 hours, typically 2 to 3 tankers per week. Operations may occur at night.
- Other smaller truck deliveries of up to 3 rigid truck deliveries per 24 hours. May include night-time deliveries, including refrigerated trucks
- Garbage collection – a maximum of 1 per day, generally between 7am and 7pm

Eaton Drive has historically been measured to carry 16,066 vehicles per day, (Mon – Fri average, DMR August 2021) in the vicinity of the development site. It classifies as a *Major road* under the Environmental Protection (Noise) Regulations 1997. Due to the high number of vehicle movements on this road, the actual movement of vehicles on site is not considered a significant noise source, refer Section 3.1 below.

The nearest noise-sensitive neighbours to the service station are approx. 24m South from its boundary, at #7 Blue Wren Drive. The most-affected two buildings are labelled as #7 East and #7 West in the site plan but are identified collectively as #7 Blue Wren Drive in the tabulated forecasts. #13 and #15 Blue Wren Drive are next-closest dwellings.

The five most-affected residences along Eaton Drive are approx. 70m from the site. They have addresses on Indigo Loop as shown.

There are also other residences around the above mentioned sites but due to their increased buffer distances these other residences are less impacted by noise from the site.

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Figure 1 - Site Plan - Service Station and Surrounds.

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3.Noise Assessment Criteria

3.1 Environmental Protection Act

The Environmental Protection Act (1986) provides for the prevention, control and abatement of pollution and environmental harm. This Act limits environmental noise in Section 3 (3) as follows:

For the purposes of this Act, noise is taken to be unreasonable if –

- (a) it is emitted, or the equipment emitting it is used, in contravention of –*
 - (i) this Act; or*
 - (ii) any subsidiary legislation made under this Act; or*
 - (iii) any requirement or permission (by whatever name called) made or given by or under this Act;*

or

- (b) having regard to the nature and duration of the noise emissions, the frequency of similar noise emissions from the same source (or a source under the control of the same person or persons) and the time of day at which the noise is emitted, the noise unreasonably interferes with the health, welfare, convenience, comfort or amenity of any person; or*

- (c) it is prescribed to be unreasonable for the purposes of this Act.*

Reverberate has used the above legislation to assess the noise impact from the site. More particularly, noises which have a distinct character, and are different to the ambient noise environment are assessed under the subsidiary legislation; the Environmental Protection (Noise) Regulations 1997. Such an assessment has been undertaken for noise sources such as vehicle starting, Truck exhaust brakes, vehicle door closing, mechanical plant / air conditioning, and tyre refilling at the development site.

Other types of noises from the site, such as that generated by vehicles driving, or manoeuvring in the carpark and drive way on site, have not been assessed under the Regulation. Reference is drawn to Section 3 (3) (b) of the Act which requires the assessment to have regard to the nature, duration and time of day of such noise emissions and the frequency of similar noise emissions from the same source.

It is noted that the adjoining road, Eaton Drive already has 16,000+ vehicles per day, so the movement of vehicles on site, per se is not considered characteristically different to that already in the area.

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3.2 Environmental Protection (Noise) Regulations 1997

The Environmental Protection (Noise) Regulations 1997 (EPR) provide limits for acceptable noise from operations and activities. The Regulations specify the maximum permissible noise levels (termed Assigned Levels) at noise sensitive premises, caused by excessive nearby noise, during various times of the day.

The Assigned Levels have been calculated for all properties using the method shown in Appendix B. The resultant Assigned Levels are presented for two representative premises below in Table 1.

Due to the proposed hours of operation, the night-time period is the critical assessment period with the most stringent noise criteria to meet.

Table 1 – Assigned Levels – #7 Blue Wren Drive

Receiving Premises	Time of Day	Assigned Level (dB)		
		L _{A10}	L _{A1}	L _{Amax}
Noise Sensitive Premises – Highly Sensitive	0700 to 1900 hours Monday to Saturday	53	63	73
	0900 to 1900 hours Sunday and public holidays	48	58	73
	1900 to 2200 hours all days	48	58	63
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and public holidays	43	53	63

The Assigned Levels above have been applied against the environmental noise emission from the sources outlined in Table 2 below.

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4.Noise Assessment

4.1 Noise Sources

Noise emission sources and ground contours were used to develop a 3-D SoundPLAN noise model as shown below in Figure 2. This figure shows the locations of the modelled noise sources, noise barriers, and site & surrounding buildings.

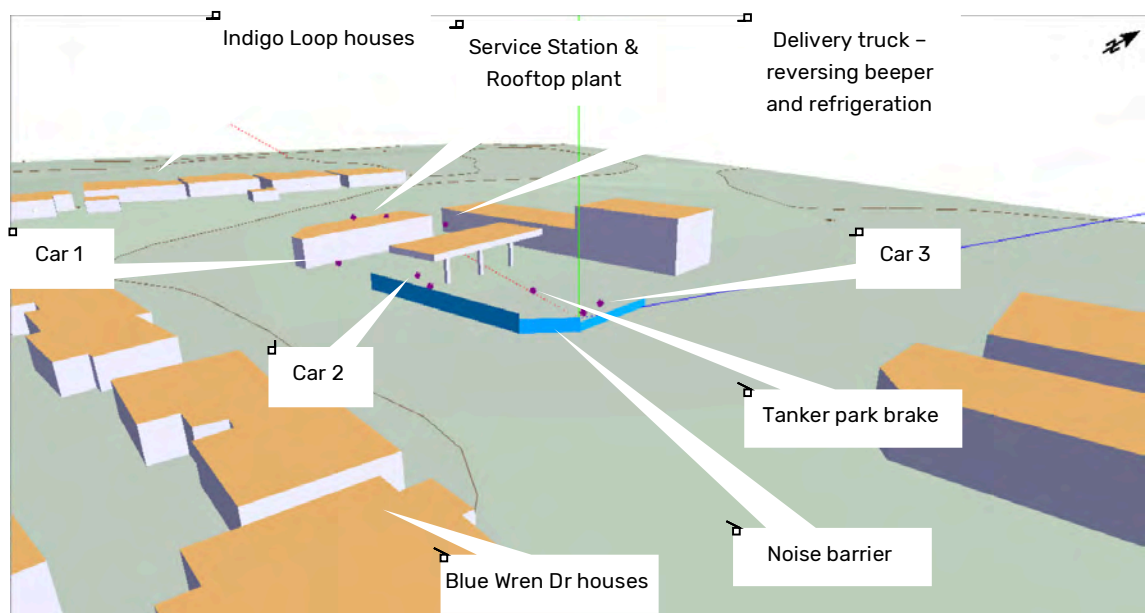


Figure 2 – 3-D SoundPLAN model of Services Station Site and Noise Sources

The noise emission levels for the project noise sources are summarised below in Table 2.

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Table 2 Summary of Noise sources

Summary of Sound Power Levels (dB)			
Noise Source	L _{A10}	L _{A1}	L _{Amax}
Fuel Tanker: park brake	-	-	101
Fuel Tanker: idle	-	96	97
Delivery Truck: refrigeration plant	-	-	96
Delivery Truck: reversing beeper	-	-	95
Passenger Car: engine start	-	85	85
Passenger car: door close	-	-	84
AC/refrigeration combined	80	82	82
Tyre Refill Beeper	-	-	88
Air compressor (silenced)	-	71	72
3 Exhaust fans (total)	71	72	73

4.2 Noise Forecast and Impact

Computer noise modelling was used to forecast the noise impacts to locations around the site. The software used was SoundPLAN Version 8.2, with the ISO9613 algorithms selected. These algorithms have been used as they allow for the influence of wind, atmospheric stability, barriers, building shielding and ground absorption. It is appropriate for the current configuration of noise sources and receiver locations.

The Input data used in modelling includes

- Meteorological Information;
- Topographical data;
- Buildings, barriers, fences, and other features which may shield noise
- Ground Absorption; and
- Source sound levels.

The following parameters were used as needed in modelling for night-time and evening operations i.e. between 7pm and 7am.

- Pasquil Stability Factor F
- Temperature 15 °C
- Wind Speed 3 m/s
- Wind Direction Worst case – i.e. all directions
- Relative Humidity 50%
- Ground Absorption 0.65 in grassed areas
- 0.10 for paved areas such as roads and carparks

Adjustments were applied for the forecast noise reaching receptor locations. Where evident at the receiving locations, the following adjustments were applied:

- +10 dB where the received noise was determined to have impulsive characteristics
- +5 dB where the received noise was determined to have tonal characteristics

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The forecast noise levels at sensitive receivers are summarised in Table 3 to Table 5 below. These forecasts are based on the maximum Sound Power Levels in Table 2 and the successful implementation of the Noise Management Plan in Appendix A.

The forecast noise levels at sensitive receivers are also shown in noise contour plots in Figure 3 to Figure 5

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Table 3 - Forecast L_{A10, adj} night-time noise emission levels

Noise Source	Receiver							
	7 Blue Wren Grnd Fl	13 Blue Wren Grnd Fl	15 Blue Wren Grnd Fl	14 Indigo Grnd Fl	18 Indigo Grnd Fl	20 Indigo Grnd Fl	22 Indigo Grnd Fl	16 Indigo Grnd Fl
Rooftop AC*	39	35	33	37	36	36	35	37
Rooftop Fans*	29	25	24	27	27	27	26	26
Overall*	39	35	33	37	36	36	35	37
Assigned Level	43	43	43	43	43	43	43	43
Compliance	Achieved	Achieved	Achieved	Achieved	Achieved	Achieved	Achieved	Achieved

Note * Tonality adjustment applied

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Table 4 – Forecast $L_{A1,adj}$ Night-time Noise Emission Levels

Noise Source	Receiver							
	7 Blue Wren Grnd Fl	13 Blue Wren Grnd Fl	15 Blue Wren Grnd Fl	14 Indigo Grnd Fl	18 Indigo Grnd Fl	20 Indigo Grnd Fl	22 Indigo Grnd Fl	16 Indigo Grnd Fl
tyre air compressor*	35	27	26	31	29	33	32	32
Rooftop fans*	31	27	26	29	29	29	28	28
Rooftop AC/Refrig*	41	37	35	39	38	38	37	39
Assigned Level	53	53	53	53	53	53	53	53
Compliance	Achieved	Achieved	Achieved	Achieved	Achieved	Achieved	Achieved	Achieved

Note * Tonality adjustment applied

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Table 5 – Forecast $L_{Amax,adj}$ Night-time Noise Emission Levels

Noise Source	Receiver							
	7 Blue Wren	13 Blue Wren	15 Blue Wren	14 Indigo	18 Indigo	20 Indigo	22 Indigo	16 Indigo
	Grnd Fl	Grnd Fl	Grnd Fl	Grnd Fl	Grnd Fl	Grnd Fl	Grnd Fl	Grnd Fl
air conditioners*	41	37	35	39	38	38	37	39
Car 1 door**	54	48	47	31	24	28	24	35
Car 2 door**	53	43	44	35	36	33	29	29
Car 3 door**	46	46	43	37	31	37	34	36
Delivery Refrig	53	44	42	45	45	49	49	42
delivery reversing beeper*	58	47	46	48	47	50	50	40
fans*	31	27	26	29	29	29	28	28
park brake**	63	62	57	50	46	49	58	46
Tanker engine start	53	50	49	49	35	38	38	36
Tyre refill beeper*	43	39	41	42	29	29	41	28
Assigned Level	63	63	63	63	63	63	63	63
Compliance	Achieved	Achieved	Achieved	Achieved	Achieved	Achieved	Achieved	Achieved

Note * Tonality adjustment applied

** Impulsive adjustment applied

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Figure 3 - Forecast LA₁₀ night-time noise emission with sources from Table 4 (1.4m above ground level)

(L_{A10} criterion at residences = 43 dB)

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Figure 4 - Forecast LA1 night-time noise emission with sources from Table 5 (1.4m above ground level)

(LA1 criterion at residences = 53 dB)

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Figure 5 - Forecast Night-time L_{max} noise emission - (1.4m above ground level)

(L_{max} criterion at residences = 63 dB)

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5. Discussion

The results in Section 4.2 show that the overall noise emission levels comply with the Assigned Levels as developed from the Environmental Protection (Noise) Regulations 1997.

This conclusion can be drawn using the conservative assumptions that

- all mechanical plant, and other plant as identified, is simultaneously operating, and at full noise emission
- Noise Regulation adjustments are needed for all items as proposed

We consider that these assumptions are conservative, and that it unlikely that these will actually occur. Where the assumptions do not hold, the overall noise emission from the site can reduce to levels below that shown in Table 3 to Table 5.

It is noted that the noise control measures recommended in this report will produce sufficient noise control to meet the Environmental Protection (Noise) Regulations 1997 requirements, for the noise sources as outlined. Where it is proposed to install plant or equipment with different noise emission to that identified in this report, we recommend that a detailed noise assessment is conducted at that stage.

Based on the noise sources, arrangement, and the conservative assumptions outlined in this report, the overall noise emission from the site is considered acceptable throughout the day.

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6. Conclusions

An assessment of environmental noise emission from the proposed Service Station development has been undertaken.

The forecast noise emission levels have been presented. The recommended treatments to control noise emissions are outlined in the Noise Management Plan (Appendix A) and these treatments have been shown to control environmental noise emission from the site so that compliance is achieved with the Environmental Protection Act (1986) and Environmental Protection (Noise) Regulations 1997.

On this basis the noise emissions from the site are considered acceptable and Unreasonable Noise, as defined in the Act is not anticipated from site.

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Appendix A: Noise Management Plan

The elements outlined below are recommended as part of a comprehensive Noise Management Plan. They are recommended for compliance with the Environmental Protection Act 1986 and its subsidiary legislation; the Environmental Protection (Noise) Regulations 1997.

Noise Source or Activity	Requirement
General Deliveries	<ul style="list-style-type: none"> Deliveries to convenience store permitted 24/7 Vehicle manoeuvring on site to be at a maximum of 5-8 km/h, and with low engine revs. The total noise emission from vehicles, including refrigeration units not to exceed a Sound Power Level of 96 dB Reversing of trucks to be minimised to avoid the unnecessary activation of the reversing beeper. "Broad band", or "white-noise" reversing beepers are preferred but not essential for all Delivery trucks at the site
Refuse Collection	<ul style="list-style-type: none"> Refuse collection is to be carried out in the quietest reasonable and practicable manner; Equipment used for refuse collection is the quietest reasonably available Collection to occur between 7 am and 7 pm Mon-Saturday, unless the contractor has a Noise Management Plan approved by Council.
Fuel Tanker Deliveries	<ul style="list-style-type: none"> Deliveries to convenience store permitted 24/7 Vehicle manoeuvring on site to be at a maximum of 5-8 km/h, and with low engine revs. "Broad band", or "white-noise" reversing beepers are preferred but not essential for all tanker trucks Reversing of trucks to be discouraged to avoid the unnecessary activation of the reversing beeper.
Barriers	<ul style="list-style-type: none"> 2.5m high boundary barrier to Blue Wren Drive, as shown in Appendix C 1.2m boundary barrier to Albatross Cres, as shown in Appendix C All barriers to be gap-free. Barriers less than 1.9m barrier can be a minimum 0.42mm BMT colourbond or acoustically equivalent material. The 2.5 m high barrier to be min 90mm masonry or acoustically equivalent
Grilles, Storm water grates & other metal covers	<ul style="list-style-type: none"> To be installed so as to be tight fitting. Where this cannot be achieved, hard rubber or other durable materials are to be used for cushioning such grates/covers

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Tyre refill alarm	<ul style="list-style-type: none">• Beepers to be located behind the boundary barrier
Outdoor Speakers	<ul style="list-style-type: none">• No music to be played through any speaker on site. The use of the speaker is to be limited to emergency messaging and patron management only
Outdoor Building Services plant	<ul style="list-style-type: none">• Refrigeration plant in this preliminary design assessment does not require localised shielding. The final acoustical design to be assessed during the Detailed Design phase of the project, after specific equipment has been selected, and location been finalised.
General	<ul style="list-style-type: none">• Noise Emission from all sources not to exceed the levels in Table 2 of this report
Air Compressor	<ul style="list-style-type: none">• air compressor to be placed in the loading dock, shielded from the nearest residences to the South.

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Appendix B: Determination of Assigned Level

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The Environmental Protection (Noise) Regulations 1997 (EPR) provide limits for acceptable noise from operations generating excessive noise. The Regulations specify the maximum permissible noise levels (termed assigned levels) at noise sensitive premises, caused by surrounding noises, during various times of the day. Time of day affects the assigned levels for noise-sensitive premises, as follows –

- Lowest levels at night (10 pm to 7 am any day, or to 9 am Sundays and Public Holidays);
- Higher levels during the evenings (7 pm to 10 pm) and on Sundays and Public Holidays (9 am to 10 pm); and
- Highest levels during the day (7 am to 7 pm Monday to Saturday).

The baseline assigned levels from the Regulations are shown below in Table 6.

Table 6 – Baseline Assigned Levels

Receiving Premises	Time of Day	Assigned Level (dB)		
		L _{A10}	L _{A1}	L _{Amax}
Noise Sensitive Premises - Highly Sensitive	0700 to 1900 hours Monday to Saturday	45+IF	55+IF	65+IF
	0900 to 1900 hours Sunday and public holidays	40+IF	50+IF	65+IF
	1900 to 2200 hours all days	40+IF	50+IF	55+IF
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and public holidays	35+IF	45+IF	55+IF
Noise Sensitive Premises – any area other than highly sensitive area	All hours	60	75	80
Commercial	All hours	60	75	80
Industrial	All hours	65	80	90

The Assigned Levels above are then increased using an Influencing Factor (IF) as defined in the Regulations. The Influencing Factor is greater than zero where there are significant areas of land uses, within 100 m and 450 m radii of the receptor, including:

- industrial land use zonings;
- commercial zonings; and
- the presence of roads carrying significant traffic.

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The Influencing Factor IF has been calculated for the applicable noise sensitive receptors in the current study. The percentage of industrial and commercial land within the prescribed circles centred on the noise sensitive premises, and the presence of roads with more than 6000 vehicles per day have been assessed for the properties.

Example Influencing Factor calculations are shown below. These factors have been added to the baseline Assigned Levels to produce the Assigned Levels in Section 3.2 above

Property = #7 Blue Wren Drive

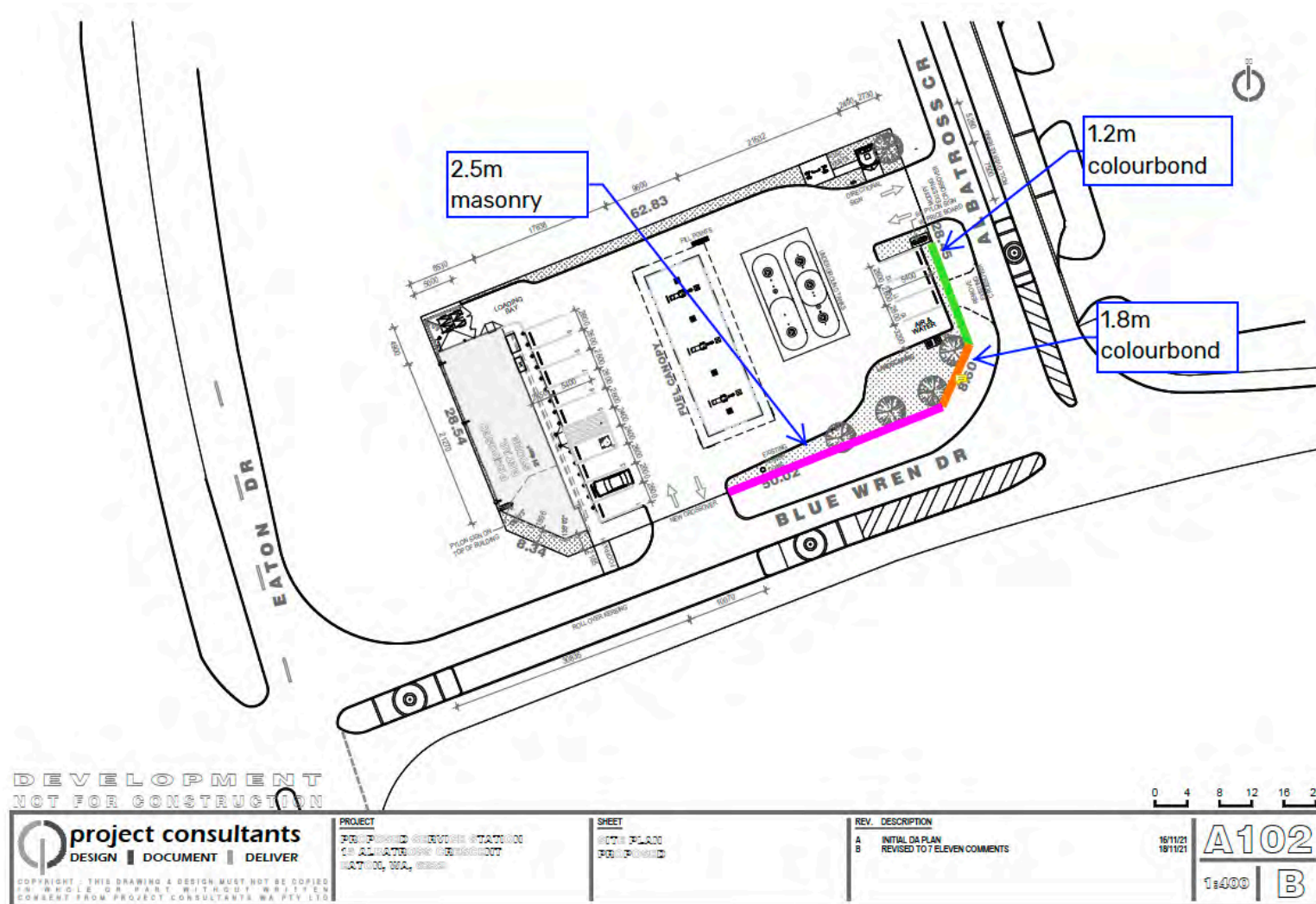
Type of Land	450m Radius	100m radius	Total	
Industrial Land	0%	0%	-	dB
Commercial Land	12%	22%	1.7	dB
Transportation Factor			6	dB
TOTAL Influencing Factor			8	dB

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Appendix C: Site Layout and Noise Barrier

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PAINT FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
P1	DULUX WASH & WEAR 101 LOW SHEEN ACRYLIC COLOUR: VIVID WHITE PW1 H9 (IF SPECIFIED TO BE PAINTED)	INTERIOR CEILING
P2	DULUX WASH & WEAR 101 SEMI GLOSS ACRYLIC COLOUR: WHITE WATSONIA PW2D4	INTERIOR WALLS
P3	DULUX SUPER ENAMEL HIGH GLOSS COLOUR: COLORBOND SHALE GREY CB16	INTERIOR DOORS & ARCHITRAVES
P4	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: VIVID WHITE PW1H9	7-ELEVEN VENT PIPES, PETROL CANOPY COLUMNS & DOWNPIPES
P5	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: 7-ELEVEN GONDOLA GREY (DULUX REF A2225) TINT FORMULA: EE=11, M=86.	BUILDING - EXTERNALLY REFER A015 FOR LOCATION. EXTERIOR DOOR, DOWNPIPES GUTTER, SELECTED EXTERNAL CONCRETE PANEL WALLS.
P6	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: BLACK	PETROL CANOPY COLUMNS AND BOLLARDS 300mm ABOVE GROUND LEVEL
P7	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: 7-ELEVEN TOWER SILVER (DULUX REF A0290)	EXTERNAL - BUILDING TOWER (IF APPLICABLE)
P8	DULUX ROADMASTER A1 - NON SLIP COLOUR: LF YELLOW ADDITIVE: GLASS BEAD	CAR PARK LINES -FACE AND TOP OF KERB IMMEDIATELY IN FRONT OF STORE & AS NOTED ON SITE PLAN
P9	DULUX WASH & WEAR 101 BARRIER. LOW SHEEN-BLACK-BASE-EXTRA BRIGHT 52L87663	FOTG ZONE INTERIOR WALL & INTERNAL WALL TO SLURPEE ZONE
P10	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: GOLDEN BANNER P15.H9	BOLLARDS
P11	DULUX WASH & WEAR LOW SHEEN ACRYLIC COLOUR: HOT LIPS S05H9	FRONT AND UNDERSIDE OF FOTG BULKHEAD

CONCRETE FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
S1	AVISTA CONC. SEALER PRIMER AVISTA CONC. SEALER EXTENDED WEAR - CLEAR AVISTA CONC. SEALER SLIP REDUCING ADDITIVE	BUILDING APRON - RAISED CONC. FOOTPATH INCLUDING RAMPS AROUND BUILDING

TILE FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
T1	JOHNSON 150x150mm JOHNSON ULTRA WHITE GLOSS CERAMIC WALL TILES WITH WHITE GROUT PRODUCT CODE: 614580	BEHIND HAND BASINS, CLEANER'S SINK, DBL BOWL SINK, PREP AREA

METAL FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
C1	FOLDED COLORBOND METAL FLASHING COLOR: COLORBOND 'SURFMIST'	ROOF FLASHING/ PARPAPET CAPPING
C2	FOLDED COLORBOND METAL FLASHING / METAL SHEET WALL CLADDING COLOR: COLORBOND 'IRONSTONE'	ROOF FLASHING/ PARPAPET CAPPING/ BIN ROOM

LAMINATE FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
LA1	GLOSS WHITE ALASKA GLIS LAMINATE SUPPLIER: HALIFAX VOGEL GROUP CODE: 018876	SERVICE COUNTER JOINERY
LA2	BLACK COMPACT LAMINATE VELVET FINISH SUPPLIER: MAICA	SERVICE COUNTER JOINERY
LA3	PREMIUM BLACK 60 LAMINATE SUPPLIER: HALIFAX VOGEL GROUP	COFFEE ISLAND JOINERY

GLASS FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
SP	DULUX BRIGHT DELIGHT TOUGHENED GLASS SPLASHBACK CODE: S08G9	SERVICE COUNTER

STAINLESS STEEL FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
M01	1mm THICK STAINLESS STEEL	SERVICE COUNTER JOINERY

STONE FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
ST1	PRODUCT: CAESAR STONE 20mm THICK SUPPLIER: NATURAL STONE QUARTZ SURFACES COLOUR: NIGHT SKY 6100	COFFEE ISLAND

VINYL FINISHES SCHEDULE		
CODE	FINISH SPECIFICATION	LOCATION
V1	ARMSTRONG EXCELON 3.2 x 305 x 305mm VINYL FLOOR TILES COLOUR: IMPERIAL TEXTURE BLUE GREY 7A519033	SALES AREA
V2	POLYFLOR XL PUR 2mm VINYL SHEET COLOUR: FLINT 3720	SLURPEE ZONE
V3	ARMSTRONG - SAFEGUARD COLOUR: LIGHT GREY SLIP RESISTANCE: R10 / P3	COOLROOM / OFFICE / STORE ROOM
V4	ARMSTRONG VINYL SHEET COLOUR: BLACK	SALES AREA COVING
V5	POLYFLOR POLYCLAD PLUS PU. 2mm THK WALL VINYL SHEET. COLOUR: NIMBUS GREY 2710	BOH WALL CLADDING

GENERAL NOTE:

1. ALL ALTERNATIVE PRODUCTS TO BE APPROVED BY 7-ELEVEN

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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
SCHEDULE
FINISHES (SERVICE STATION)

REV. DESCRIPTION
A INITIAL DA PLAN
B REVISED TO 7 ELEVEN COMMENTS
C TOWER SIGN ADDED, OTHER AMENDMENTS

16/11/21
18/11/21
23/11/21

A002
NTS | C



LEGEND

LANDSCAPING

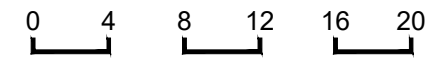
NOTE: LANDSCAPING AS PER VEGETATION PLAN AND LANDSCAPE DESIGNER DRAWINGS

SITE DETAILS:

TOTAL SITE AREA:	2112.7 m ²
LANDSCAPED AREA:	292.6 m ²
BUILDING AREA:	215 m ²
No. OF CAR BAYS:	12
No. OF SHADE TREES:	5



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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
SITE PLAN
PROPOSED

REV.	DESCRIPTION
D	NOISE WALLS ADDED
E	SITE DETAILS ADDED
F	FOOTPATH ADDED TO ALBATROSS CRES
G	NOISE WALLS ADJUSTED FOR SIGHT LINES
H	MINOR MODIFICATIONS
J	SURVEY ADDED, COUNCIL PATH ADDED
K	MODIFIED VEGETATION PLAN
L	NOTES AMENDED

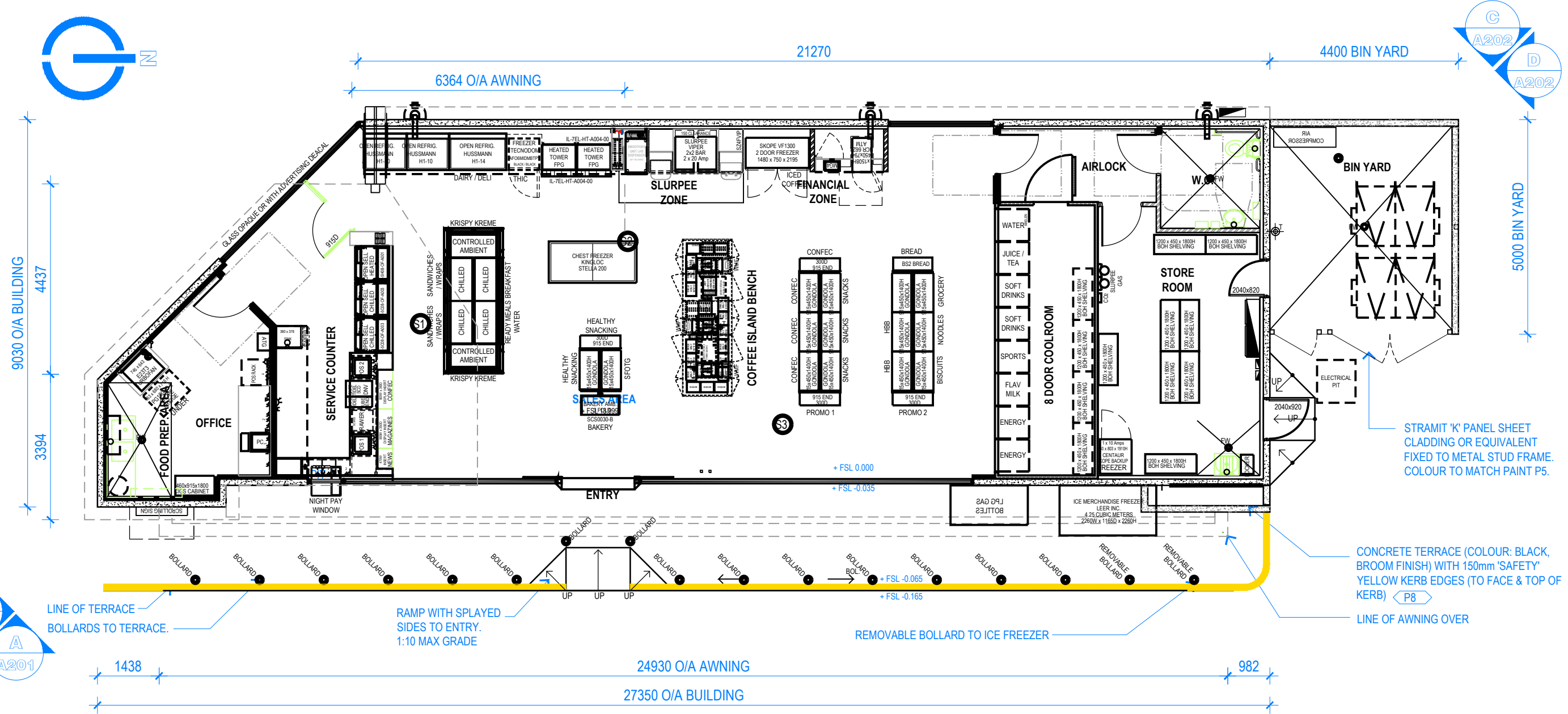
06/12/21
07/12/21
13/12/21
16/12/21
28/01/22
02/03/22
02/03/22
04/03/22

A102
1:400 | L

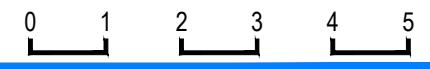
NOTE
ALL DIMENSIONS GIVEN ARE TO STRUCTURAL WALLS & FRAMING, UNLESS NOTED OTHERWISE

WALL TYPE LEGEND

- 150mm THICK PRECAST CONCRETE WALL PANELS). PLASTERBOARD LINING INTERNALLY WALL INSULATION TO MEET PART 'J' OF BCA REQUIREMENTS.
- 90mm STUD WALL WITH 13mm PLASTERBOARD LINING INTERNALLY AND 9mm CFC SHEET LINING EXTERNALLY, WALL CONSTRUCTION TO MEET PART 'J' OF BCA REQUIREMENTS, REFER NOTES ON PLAN.
- WALL CONSTRUCTION TO RELEVANT AUTHORITY'S REQUIREMENTS TO ACHIEVE 'CLASS 2' FOOD REGISTRATION COMPLIANCE.
- 75mm THICK INSULATED COOLROOM PANELS



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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
PLAN
BUILDING

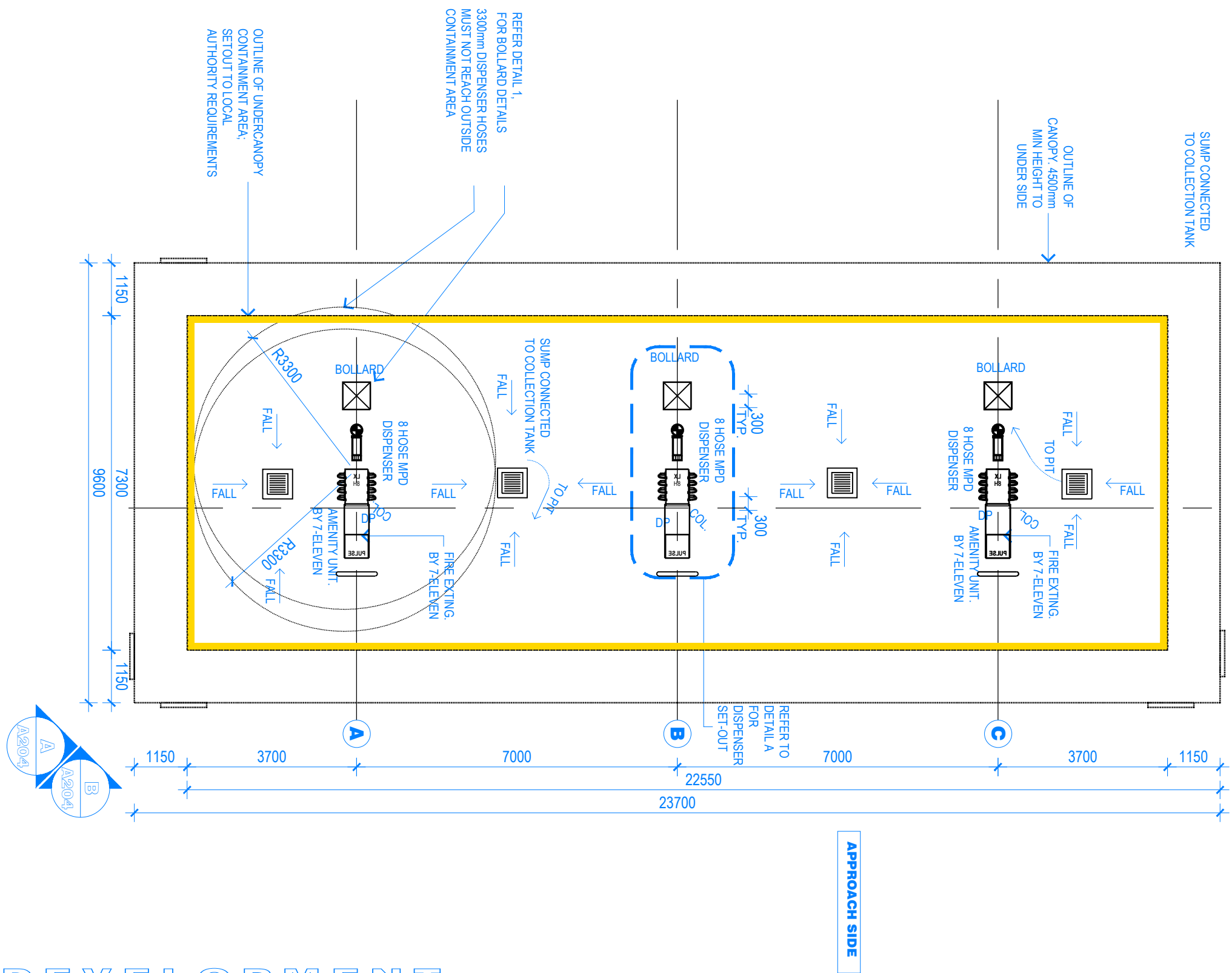
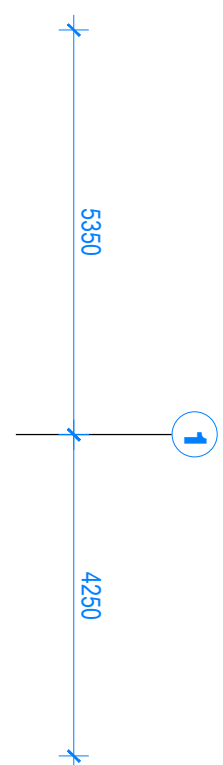
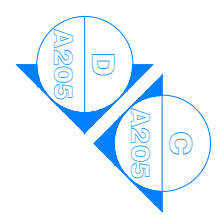
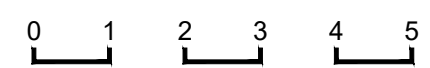
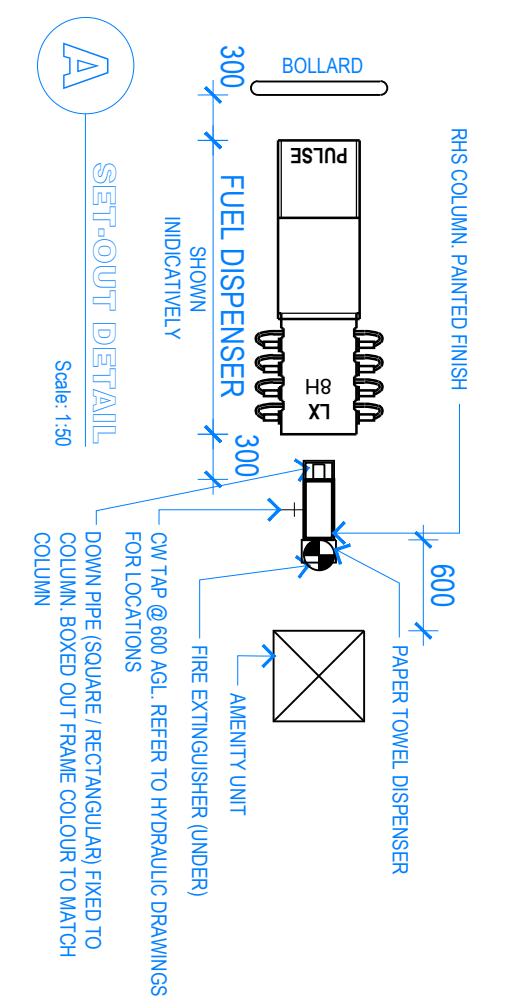
REV.	DESCRIPTION
A	INITIAL DA PLAN
B	REVISED TO 7 ELEVEN COMMENTS
C	TOWER SIGN ADDED, OTHER AMENDMENTS

16/11/21
18/11/21
23/11/21

A103
1:100 | C



- GENERAL NOTES**
1. THIS GENERIC DRAWING IS FOR INFORMATION ONLY AND NOT TO BE USED FOR CONSTRUCTION
 2. CANOPY DESIGN MUST BE IN COMPLIANCE WITH AS1940 AND ALL OTHER RELEVANT STATUTORY AND LOCAL AUTHORITY'S REQUIREMENTS
 3. BUNDLED AREA TO BE DESIGN TO SUIT SITE CONDITIONS



APPROACH SIDE

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PROJECT
PROPOSED SERVICE STATION
147 - 149 GRAND ENTRANCE
AUSTRALIND, WA, 6233

SHEET
PLAN
CANOPY
CAR

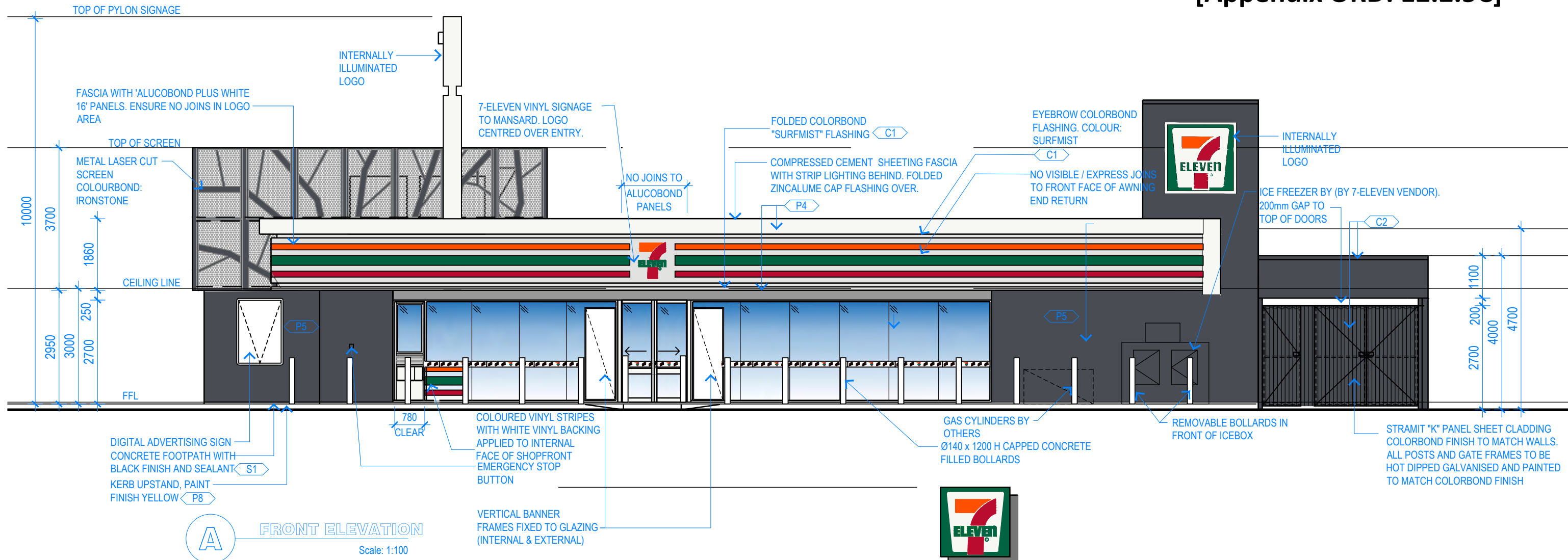
REV.	DESCRIPTION
A	INITIAL DA PLAN
B	REVISED TO 7 ELEVEN COMMENTS
C	TOWER SIGN ADDED, OTHER AMENDMENTS

16/11/21
18/11/21
23/11/21

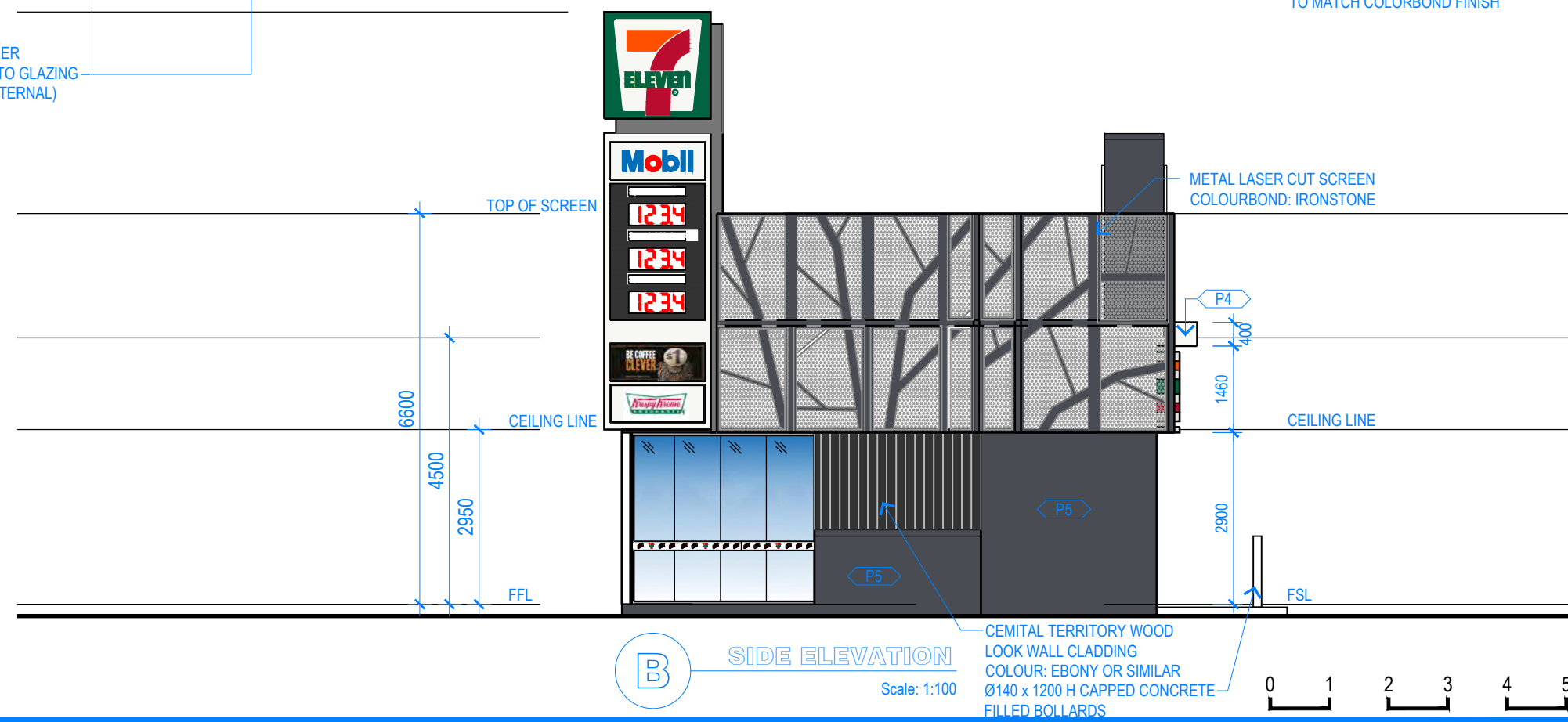
A104

1:100

C



A FRONT ELEVATION
Scale: 1:100



B SIDE ELEVATION
Scale: 1:100

- GENERIC NOTES**
1. ALL WORKS TO BE COMPLETED BY LESSOR UNLESS NOTED
 2. REFER TO DRAWING G002 FOR FINISHES SCHEDULE

DEVELOPMENT
NOT FOR CONSTRUCTION

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DESIGN | DOCUMENT | DELIVER

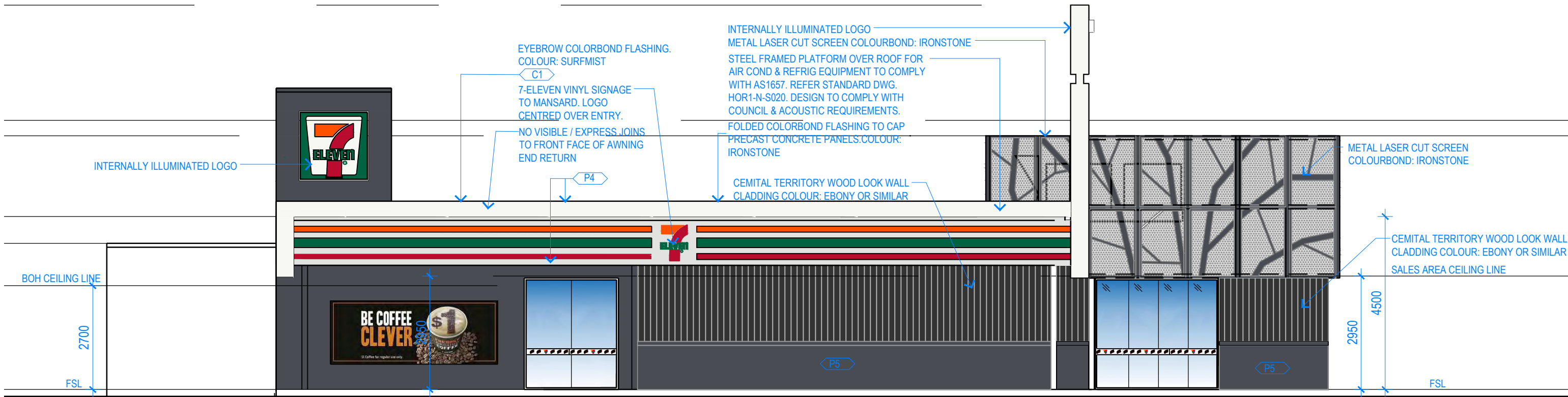
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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
ELEVATION
BUILDING
SHEET 1

REV.	DESCRIPTION	DATE
A	INITIAL DA PLAN	16/11/21
B	REVISED TO 7 ELEVEN COMMENTS	18/11/21
C	TOWER SIGN ADDED, OTHER AMENDMENTS	23/11/21

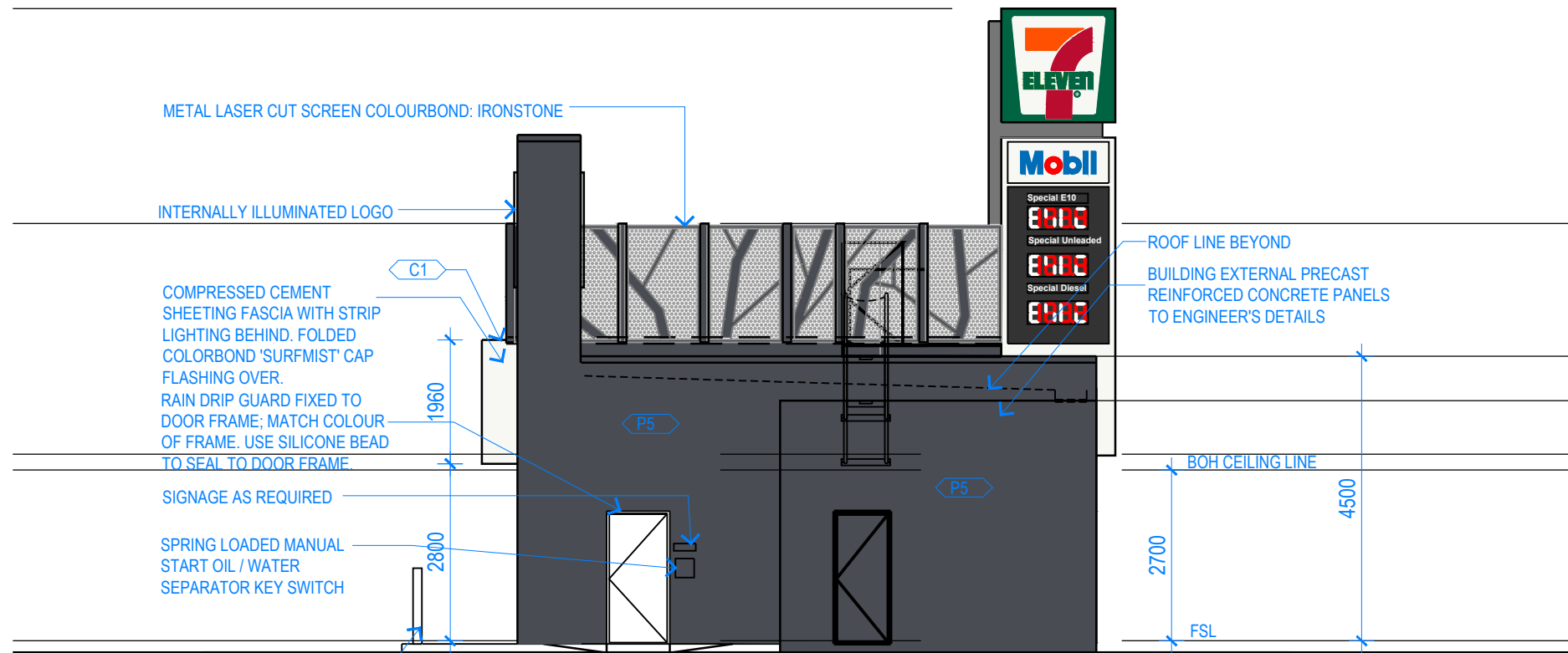
A201
1:100 | **C**



C REAR ELEVATION
Scale: 1:100

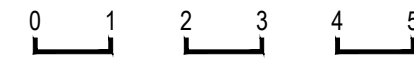
GENERAL NOTES

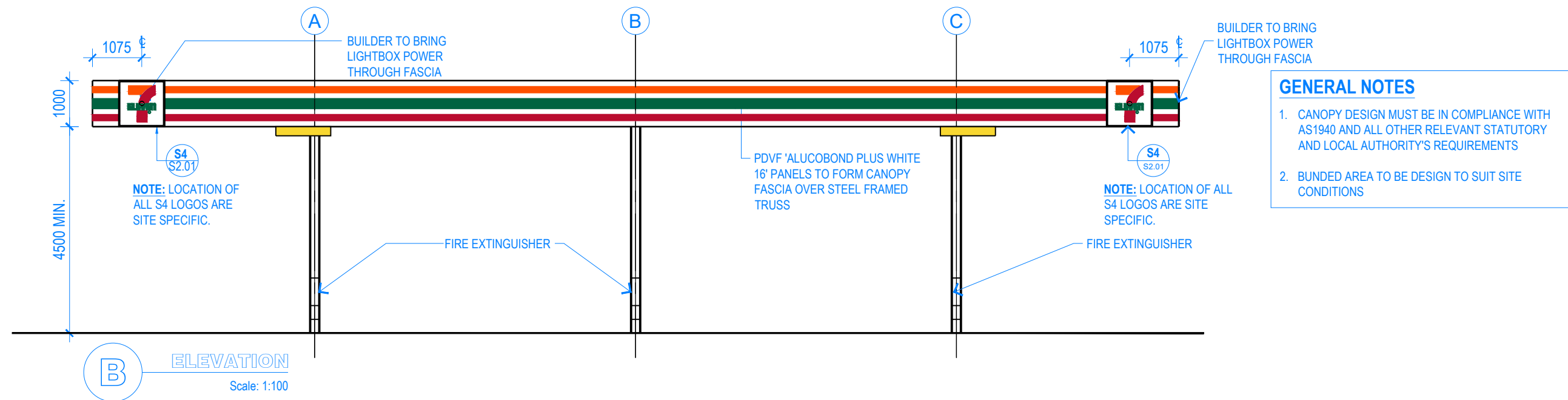
1. REFER TO DRAWING GEN17-D-A033 FOR SIGNAGE DETAILS
2. ALL 7-ELEVEN BRANDING TO BE SUPPLIED AND INSTALLED BY 7-ELEVEN
3. REFER STANDARD DRAWING GEN17-D-S010 & S030 FOR PANELS AND POSTS DETAILS



D SIDE ELEVATION
Scale: 1:100

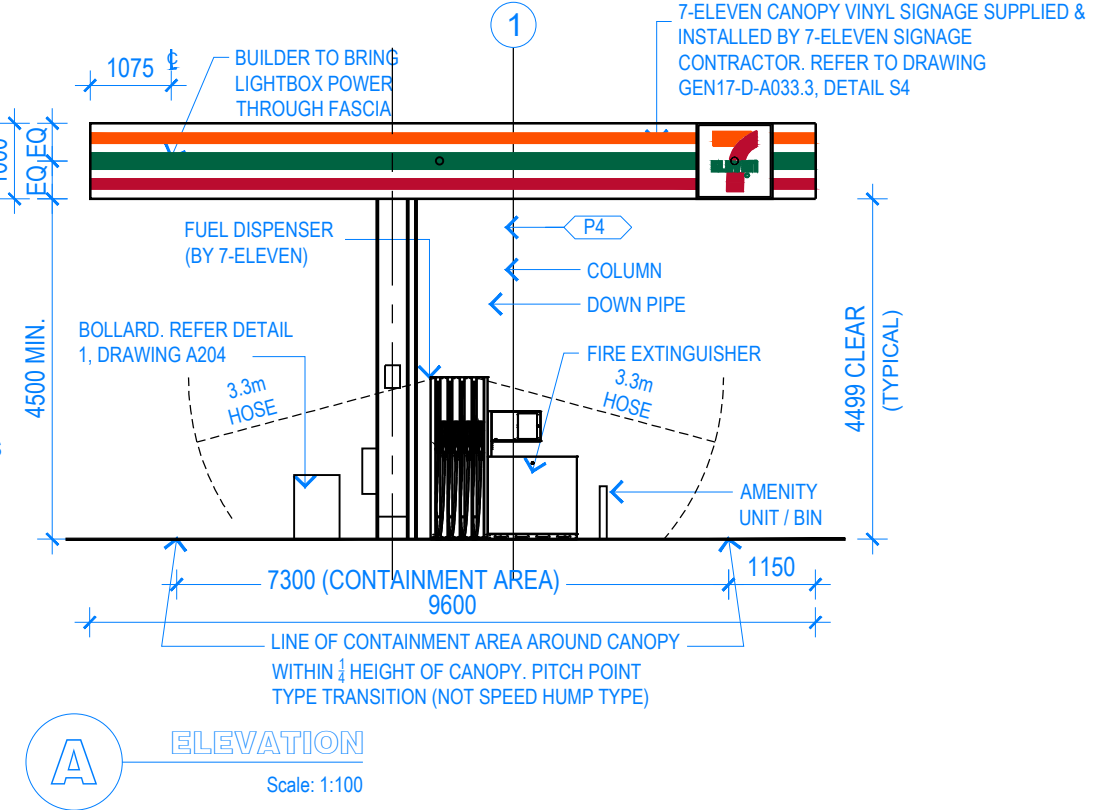
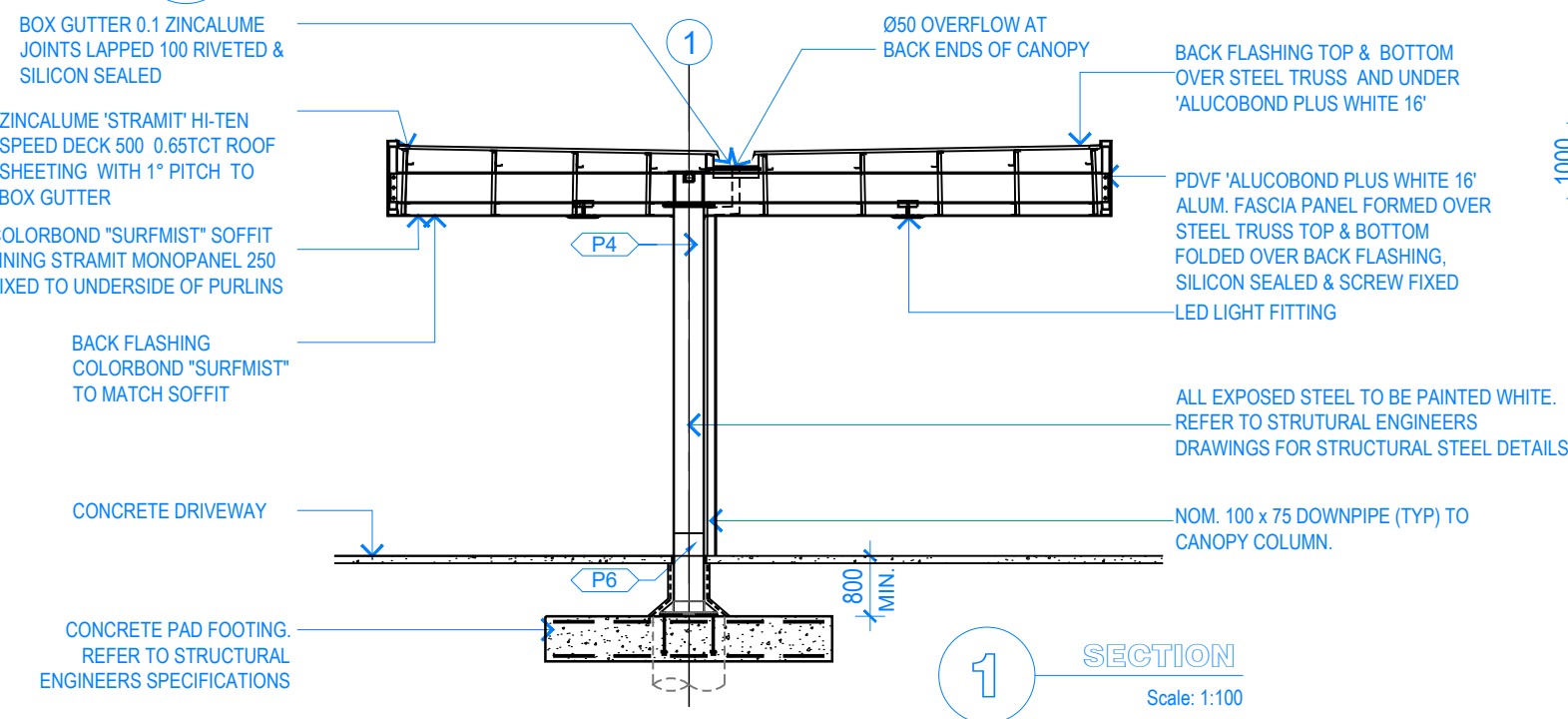
DEVELOPMENT
NOT FOR CONSTRUCTION



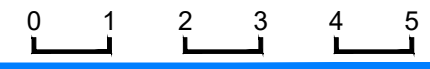


GENERAL NOTES

- CANOPY DESIGN MUST BE IN COMPLIANCE WITH AS1940 AND ALL OTHER RELEVANT STATUTORY AND LOCAL AUTHORITY'S REQUIREMENTS
- BUNDED AREA TO BE DESIGN TO SUIT SITE CONDITIONS



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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

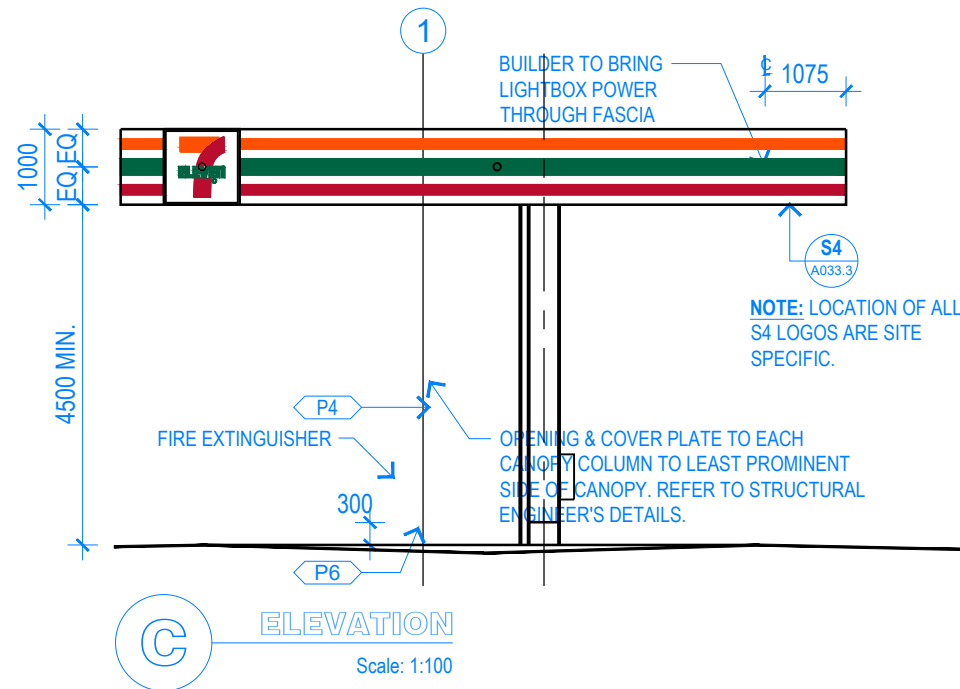
SHEET
ELEVATION
CANOPY
CAR
SHEET 1

REV.	DESCRIPTION	DATE
A	INITIAL DA PLAN	16/11/21
B	REVISED TO 7 ELEVEN COMMENTS	18/11/21
C	TOWER SIGN ADDED, OTHER AMENDMENTS	23/11/21

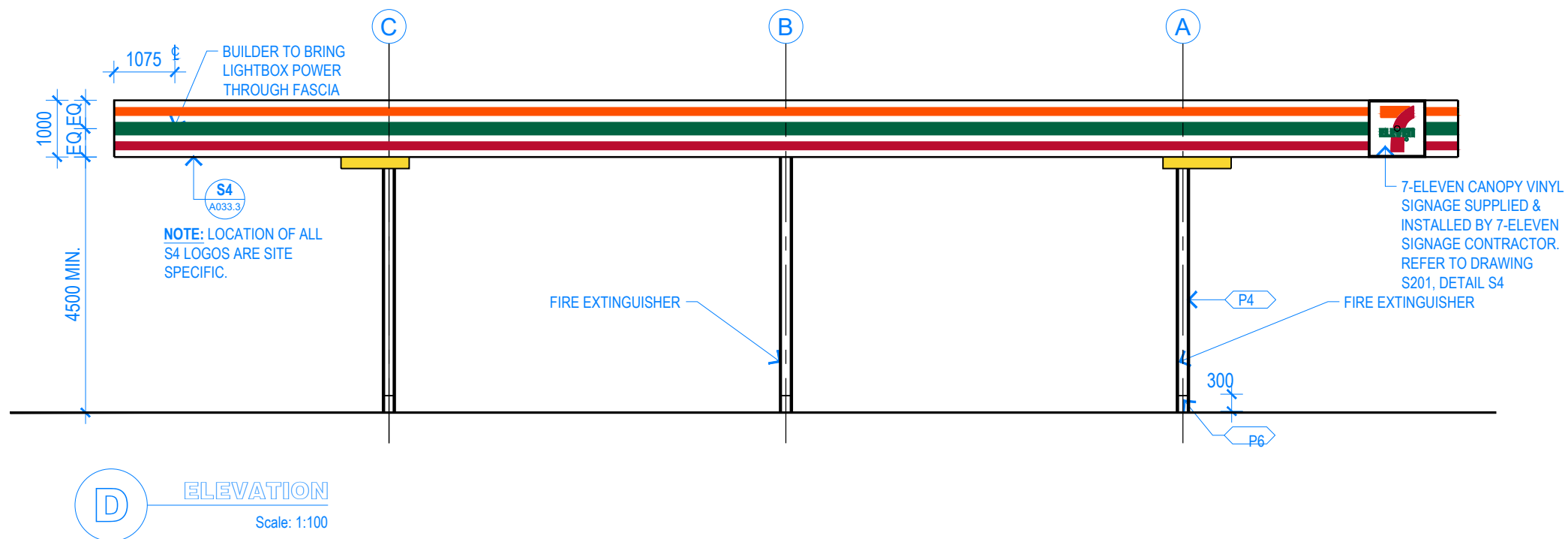
A203

1:100

C



- GENERAL NOTES**
1. THIS GENERIC DRAWING IS FOR INFORMATION ONLY AND NOT TO BE USED FOR CONSTRUCTION
 2. CANOPY DESIGN MUST BE IN COMPLIANCE WITH AS1940 AND ALL OTHER RELEVANT STATUTORY AND LOCAL AUTHORITY'S REQUIREMENTS
 3. BUNDED AREA TO BE DESIGN TO SUIT SITE CONDITIONS



DEVELOPMENT
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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
ELEVATION
CANOPY
CAR
SHEET 2

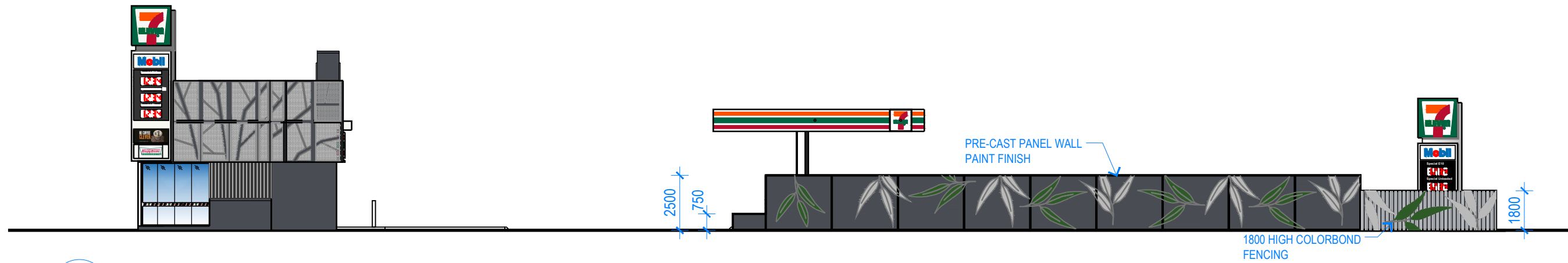
REV.	DESCRIPTION	DATE
A	INITIAL DA PLAN	16/11/21
B	REVISED TO 7 ELEVEN COMMENTS	18/11/21
C	TOWER LOGO ADDED, OTHER AMENDMENTS	23/11/21



A204

1:100 | C

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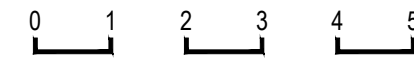


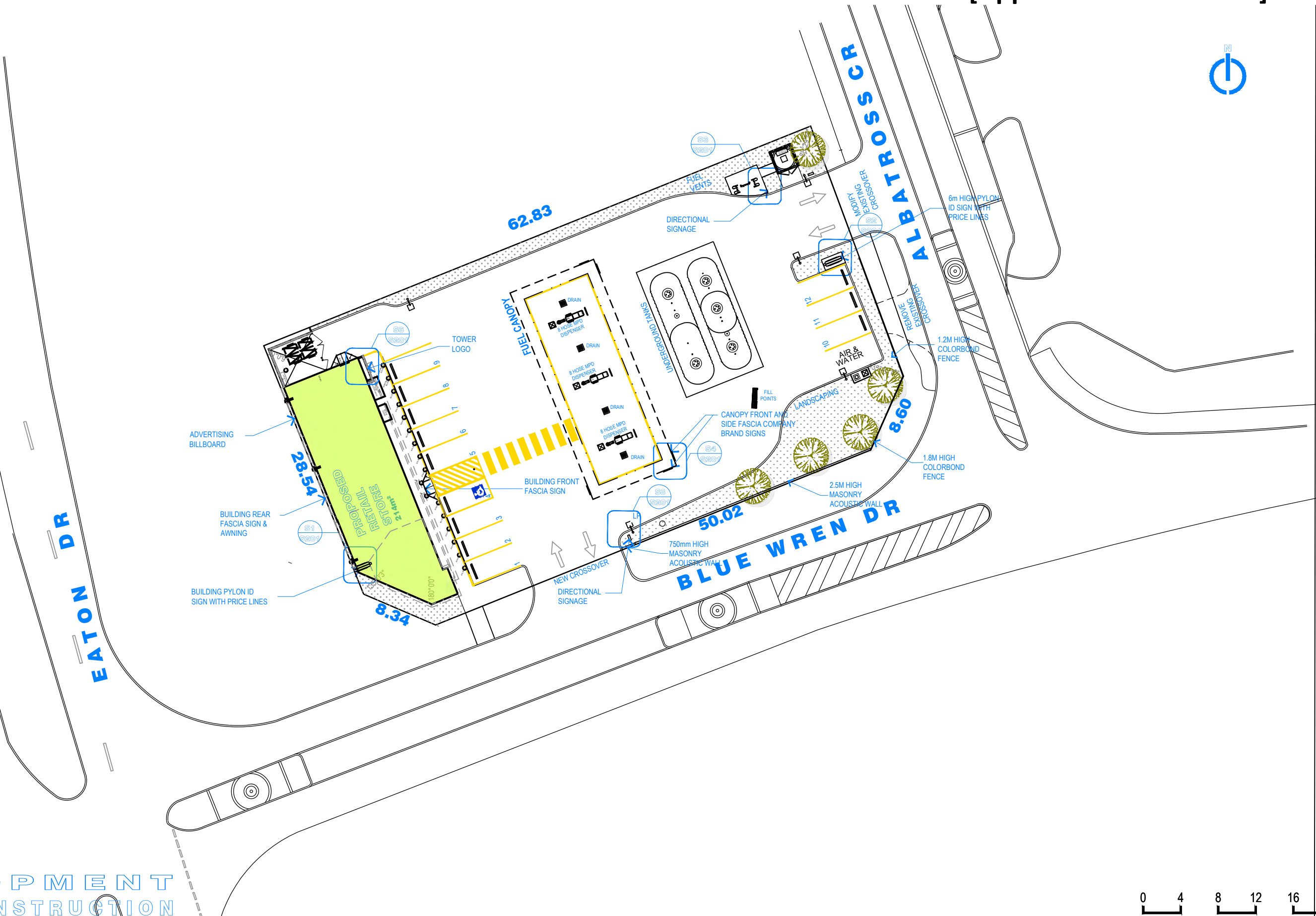
E BLUE WREN DRIVE ELEVATION
Scale: 1:200



F STREET ELEVATION
Scale: 1:100

DEVELOPMENT
NOT FOR CONSTRUCTION





DEVELOPMENT
NOT FOR CONSTRUCTION

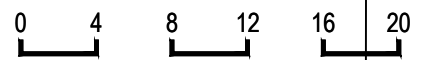


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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
PLAN
SITE
SIGNAGE

REV.	DESCRIPTION
A	INITIAL DA PLAN
B	REVISED TO 7 ELEVEN COMMENTS
C	TOWER SIGN ADDED, OTHER AMENDMENTS
D	NOISE WALLS ADDED
E	NOISE WALLS AMENDED

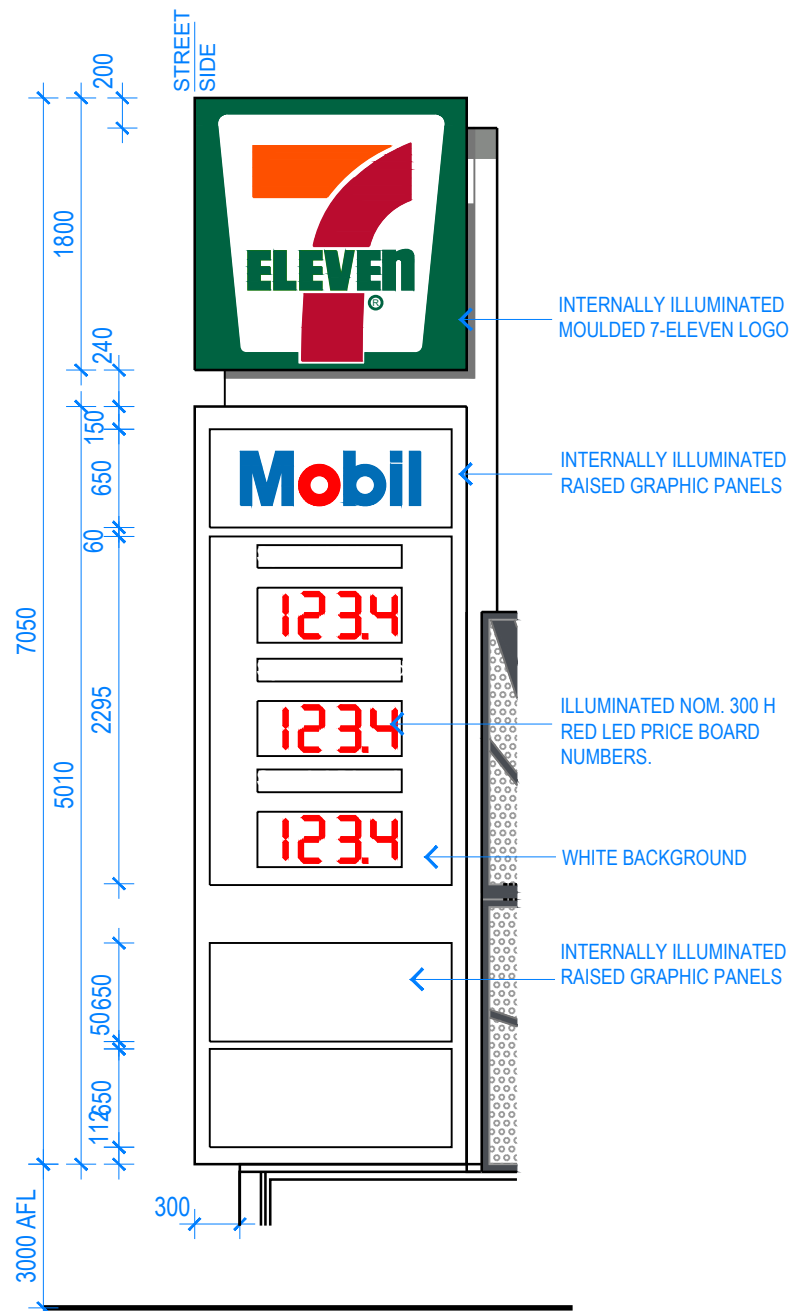


16/11/21
18/11/21
23/11/21
06/12/21
13/12/21

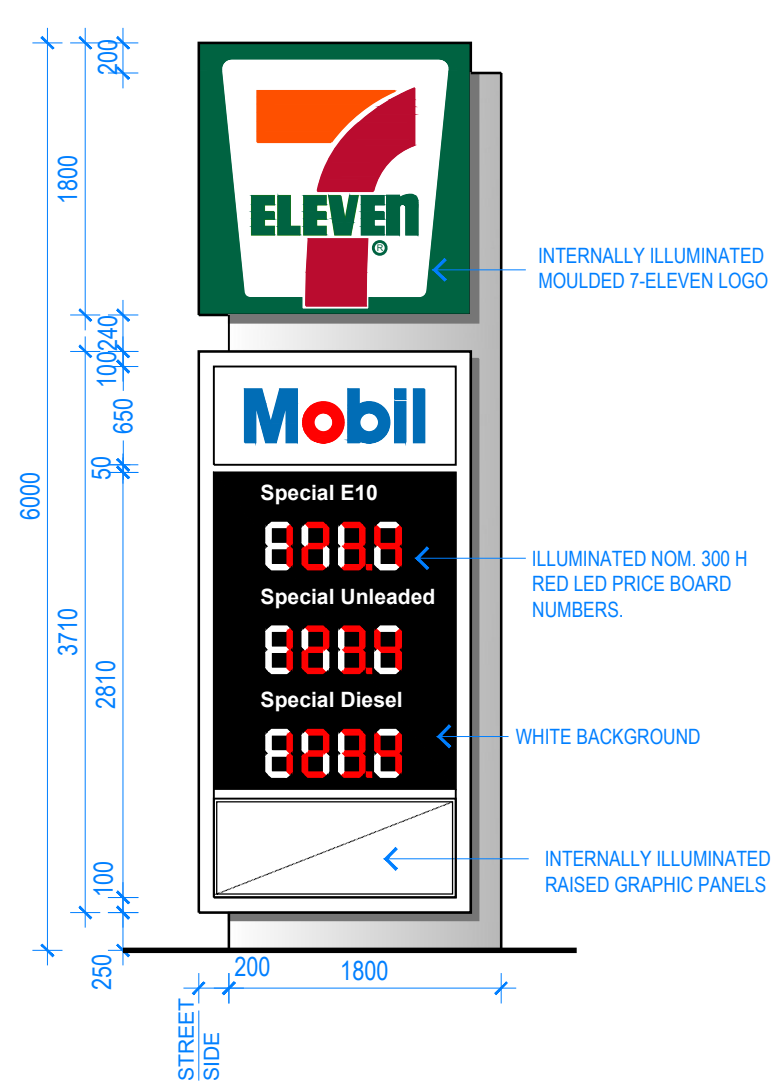
S101

1:400

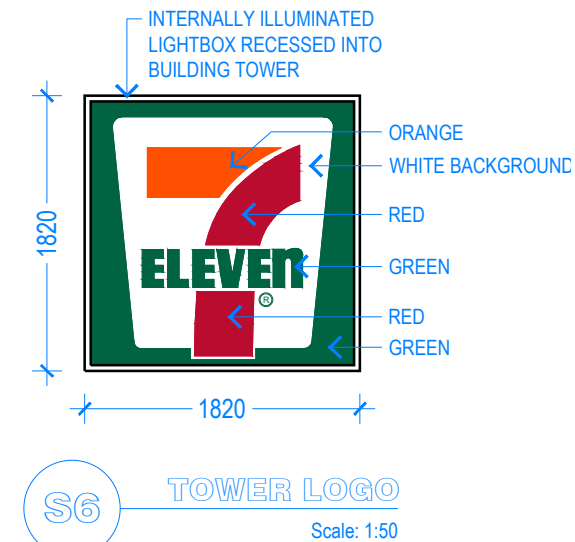
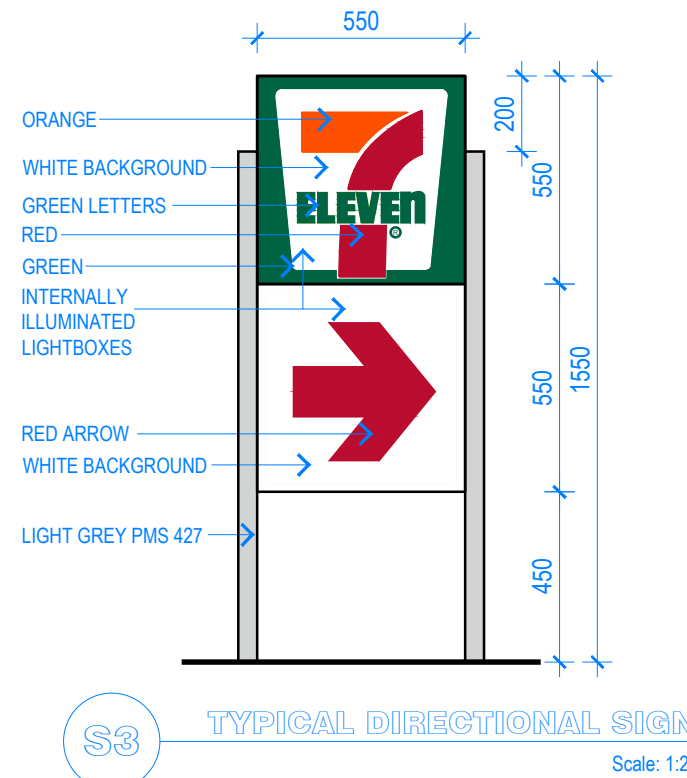
F



S1 BUILDING SIGN ELEVATION
Scale: 1:50

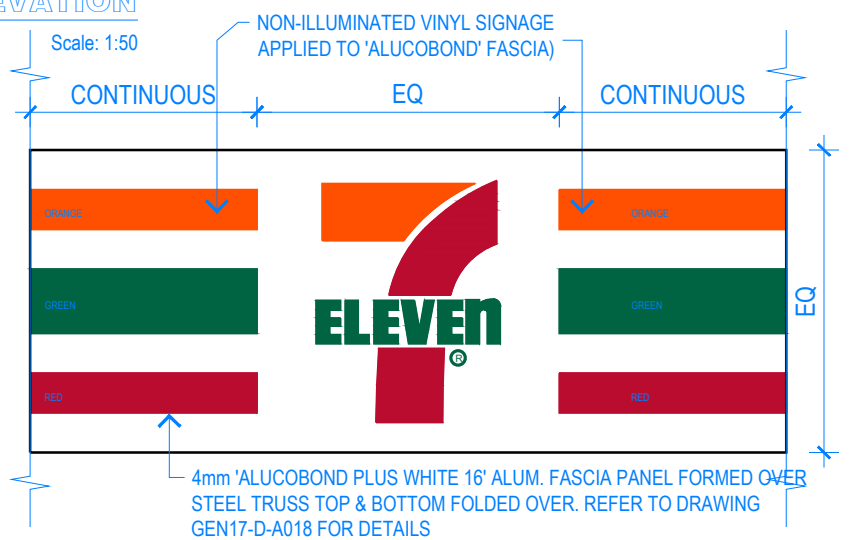


S2 6M PYLON SIGN ELEVATION
Scale: 1:50

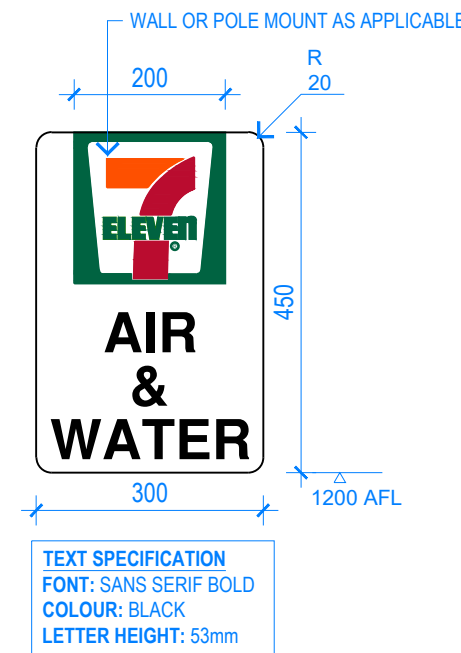


PRICEBOARD NOTES

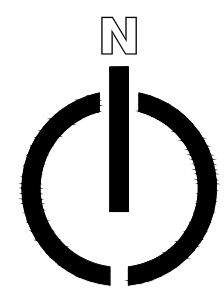
1. PRODUCTS DISPLAYED ON PRICE BOARDS ARE INDICATIVE ONLY
2. STEEL FRAME TO BE GALVANISED. REFER TO ENGINEER'S DETAIL FOR STRUCTURAL DETAIL



NOTE: REFER TO PROPOSED BUILDING ELEVATIONS FOR DIMENSIONS

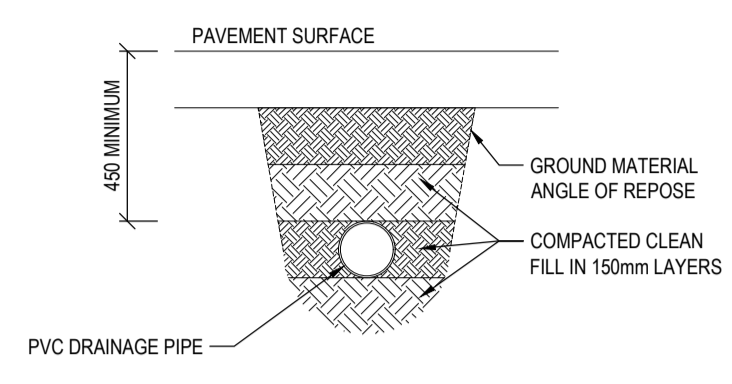


DEVELOPMENT
NOT FOR CONSTRUCTION



SITE DATA:
 SITE CLASSIFICATION = ASSUMED CLASS A SAND
 SOIL PERMEABILITY = ASSUMED 2.5 m/day
 GROUND WATER TABLE = ASSUMED OVER 2.0m BELOW EXISTING GROUND LEVEL

- GENERAL NOTES:**
- DATUM IS LOCAL AND TO BE VERIFIED ON SITE.
 - CHECK ALL DIMENSIONS ON SITE. READ ALL ENGINEERING DRAWINGS IN CONJUNCTION WITH ARCHITECTURAL & SURVEY DRAWINGS. ANY DISCREPANCIES BETWEEN ENGINEERING DRAWINGS AND ARCHITECTURAL DRAWINGS SHALL BE CONFIRMED PRIOR TO COMMENCING CONSTRUCTION. DO NOT SCALE FROM THESE DRAWINGS.
 - ALL WORK TO BE IN ACCORDANCE WITH 'AS3500-2003 PLUMBING & DRAINAGE'. THE 'BUILDING CODE OF AUSTRALIA' AND THE LOCAL AUTHORITY'S STANDARD SPECIFICATIONS.
 - LOT CONNECTION PIT (WHEN APPLICABLE) TO LOCAL AUTHORITY SPECIFICATIONS.
 - WHERE MANHOLES ARE LOCATED IN THE AREAS SUBJECT TO VEHICULAR LOADING, STANDARD TRAFFICABLE LIDS ARE TO BE INSTALLED & BASED TO BUILDERS DETAIL.
 - ALL DRAINAGE PIPEWORK SHALL BE PVC CLASS HD STORMWATER, UNLESS WHERE LOCATED UNDERNEATH ANY STRUCTURES PIPEWORK SHALL BE PVC SEWER CLASS S88.
 - ALIGNMENT OF PIPES SHALL BE AS SHOWN ON THE PLAN & SHALL BE TO THE PIPE OR MANHOLE CENTRELINE.
 - BEFORE CONSTRUCTION COMMENCES, THE CONTRACTOR SHALL:
 - CHECK ON SITE THE LOCATION OF THE EXISTING SERVICES WITH THE APPROPRIATE AUTHORITY. ENSURE PROPOSED STORMWATER PIPE DOES NOT CLASH WITH ANY EXISTING SERVICES.
 - ARRANGE FOR THE LOCATION AND THE LEVEL OF THE CONNECTION POINT TO EXISTING STORMWATER MANHOLE TO BE VERIFIED BY A SURVEYOR.
 - CONFIRM THAT BOUNDARY PEGS OR OTHER SURVEY REFERENCE POINTS TO BE USED IN SETTING OUT OF THE PROJECT ARE LOCATED IN THE CORRECT POSITIONS.
 - ENSURE A PERMIT & REINSTATEMENT SPECIFICATIONS ARE OBTAINED FROM THE LOCAL AUTHORITY IF EXCAVATION WILL BE IN A ROAD RESERVE OR RIGHT OF WAY.
 - ENSURE ALL DETAILS HAVE BEEN CHECKED AND THAT NO DISCREPANCIES EXIST. ALL QUERIES AND DISCREPANCIES ARE TO BE RESOLVED PRIOR TO COMMENCING WORKS.
 - ALL EXCAVATIONS SHALL BE SECURED & MADE SAFE IN ACCORDANCE WITH REQUIREMENTS OF THE OCCUPATIONAL SAFETY & HEALTH ACT OF 1984, THE OCCUPATIONAL SAFETY & HEALTH REGULATION 1996 & OF ANY RELEVANT REGULATORY BODY.
 - PROPERTIES WHICH HAVE BEEN EXCAVATED SHALL BE RETURNED TO AT LEAST A SIMILAR CONDITION TO THAT WHICH EXISTED PRIOR TO CONSTRUCTION.
 - TRENCH BACKFILL SHALL BE CLEAN GRANULAR MATERIAL, COMPACTED TO A LEVEL NOT LESS THAN THAT OF THE SURROUNDING UNDISTURBED GROUND, FOR THE FULL DEPTH OF EXCAVATION. BACKFILL UNDER ROADS SHALL BE COMPACTED TO THE REQUIREMENTS OF THE LOCAL AUTHORITY.
 - ALL CONNECTION INTO EXISTING LOCAL AUTHORITY STORMWATER ARE TO BE CARRIED OUT BY THE CONTRACTOR TO LOCAL AUTHORITY SPECIFICATIONS. THIS STORM WATER DESIGN IS TO BE READ IN CONJUNCTION WITH THE BUILDERS ARCHITECTURAL DRAWINGS (PARTIALLY REPRODUCED HERE). CLIENT IS TO ENSURE LOCAL AUTHORITY HAVE APPROVED THESE DRAWINGS BEFORE BEING ISSUED FOR PRICING, TENDER & CONSTRUCTION.

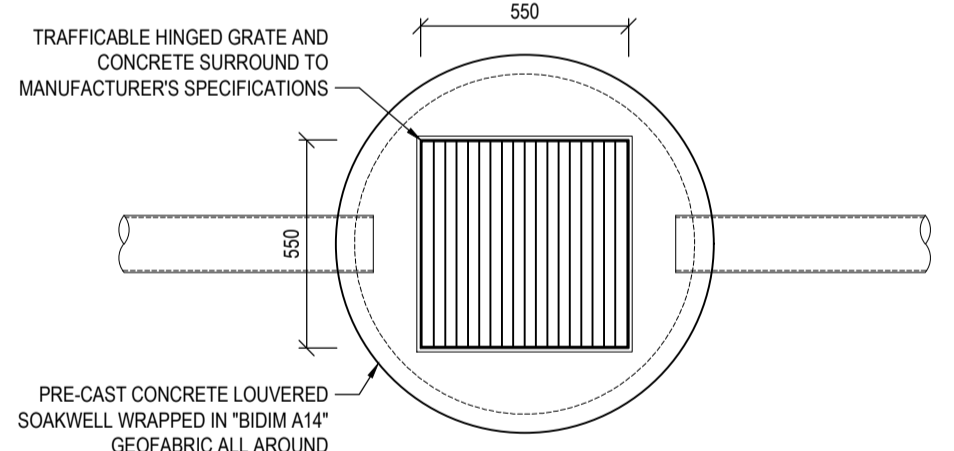


TYPICAL PVC DRAINAGE PIPE BEDDING DETAILS
SCALE 1:20

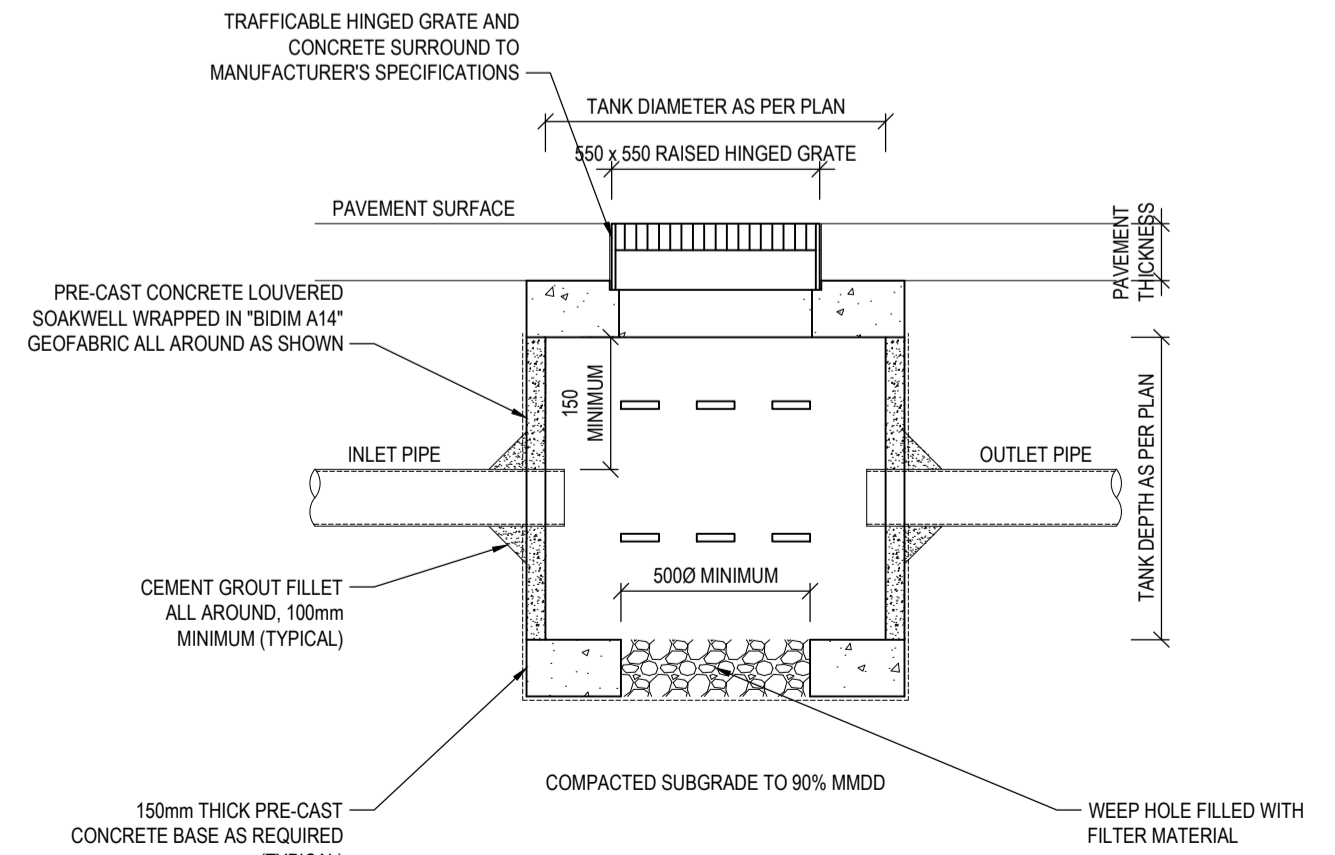
- LEGEND:**
- 1500 uPVC CLASS S88 PIPE
 - 1500 uPVC CLASS S88 PIPE FROM BUILDING DOWNPIPE UNO
 - DN160 UPP FUEL PIPE OR EQUIVALENT
 - ⊞ TRAFFICABLE STORMWATER TANK WITH GRATE COVER
 - ⊞ TRAFFICABLE STORMWATER TANK WITH CONCRETE COVER
 - ⊞ TRAFFICABLE PRECAST SUMP PIT WITH GRATE COVER
 - FALL DIRECTION
 - IL+10.00 PIPE INVERT LEVEL
 - TT+10.00 TOP OF TANK LEVEL
 - RL+10.00 PAVEMENT LEVEL
 - HP+10.00 PAVEMENT HIGH POINT
 - LP+10.00 PAVEMENT LOW POINT

DRAINAGE CALCULATIONS:

IMPERVIOUS AREA	1777 sqm
REQUIRED STORAGE	1777 / 65
	27.34 cum
SOAKWELL SIZE	1800mm Ø x 1200mm DEEP
STORAGE CAPACITY PER SOAKWELL	3.05 cum
NUMBER OF SOAKWELLS	9
TOTAL STORAGE CAPACITY	27.45 cum



TYPICAL SOAKWELL TANK PLAN
SCALE 1:20



TYPICAL SOAKWELL TANK DETAILS
SCALE 1:20

TANK SCHEDULE

TANK NUMBER	TANK SIZE	TOP OF TANK LEVEL	INLET/OUTLET PIPE INVERT LEVEL
T1 - T2	18000 x 1200DEEP	TT + 13.40	IL + 12.60
T3	18000 x 1200DEEP	TT + 13.50	IL + 12.70
T4 - T7	18000 x 1200DEEP	TT + 13.55	IL + 12.75
U1	18000 x 1200DEEP	TT + 13.45	IL + 12.95
U2	18000 x 1200DEEP	TT + 13.60	IL + 13.10

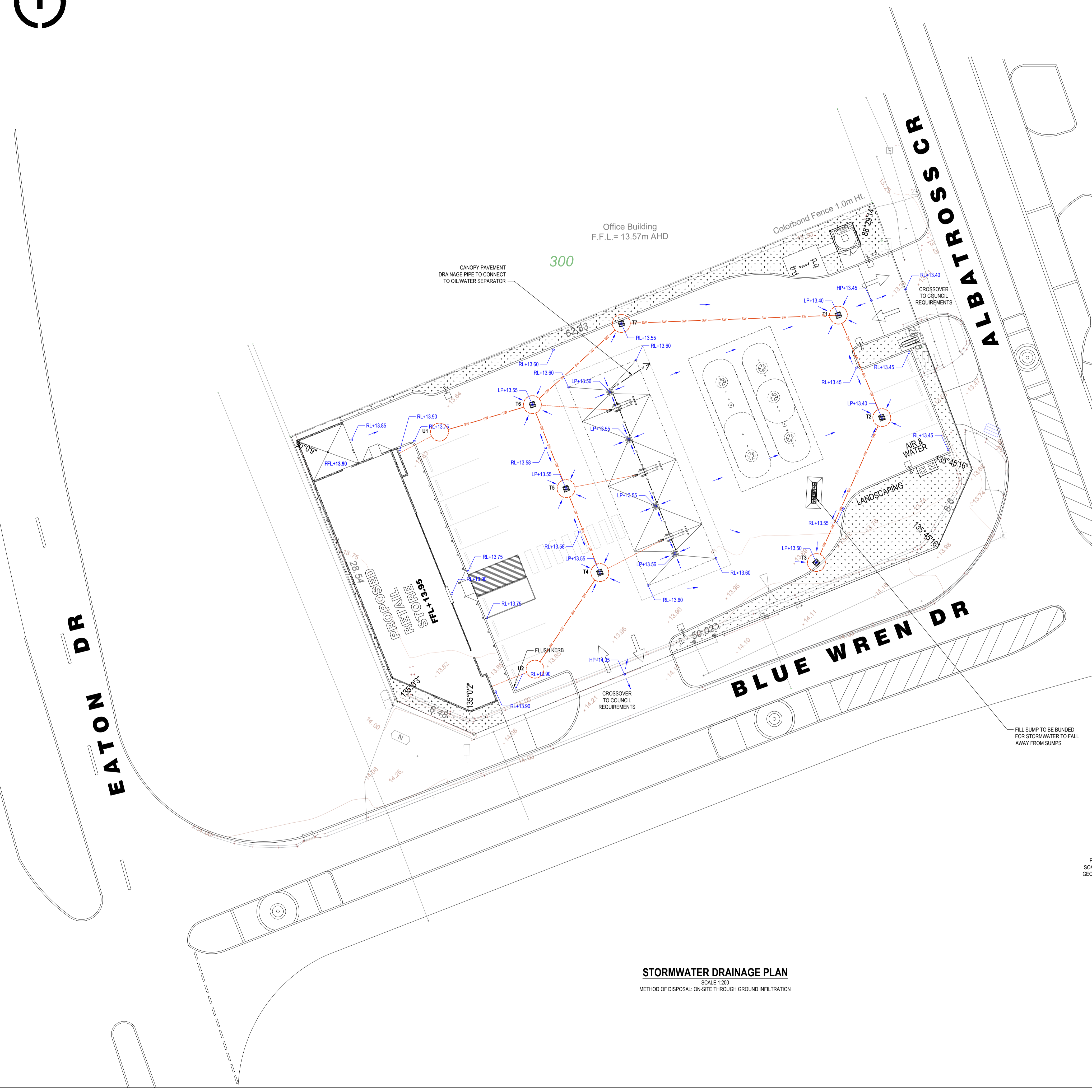
REV	DESCRIPTION	BY	DATE
A	ISSUE FOR DEVELOPMENT APPROVAL	JC	12-DEC-2021

ISSUE FOR DEVELOPMENT APPROVAL

PROJECT: **PROPOSED SERVICE STATION**
15 ALBATROSS CRESCENT, EATON, WESTERN AUSTRALIA

TITLE: **STORMWATER DRAINAGE PLAN AND DETAILS**

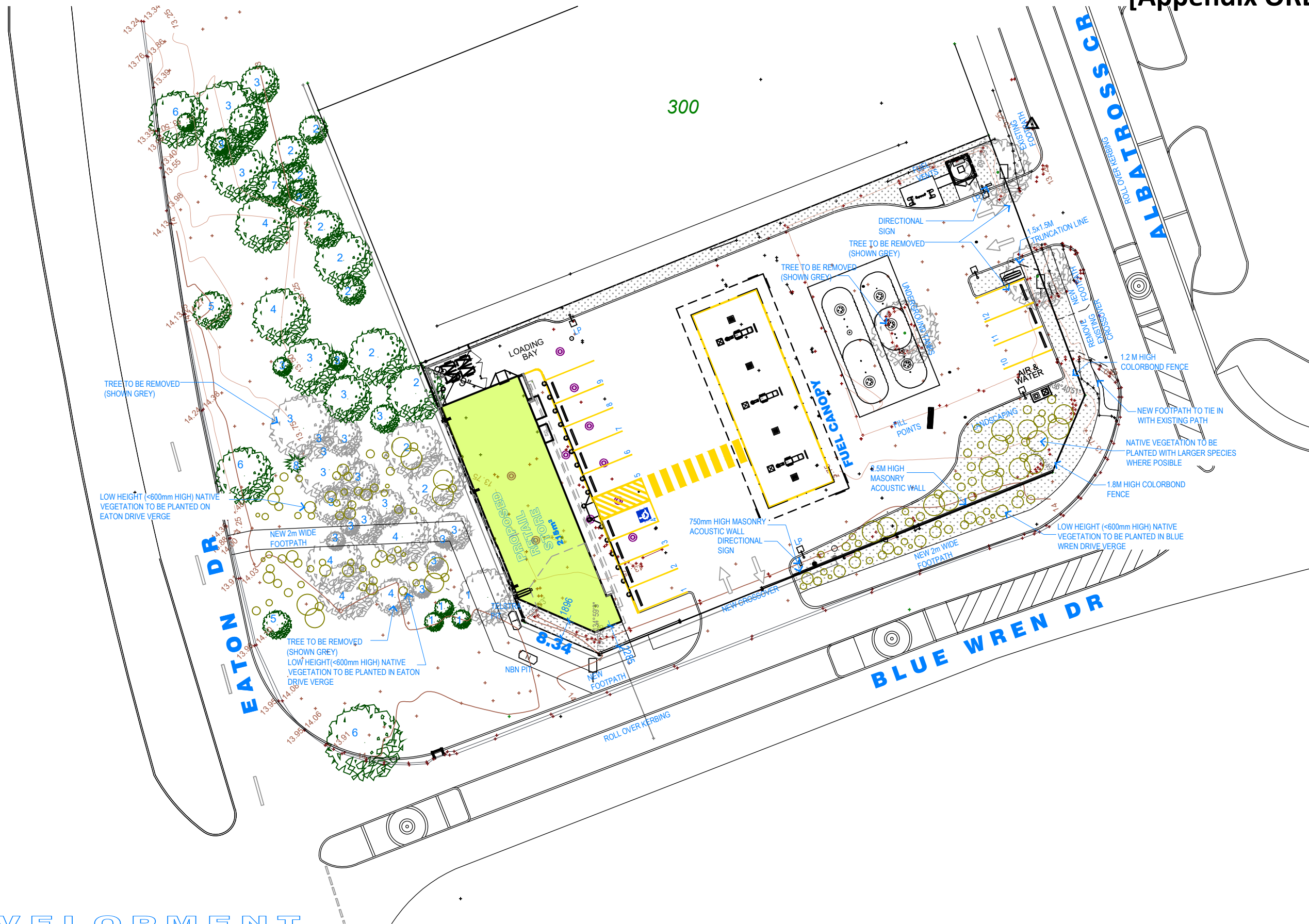
DRAWING:	SCALE:	PROJECT #:	REVISION:
J. GABRIEL	AS NOTED	20673	A
DESIGN:	PAPER SIZE:	DRAWING #:	
J. CUBONG	A1	C1	



STORMWATER DRAINAGE PLAN
SCALE 1:200
METHOD OF DISPOSAL: ON-SITE THROUGH GROUND INFILTRATION

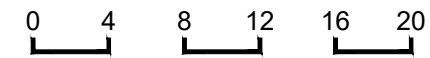
NOTE: ALL UNITS ARE IN MILLIMETERS UNO





LEGEND	
	LANDSCAPING
SITE DETAILS:	
TOTAL SITE AREA:	2112.7 m ²
LANDSCAPED AREA:	292.6 m ²
BUILDING AREA:	215 m ²
No. OF CAR BAYS:	12
No. OF SHADE TREES:	5
TREE LEGEND	
1.	Dwarf Bottle Brush
2.	Gum (Jarrah)
3.	Melaleuca (Paperbark)
4.	Eucalyptus (Yellow Flowering Gum)
5.	Peppy Tree
6.	Frangipani
7.	Bull Banksia
8.	Grass Tree

DEVELOPMENT
NOT FOR CONSTRUCTION



project consultants
DESIGN | DOCUMENT | DELIVER

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PROJECT
PROPOSED SERVICE STATION
15 ALBATROSS CRESCENT
EATON, WA, 6232

SHEET
VEGETATION PLAN
PROPOSED

REV.	DESCRIPTION	DATE
A	INITIAL ISSUE	02/03/22
B	CLIENT REVIEW	02/03/22
C	NOTES AMENDED	04/03/22

A105
1:400 | **C**

RISK ASSESSMENT TOOL

OVERALL RISK EVENT: JDAP Development Application for Service Station Development – Lot 303 (15) Albatross Crescent, Eaton

RISK THEME PROFILE:

7 - Environment Management

RISK ASSESSMENT CONTEXT: Strategic

CONSEQUENCE CATEGORY	RISK EVENT	PRIOR TO TREATMENT OR CONTROL			RISK ACTION PLAN (Treatment or controls proposed)	AFTER TREATMENT OR CONTROL		
		CONSEQUENCE	LIKELIHOOD	INHERENT RISK RATING		CONSEQUENCE	LIKELIHOOD	RESIDUAL RISK RATING
HEALTH	No risk event identified for this category.	Not Required - No Risk Identified	N/A	N/A	Not required.	Not required.	Not required.	Not required.
FINANCIAL IMPACT	No risk event identified for this category.	Not Required - No Risk Identified	N/A	N/A	Not required.	Not required.	Not required.	Not required.
SERVICE INTERRUPTION	No risk event identified for this category.	Not Required - No Risk Identified	N/A	N/A	Not required.	Not required.	Not required.	Not required.
LEGAL AND COMPLIANCE	No risk event identified for this category.	Not Required - No Risk Identified	N/A	N/A	Not required.	Not required.	Not required.	Not required.
REPUTATIONAL	No risk event identified for this category.	Not Required - No Risk Identified	N/A	N/A	Not required.	Not required.	Not required.	Not required.
ENVIRONMENT	<p>Short term off site impacts may occur if noise and stormwater issues are not managed correctly.</p> <p>There may also be some community upset if all vegetation on the Eaton Drive verge is cleared.</p> <p>Increased traffic as a result of the proposal may impact vehicle movement along Eaton Drive.</p>	Moderate (3)	Likely (4)	Moderate (5 - 11)	<p>Not required.</p> <p>Impacts will be minimal and managed through the imposition of conditions.</p>	Not required.	Not required.	Not required.

